



CITY OF BREVARD

**COMPREHENSIVE
PEDESTRIAN PLAN**

2006

RECOMMENDED DRAFT
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PLANNING DEPARTMENT
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SECTION 1: INTRODUCTION

• Vision Statement

Brevard (population approximately 6,800) is the county seat of Transylvania County in the southern mountains of western North Carolina. Many residents have chosen to live in Brevard because of the topography and climate of the region and because of the city's walkability. Brevard's mild climate is such that walking is a pleasant form of transportation almost year-round. The topography makes walking for recreational purposes appealing to both visitors and residents with varying degrees of athletic abilities.

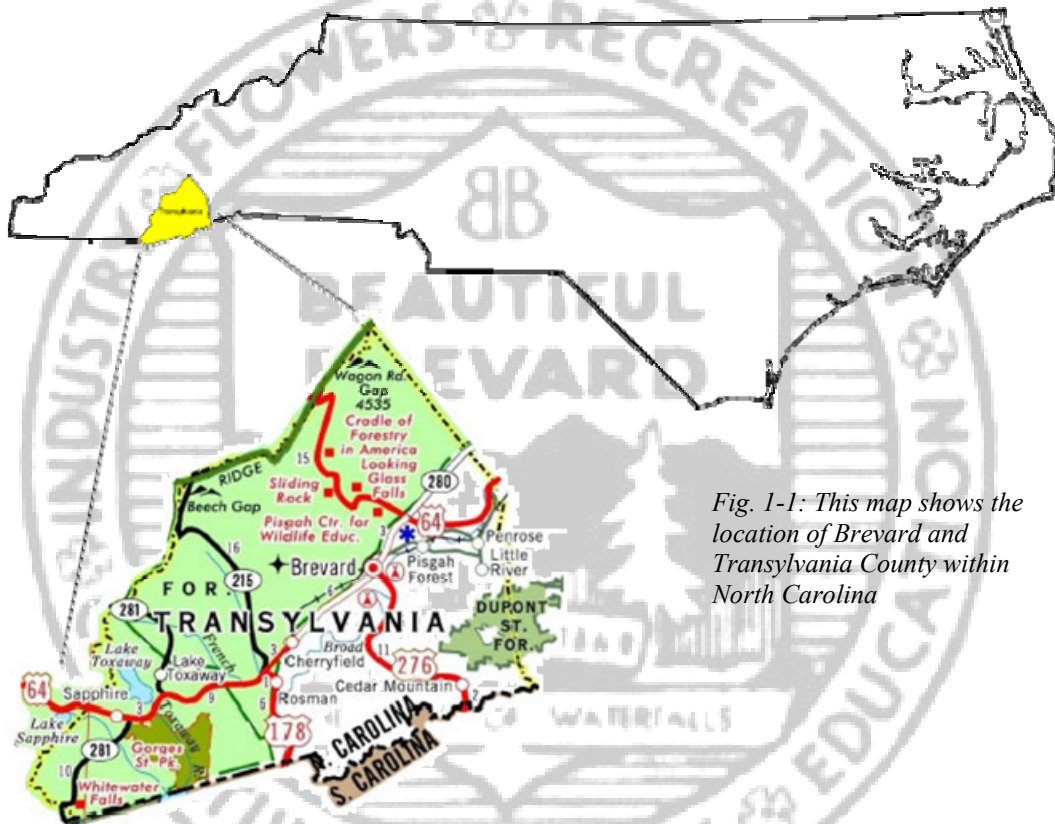


Fig. 1-1: This map shows the location of Brevard and Transylvania County within North Carolina

Brevard's pedestrian user groups include, but are not limited to, school children, students and faculty of Brevard College, active seniors, and visitors to the area. Facilities must be attractive and accessible to get much use. The downtown area is pedestrian-friendly, but facilities are more scarce as one ventures further from the central area. Many residents interested in pedestrian access in Brevard would like to see, and plans are in the works for, facilities linking Downtown to the Pisgah National Forest. Residents of Brevard walk for exercise and to run errands or shop, using sidewalks and streets within their own neighborhoods, sidewalks and streets in other neighborhoods, and trails or paths. Safe routes are desired by all, and will likely result in more residents becoming pedestrians.

This Pedestrian Plan will be the groundwork for future pedestrian facility installation and maintenance and includes program ideas and initiatives. It is desired that pedestrians will be able to safely walk from any point in Brevard to another. To

ensure this, sidewalks and other pedestrian facilities will have to be installed in locations where none are available, and existing facilities will need to be maintained or upgraded. If public transit is to be available in the future, there will need to be access points where pedestrians become transit users and vice versa.

There are many benefits to making pedestrian trips. When people get out of their vehicles and walk short distances, results include their own improved health, a cleaner environment, and a safer place for other pedestrians. One person may not decrease the volume of traffic by much, but every person that takes a walking trip instead of using a motor vehicle is making a difference.

• History

With the high prices of gasoline in the forefront of people's minds, many are considering alternate forms of transportation. In the past few years, non-motorized transportation has seen a renaissance of interest. More people are seeing the benefits of walking, including better health, a cleaner environment, more chances for social interaction, and the obvious economic benefits of not having to pay for gasoline.

The City of Brevard has taken the initiative to expand the existing network of sidewalks beyond their current bounds and filling in areas that are missing. In the past decade, the Thoroughfare Plan for the City of Brevard, the City of Brevard Land Use Plan, the Brevard Long Range Transportation Plan, and the Brevard Downtown Conceptual Master Plan have been drafted and adopted by the city. As a result, ordinances are now in place that require incoming businesses and developments to construct sidewalks, grant easements, or pay a fee-in-lieu that will help cover the cost for the city to install a comparable pedestrian facility. In September of 2003, a milestone was reached when the first of three phases for the currently planned multi-use path (MUP) was opened – a 10-foot wide asphalt path running 1.3 miles from the City of Brevard Sports Complex to the Blue Ridge Community College Transylvania Center. The trail serves Pisgah Forest Elementary School, the Transylvania County Recreation Center, and Transylvania Community Hospital along its route. Currently, the City of Brevard is planning for the existing section of the multi-use path to extend from both ends. From the City of Brevard Sports Complex, the path will extend to the Pisgah National Forest, and from Blue Ridge Community College the path will extend past Brevard Middle School to McLean Road.

Several local agencies and groups are involved in pedestrian awareness initiatives. Among them are the City Government, Transylvania County Schools, the healthcare community, Brevard College, and private citizen groups. Each of these groups is represented in some way on the Pedestrian Plan Project Steering Committee (PSC). A community-wide visioning process known as Focus 2020 was initiated by the citizens of Brevard in the late 1990s to gather information about how residents of Brevard envisioned the city and county developing through 2020. Several guiding principles were formed, including the desire to plan for a well-designed, connected community with a traditional, friendly, mountain character and practicing environmental stewardship. As an additional statement of community support, when asked to fill out a Pedestrian Needs Assessment Survey and return it to the city, 167

surveys were returned, and nearly 100 respondents wrote out thoughtful comments regarding the pedestrian facilities in Brevard.

• Goals and Objectives



The long range goal for pedestrian facilities within Brevard is that pedestrians will be able to safely and efficiently walk from any pedestrian generator to any destination. In the more immediate term, pedestrians should be able to safely walk on existing facilities, and key destinations such as schools should be linked with large pedestrian generators such as residential neighborhoods. By providing a greater number of safe and accessible walkways, it is anticipated that people who currently prefer using motorized transportation will look more positively at walking to destinations rather than traveling by automobile. In addition, the provision of pedestrian facilities improves the quality of life and livability of an area. New residents will be attracted to the area, as will businesses, so economic development will also increase.

• Scope and Purpose of Plan

One of the key issues that keep projects from getting done is the sheer number of candidate projects. There are so many things that need to be done that some fall through the cracks. To prevent this from being a problem, there needs to be a comprehensive list of projects, prioritized and reviewed on a regular basis. Completed projects should be removed and new projects added as demand changes.

The study area for this plan primarily incorporates the limits of the extra-territorial jurisdiction (ETJ) of the City of Brevard. Also included in the study area were Davidson River Road, Deavor Road, and the portion of Hendersonville Road between Asheville Highway and Upper Glade Creek Road.

This pedestrian plan proposes the construction or improvement of various sidewalk segments and facilities to offer connectivity throughout the aforementioned study area. It will also provide suggestions for maintenance of existing and future pedestrian facilities after their construction. Both on-road and off-road facilities will be included. Pedestrians will be the primary beneficiaries – by design, off-road multi-use paths will also benefit bicyclists.



Fig. 1-2: The red and purple outlined areas above show the area included in consideration for projects in this plan.

The topography, hydrology, location of property lines, finances, and growth in and around Brevard limit the number of sidewalks that are in compliance with the Americans with Disabilities Act Accessibility Guidelines (ADA-compliant), including having sidewalks on both sides of all roadways. Constructing sidewalks on one side of the road where two are not feasible is allowable. Since the proximity to the Pisgah National Forest, French Broad River, and Davidson River draws in athletic, environmentally-conscious people who want to walk from place to place, connecting pedestrian generators and attractors is especially desirable.

This plan should aid in the implementation of a complete network of pedestrian facilities throughout the study area. The plan does not focus only on sidewalks, but also includes multi-use paths, curb cuts, crosswalks, and signage standards. Through creating a more pleasant system for pedestrians to use, walking trips will be increased. Access will be improved and people will realize walking as a feasible transportation option.

The majority of the existing sidewalk sections in Brevard are in good to excellent condition. Approximately one-third of all sidewalk sections in Brevard are in fair or poor condition. A list of Existing Sidewalk Conditions can be seen in Appendix B. The first phase of the multi-use path, opened in 2003, is in excellent condition. With the first phase in place, there is an increased desire for other sections to be built. Plans are in the works for the path to extend past Blue Ridge Community College and Brevard Middle School towards McLean Road. The path is also intended to extend along Ecusta Road from the Sports Complex to the Pisgah National Forest. The NCDOT is currently installing and synchronizing new traffic signals along US 64 / US 276 / Broad Street through Brevard. While they are making those improvements for traffic flow, they are also installing pedestrian signal heads. In addition, they are re-striping lanes and marking crosswalks across this major thoroughfare. Opportunities for the near future include building a section of multi-use path on Gallimore Road in front of the Boy's and Girl's Club in conjunction with an NCDOT turn-lane project and constructing a multi-use path that connects the Bracken's Creek watershed to the downtown area. Each new development or redevelopment in the area also provides the opportunity for pedestrian facility construction and easement dedication. In the more distant future, pedestrian facilities may be included in projects that are listed in the Transportation Improvement Plan (TIP) or Land-of-Sky Comprehensive Transportation Plan (CTP). For example, while acquiring easements for and designing for the widening of Caldwell Street to three lanes, sidewalk construction on both sides should be considered. When built, the proposed West Loop, which follows the path of and would replace a portion of Phase 3 of the Brevard Multi-use Path, should include a multi-use path along one side or the other.

To better understand the perceived needs of prospective users, a survey was conducted. Survey responses indicated that residents walk primarily for exercise and to complete errands. People want to get from one location to another in the least complicated way, whether by foot or by motor vehicle. This requires attention to crosswalks and parking lots, where pedestrians and motorists have to use the same space. Many survey respondents noted that heavy or speeding traffic impeded them from walking, as did a lack of sidewalk availability. Pedestrians desire to be a safe

distance from traffic on well-maintained sidewalks so that tripping hazards are reduced. Brevard is home to senior citizens, college students, grade school students, wheelchair-dependents, blind, deaf, and cane users, and all of their needs should be met. Following ADA guidelines creates a universal design which can be navigated by all groups, so all pedestrian facility construction will follow ADA guidelines and NCDOT design standards will be used.

In the short-term, connections within Brevard and to the Pisgah National Forest are desired. Programs that educate drivers and pedestrians about pedestrian-related laws and safety are also a top priority. In the long range, connections should also be provided to areas that are just outside of Brevard, such as Pisgah Forest and Rosman, and to areas around the county. The ideal goal is to provide pedestrian connections between Brevard and Transylvania County and the surrounding counties – Henderson, Buncombe, Haywood, and Jackson – as well as the entire mountain region. Mapping of the system as it progresses should be made available and easily accessible to the public.

Several projects on the 2006-2012 TIP are located within Brevard's planning jurisdiction. Among these projects is the proposed US-64 bypass (R-2702) slated for post years, meaning the project will likely be constructed after 2012. When this project is carried out, both motorized and non-motorized transportation modes must be accommodated. The multi-use path from downtown to Pisgah National Forest (E-4143) is currently under construction. One phase has been completed and other phases are in design. Several bridge replacements are being planned, including bridge #193 over the Davidson River (B-4291, FFY 07-08), bridge #12 over the East French Broad River (B-4823, FFY 09-10), and bridge #13 over Tucker Creek (B-4822, FFY 09-10). When these bridges are replaced, they should be designed and built to accommodate both motorized and non-motorized transportation.

Currently, public transit in the Brevard area is limited. Transylvania County has an on-call bus service operated by Western Carolina Community Action. Anyone can ride the bus, though certain buses are dedicated to special populations. Prospective riders must call and schedule a pickup one day in advance. To enhance the connection points between pedestrian and future public transit modes of travel, facilities such as benches and waste receptacles could be provided in areas that would logically lend themselves as future transit stops. In the future, if or when such plans become a reality, transit shelters could be built around the existing facilities.

To see the end result, much planning and fundraising will need to be done. Pedestrian advocacy groups, citizens, elected officials, and government agencies will have to work together to see that this plan becomes a reality. A number of possible facilities are listed in Section 7 of this document. Policies have also been recommended to take advantage of incoming development to expand the pedestrian network. About 75% of the funding for these projects will need to come in the form of grants and government budgeting, as the city can only cover about 25%. Other programs to raise awareness for pedestrians, their needs, and safety could be started voluntarily. Information distributed in a timely manner will allow many to get involved.

SECTION 2: EVALUATING CURRENT CONDITIONS

• Overview

Approximately two-thirds of sidewalk sections in Brevard are in good to excellent condition and currently require only routine maintenance to keep them operating at a reasonable level. The rest of the sidewalk sections in Brevard are in fair or poor condition, and these are the ones that require the most immediate attention, whether in the form of major repairs and upgrades or replacement. Map 8-2 illustrates the current condition of existing facilities. A list of Existing Sidewalk Conditions can be seen in Appendix B.

• Determine Community Concerns, Needs and Priorities

In order to address the concerns of the community, public involvement was necessary. Many opportunities were given for the public to give their input. Residents have always had the option of making requests directly to city officials. To draw information from those who may prefer to give an anonymous account, surveys were mailed with the City of Brevard Newsletter in early June 2005 and responses were received in mid-June 2005. Public Input Sessions were also conducted on July 28 and December 6, 2005 as additional opportunities for the public to voice their needs, concerns, and priorities. Pedestrian advocacy groups were specifically invited to the Public Input Session, along with survey respondents and members of demographic groups that were under-represented by the surveys, primarily college and high school students. Previous media contacts were used and new ones found to spread the word about the Pedestrian Plan and various input opportunities. A complete listing of contacts can be found in Appendix C.

The Pedestrian Plan Project Steering Committee (PSC) was also formed to help ensure that various groups were represented. The PSC consisted of:

- Josh Freeman, Brevard Planning Director;
- Don Owen, Brevard Public Works;
- Susan Stromberg (pre-09/05) / Amber Morrish (post-09/05), Brevard College;
- Captain David Carter, Brevard Police,
- Carrie Runser-Turner, Land-of-Sky Regional Planning Organization;
- Russ Knights, Resident;
- Frank Pearsall, Resident;
- John Tinsley, Transylvania County Board of Education;
- Steve Smith, Transylvania County Health Director; and
- Tissie Stroup, Transylvania County Recreation Department.

Staff and agency concerns and perceived issues are also important. The Planning Department wants to ensure that no opportunities are missed to improve the quality of life in Brevard. They are responsible for writing and enforcing the ordinances that require new development and redevelopment throughout the city to install and maintain pedestrian facilities and/or provide right-of-way for the city to construct

such facilities. The City of Brevard Streets Division installs and repairs streets, sidewalks, and curb ramps; paints crosswalks; mows within the right-of-way; and erects and maintains traffic-related signs throughout the city. They are constrained by budgets and time, and this plan will help set a timeline of projects and aid in the acquisition of funds. To the Transylvania County Recreation Department, multi-use paths and sidewalks provide greater opportunities for recreation and connect a large number of people to existing parks in the area. The Police enforce pedestrian and traffic laws in order to increase safety for everyone. Having well-defined areas for pedestrians to safely walk lowers the chance that an officer will be called on to respond to a crash and ticket or even jail someone for injuring a pedestrian. The health community has long known the benefits of exercise. Walking from place to place helps people burn fat, lose weight, and lower cholesterol, among other benefits. Healthier people make fewer expensive medical insurance claims. At the same time, a safe route for walking must be provided so that the number of injured pedestrians is limited.

The public wants safe routes to popular destinations. Brevard Music Center lends many pedestrians to Probart Street in the summer, and it can be dangerous in areas where no sidewalks are provided. Gallimore Road is a popular walking route to both Brevard Elementary and High Schools, and no pedestrian facilities are provided, nor is there any signage on Greenville Highway to inform drivers that pedestrians are present. Many people want to see a safe way to get from downtown to the Pisgah National Forest, as Broad Street is a dangerous route with heavy traffic and many driveways severing the available sidewalks, making them discontinuous.

Nine collisions involving pedestrians were recorded in the jurisdiction of the Brevard Police Department between March 2004 and May 2005. Of those documented, two took place in the Walmart parking lot and three were due to a failure to yield to a pedestrian in a crosswalk. None of the collisions resulted in life-threatening or disfiguring injuries, but almost all resulted in apparent or possible injuries to the pedestrian. The crash data available did not indicate that any one group of people were more susceptible to pedestrian collisions than others, though a larger set of data may have indicated otherwise.

According to *walkinginfo.org*, 32.2% of all crashes involving pedestrians occur within 50 feet of an intersection. 30% of these involved a turning vehicle, 22% involved pedestrians darting out or running across an intersection in front of a vehicle whose view was blocked prior to impact, and 16% were caused by drivers failing to yield right-of-way¹. Pedestrians must practice “defensive walking”, much as drivers are educated to practice “defensive driving”. It cannot be assumed that every driver knows and obeys laws relating to pedestrians. At the same time, drivers must be educated about how to

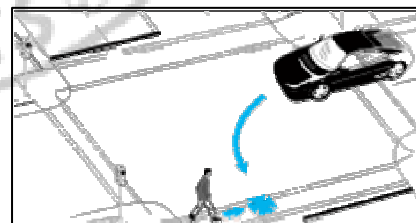


Fig. 2-1: Nearly one-third of all pedestrian crashes occur in or near intersections.¹

¹ *Pedestrian Crashes – Crash Types*. Pedestrian and Bicycle Information Center. 07 March 2006
<<http://www.walkinginfo.org/pc/types.htm>>

interact with pedestrians. One of the topics that must be reiterated is that drivers are to yield to pedestrians in crosswalks. This may be done through the use of “Yield to Pedestrians” signs at crosswalks and through education. The NCDOT Division of Bicycle & Pedestrian Transportation has put together *A Guide to North Carolina Bicycle and Pedestrian Laws: Guidebook on General Statutes, Ordinances, and Resources*. This handbook can serve as a valuable educational tool and is available to motorists, pedestrians, law enforcement, and the general public.

• Assess Pedestrian Friendliness of Local Transportation System

The idea of designing for pedestrian access is becoming more mainstream, so the local roadway system is becoming friendlier to pedestrians, but there is still much to be done. In Downtown Brevard, sidewalks are ample and crosswalks are painted. There are a fair number of pedestrian signals in this area, so pedestrians know when they can cross the streets. Further out from downtown, neither marked crosswalks nor pedestrian signals are provided, making street crossings uncomfortable for some pedestrians. Some bridges and culvert crossings in the area are so narrow that pedestrians must walk in the travel lane if they want to cross. All of these situations are dangerous and should be taken care of as soon as possible.

The pedestrian system must connect popular origins and destinations. On-road and off-road facilities can both be used. Both types of facilities will be used by many users for both transportation and recreation, especially if they connect popular destinations.



Fig. 2-2: This section of sidewalk on Broad Street near Chestnut Street (seen left of center in this photo) leaves pedestrians with no refuge from swift-moving traffic.

Currently, there are deficiencies in the system. One example is the sidewalk on Broad Street near Chestnut Street, where pedestrians are dumped into the travel lane, creating a very dangerous situation. A section of sidewalk is missing on West French Broad Street between Broad Street and Caldwell Street with no safe alternative route within two blocks. The intersection of Broad Street and Caldwell Street north of downtown creates a hazardous sea of asphalt with

no safe way for pedestrians to cross. This intersection separates Brevard College from a nearby grocery store, video store, and restaurant, among other things. The lack of a sidewalk or lighting down the steep terrain of Probart Street creates unsafe walking conditions to and from the Brevard Music Center and Brevard Academy for students, parents, and visitors. Barriers have also been created between neighborhoods and streets because of past development practices and the failure to provide connections or easements for those connections to be made in the future.

No regularly scheduled transit is currently available in Brevard, but it very well may be offered in the future, and sidewalk access would be a necessity. Rest areas with benches, bicycle racks, and waste receptacles could be provided along sidewalks in locations that could logically serve as transit stops in the future, with plans to add

shelters as transit is offered. Several areas that tend to lend themselves to this duty include the Wal-Mart Shopping Center, Transylvania Community Hospital, Blue Ridge Community College's Transylvania Campus, Chestnut Square, the K-Mart and Ingles Shopping Center, College Walk, Brevard College, Food Lion, the Transylvania County Courthouse, and the old Winn-Dixie site south of town. Increased pedestrian and transit access may also lead to economic redevelopment of areas like the former Winn-Dixie site and Chestnut Square.

• **Current Usage / User Demographics**

A Pedestrian Needs Assessment Survey was distributed through the Spring 2005 City of Brevard Newsletter. 167 surveys were returned by the middle of June 2005. Those surveys provide a good sample of pedestrian usage data from the residents of Brevard. An example survey form and the resulting data and analysis can be found in Appendix D.

Most of the pedestrians in Brevard walk for exercise (93%) or to run errands or shop (85%) using sidewalks, streets, or roads in their own neighborhoods (95%). They also use walking trails or paths (69%) and sidewalks, streets, or roads outside their neighborhoods (63%). The presence of fast traffic (52%) is the most popular reason for not walking more. Other popular reasons include too many cars (43%), unsafe street crossings (35%), no nearby sidewalks (35%), too busy (34%), and unsafe or poorly maintained sidewalks (29%). Users rated availability of sidewalks (92%), keeping a safe distance from traffic (92%), easy access to safe street crossings (89%), speed of traffic (87%), and reasonable distance to destinations (81%) as the five most important factors in their decisions to walk. The five least important factors to respondents were audio crossing signals (21%), lack of access to parking (25%), having other pedestrians nearby (31%), objects blocking passage of sidewalks (37%), and availability of benches (40%).

Nearly one-fourth (24%) of survey respondents have lived in Brevard for 20 years or more. In contrast, 28% have lived in Brevard for fewer than five years. 17% have lived in the area for 5-10 years, 17% for 10-15 years, and the remaining 14% have lived in the area for 15-20 years. 83% of respondents expect to be living in Brevard 10 years from now. 60% of respondents are female. Only 21% of respondents live with children under 18 years of age, and 31% of those children and youth walk to school. Of the 69% of youth and children who do not walk to school, nearly half (48%) would walk to school if pedestrian facilities were available. 43% of survey respondents are over the age of 65, due in part to the draw Brevard has on retirees.

Brevard is punctuated with mountains and rivers. Some areas are more difficult for pedestrians to navigate than others. Some areas are not currently served by pedestrian facilities due to their proximity to slopes and/or water. One area where this is the case is on Highway 276 / Greenville Highway west of Hap Simpson Park. On the north side of this two-lane road is a substantial slope, and on the south side of the road is a severe drop toward the French Broad River. Pedestrians cannot safely make the trip from downtown to the park because of this. There are other locations throughout the city where similar situations exist.

Like many other mountain cities in this state, Brevard is inhabited by residents with a long family history in the area and others who are relative newcomers. Both agree that pedestrian improvements are important to the area. Many of the survey respondents mentioned the walkability of the city as one of the reasons they moved to Brevard. At the same time, many of the people choosing to move to the area are of retirement age. Keeping this in mind, existing facilities will need to be upgraded so that these citizens can make use of them for years to come, and new facilities must be designed with them in mind.

• **Inventory and Assess Existing Facilities**

A map of existing facilities is shown in Section 8. Facilities range in condition from excellent to poor based on surface, width, traffic separation, vegetation, debris, obstructions, and ADA curb ramps. A spreadsheet of existing sidewalk facility conditions can be found in Appendix B. Standards were met when all of the facilities were built, but some of those standards have changed. Sidewalk segments that do not meet current standards need to be updated, but building sidewalks in some areas that currently have no pedestrian access is often a higher priority. All curb ramps must be updated to ADA standards so that facilities can be used by all pedestrians. Crosswalks at busy intersections that are not marked should be painted when pedestrian signals are installed.

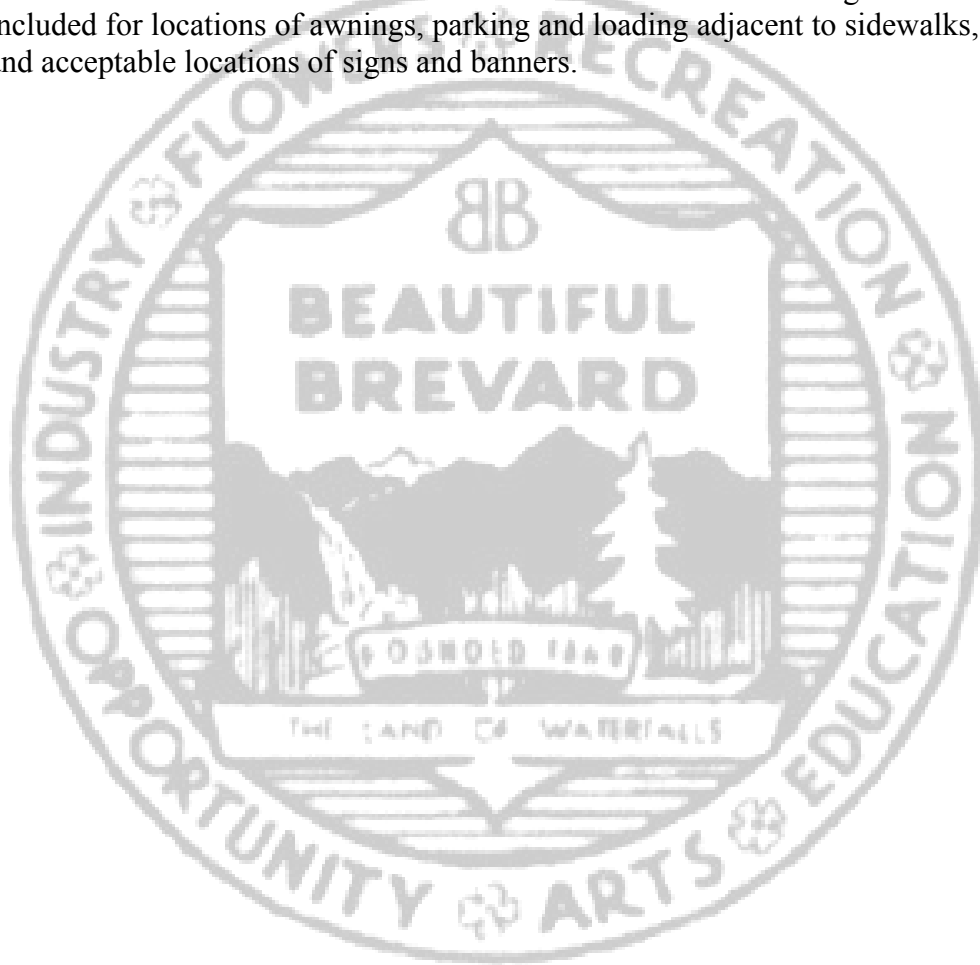
Sidewalks and paths throughout the city are used for both transportation and recreation. Trip origins and destinations are located in all of the far reaches of the city, and are not all connected at this time. Some areas are cut off by natural barriers including rivers and steep terrain. Neighborhoods that are proximal to one another are cut off because of cul-de-sacs, fences, and property lines. Some locations have sight obstructions due to overgrown vegetation and sloping mountainsides. High volumes of traffic at high speeds also limit the functionality of some facilities, such as the sidewalk on Broad Street. Missing sidewalk sections dump pedestrians into the travel lane, vulnerable to large trucks and other drivers.

• **Pedestrian Statutes and Ordinances**

Brevard City Council adopted a new Unified Development Ordinance (UDO) in April of 2006, concurrent with preparation of the 2006 City of Brevard Pedestrian Plan. A compiled list of existing sidewalk regulations can be found in Appendix E. Before the UDO was adopted, Brevard had regulations related to public improvements, definitions and minimum standards for construction, fire prevention and protection, health and sanitation (including regulations against spitting and littering), and miscellaneous provisions including regulations against loitering and possession or use of drugs or alcohol and prohibiting damage to streetlights, signs or other fixtures. Also included in the older city ordinance was a chapter devoted to peddlers and solicitors, including street or sidewalk vendors. Bikeways (multi-use paths) and street festivals were also regulated in the ordinance.

The UDO replaced the old zoning ordinance and indicates where construction of, dedication of right of way for, or payment of fees-in-lieu of sidewalks is required.

The UDO requires open space in new developments – greenways would fulfill this requirement. Residential developments are required to maintain a certain level of connectivity through the use of sidewalks and multi-use paths. New requirements are in place to insure that parking lots are safer for and that the buildings they serve are more accessible to pedestrians. Lighting standards have been updated to provide street and area lighting at a more pedestrian scale. Within the City Limits of Brevard, all new developments are required to provide pedestrian facilities and dedicate pedestrian easements to the City; within the ETJ, pedestrian facilities are optional while easements are required. Widths of pedestrian facilities based on zoning district are provided in the new UDO. The UDO also illustrates detailed street design and sidewalk cross-sections for various street classifications. Regulations are also included for locations of awnings, parking and loading adjacent to sidewalks, and size and acceptable locations of signs and banners.



SECTION 3: EXISTING PLANS, PROGRAMS AND POLICIES

• Review Relevant Local, Regional and State Plans and Guidelines

The North Carolina Board of Transportation adopted *Bicycling and Walking in North Carolina: A Long-Range Transportation Plan* in 1996. Bicycling is the primary focus of the document because the pedestrian aspect of the Office of Bicycle and Pedestrian Transportation was still relatively new. Bicyclists and pedestrians are spoken of mostly as one group, but their needs vary sharply.

At the turn of the 21st Century, municipalities and their citizens began to refocus their attention on walking and pedestrian facilities. Pedestrian plans and greenway plans have been drafted by many municipalities, but have not yet been drafted for the Land-of-Sky Regional Council or Transylvania County. The City of Brevard is planning to develop a Parks and Greenways Master Plan in the near future.

Transylvania County and the City of Brevard are each working on completing their CTPs with the help of the Land-of-Sky Regional Council / Rural Planning Organization (RPO) and the NCDOT. The County Thoroughfare Plan adopted in 1999 does not address pedestrians. Brevard's current thoroughfare plan does not address pedestrians, with the exception of how access control on thoroughfares benefits them. Maps in Brevard's Transportation Plan show off-road bicycle paths that are intended to be used by pedestrians as well. The Project Priority List from the City of Brevard CTP is located in Appendix F.

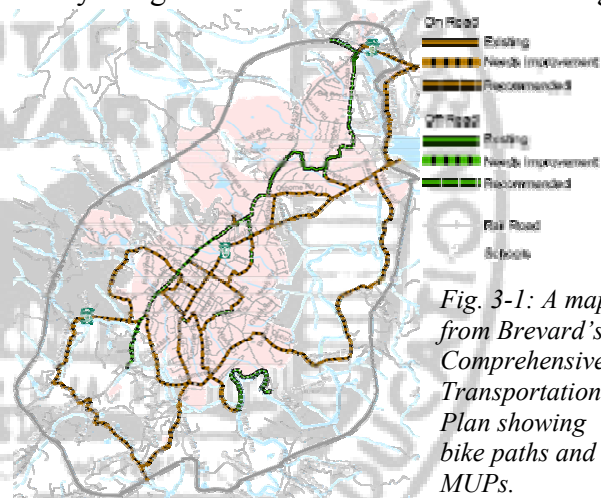


Fig. 3-1: A map from Brevard's Comprehensive Transportation Plan showing bike paths and MUPs.

A land use plan for the City of Brevard was adopted in 2002. Some of the goals listed in The City of Brevard Land Use Plan, as taken from the Focus 2020 Community Report, that directly relate to pedestrian travel are:

- 2.1 – Maintain and preserve local air quality;
- 3.6 – A safe and accessible downtown with improved pedestrian/vehicular circulation...;
- 5.1 – Provide a traffic circulation system for Brevard and surrounding environs that is safe and flexible and meets the needs of drivers, pedestrians, bicyclists, and skaters;
- 5.5 – Provide a network of paths and sidewalks that will enable safe pedestrian and bicycle access to downtown, schools, parks, shopping areas, recreational centers and residential neighborhoods; and
- 7 – Foster a culture of healthy lifestyle awareness for all citizens.

This Pedestrian Plan is intended to aid in the completion of these goals. The Land Use Plan also suggests using a land trust to facilitate acquisition of property/easements for multi-use paths. A new stand-alone Sidewalk Ordinance that set standards for design and construction was also called for in the Land Use Plan, and those design standards are covered in Chapter 5 of this plan. No example of roadway design plans is available at this time.

The City of Brevard implements a Capital Improvement Plan (CIP) that is updated annually. Sidewalk repairs and recommended installations for the fiscal year are listed. If a project is not completed during the fiscal year, it rolls over to the next CIP. There are several installations of new sidewalk segments that have been listed on the CIP for multiple years, and each of them is now being ranked as a very high priority.

Street design guidelines drawn up by the NCDOT for Subdivision Roads have been adopted by regional and local authorities. Sidewalk requirements based on roadway classification are included in the UDO, and as design guidelines illustrated in the UDO are more stringent than those suggested by the NCDOT, the guidelines in the UDO take precedence.

The Downtown Master Plan adopted in 2003 suggests “a safe and accessible downtown with improved pedestrian/vehicular circulation and effective graphics” (p.3). The streetscape improvements recommended throughout the Downtown Master Plan create a better environment for pedestrians, both aesthetically and from a safety aspect.

The Focus 2020 Community Report, as mentioned before, compiled the concerns, opinions, and interests of Brevard area residents and how they want to see the city improve by the year 2020. Initiated in 1997, Focus 2020 was intended to be a 20-year plan to facilitate improvement in those categories deemed most important by citizens: Economic Development; Environmental Quality; Gateways, Corridors, and Downtown; Cultural, Educational, and Recreational; Housing and Neighborhoods; and Public Safety and Health. Many of these are associated with pedestrian access and activity.

• Programs and Initiatives Currently Underway or Planned

Brevard has the benefit of several projects on the 2006-2012 TIP. The proposed US-64 bypass (R-2702, pedestrian facilities incidental) should provide access for both motorized and non-motorized transportation. One phase of the multi-use path from downtown to Pisgah National Forest (E-4143, independent pedestrian facility) has been completed and others were in the design phase at the time of this report. Several bridge replacements (B-4291 – bridge #193 over the Davidson River, B-4823 – bridge #12 over the East French Broad River, and B-4822 – bridge #13 over Tucker Creek; all pedestrian facilities incidental) are scheduled. When each of these bridges is replaced, they must be designed and built for use by both motorized and non-

motorized transportation modes. A copy of CIP projects currently suggested by the Land-of-Sky RPO for inclusion in future TIPs is located in Appendix G.

Presently, safety and education programs and resources for pedestrians are limited. The School Resource Officers in schools in Brevard have the task of conducting lessons on pedestrian safety, as well as bike and bus safety. Most of these lessons are being presented to elementary school children. The City has plans to work with the school system to promote walking to school.

In the past, the Brevard Police Department has tried to initiate programs for seniors regarding pedestrian safety, but due to low attendance, the program was discontinued. With sufficient interest, pedestrian safety programs could be held for any age group, as most people do not fully understand or recognize pedestrian related laws. The police force in Brevard relies on state statutes when a possible pedestrian violation is observed. Television commercials that inform drivers that failing to yield to pedestrians can result in receiving four points on their license, in turn causing higher auto insurance premiums, were shown in the Asheville area in late October 2005 to discourage “right on red” movements when a pedestrian is in or near a crosswalk.

Brevard and Transylvania County, in a joint effort, offer pedestrian commuters encouragement through the Employee Walking Club. The annual White Squirrel Festival, held over Memorial Day Weekend, is a major event in Brevard that gets people walking around the downtown area. The downtown business community, the Heart of Brevard, has put together a pamphlet showing the location of many downtown shops relative to the sidewalk system.

• **Policies and Institutional Framework**

No pedestrian plan can be implemented without funding. Several sources of funding are available for pedestrian facilities, from transportation-related to parks and recreation. Funding is also available at a variety of levels – federal, state, local, and private sources can all be tapped to complete the goals set forth in this plan.

Federal sources include money set aside by the *Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users Act* (SAFETEA-LU) and Recreational Trails Program. SAFETEA-LU has put an emphasis on traffic safety near elementary and middle schools by creating the Safe Routes to School (SRTS) program. State funding is available from the Parks and Recreation Trust Fund (PARTF) and through DOT improvements. DOT improvements are classified as either incidental – built as part of a new construction project or as a portion of scheduled highway projects – or independent, with an example being the first phase of Brevard’s multi-use path. Sidewalk repairs and new sidewalk installations within the city limits are funded primarily through the city’s Capital Improvement Program (CIP). Many aesthetic improvements can be financed by private groups and citizens, with an example being the purchase of benches, lamps, and waste receptacles for the downtown streetscape. Raising money through the sale of such items as engraved bricks has also proven helpful to many municipalities and groups in the past.

Another key to realizing this pedestrian plan is through mainstreaming pedestrian considerations. The UDO has many ordinances relating to sidewalks and other pedestrian facilities, ranging from construction standards to cleaning to the prohibition of blocking sidewalk passage. In addition to these ordinances, the new UDO includes street schematics illustrating sidewalk dimension requirements for various roadway classifications. By requiring sidewalks for new development and through the DOT's efforts, consideration for pedestrian facilities will become more commonplace. Developers are currently asked to construct sidewalks, grant easements for pedestrian facilities, and/or pay a fee-in-lieu of building facilities, usually at the estimated cost for the city to build the facility.

In addition to the UDO, the Land Use Plan and Downtown Master Plan call for improved facilities. The Land Use Plan mentions designing sidewalks and pathways for pedestrians, skaters, wheelchair users, and bicyclists and creating a network of multi-use paths and green spaces. An agreement with the county and surrounding towns to build an even larger network is also suggested. The Downtown Master Plan suggests traffic light coordination and an inventory of sidewalks so that prioritization of replacements and installation can be facilitated. The brick furniture zones provided in parts of downtown are functional as well as aesthetic, and are desired to be included in future sidewalk improvements in the downtown area.

In order to assure that facilities will be used when they are provided, it only makes sense to listen to the people who are most likely to use them. Pedestrians need to know who to voice concerns and suggestions to. The City of Brevard has had, since October of 1996, a Recreation Advisory Commission appointed by City Council. The Pedestrian/Bicycle Pathways Study Committee has served as the Project Steering Committee (PSC) for this Pedestrian Plan. The members of the PSC and the Planning Director of Brevard are committed to keeping pedestrian needs in the spotlight.

Many agencies have a vested interest in walking, pedestrians, and pedestrian facilities. Transylvania Community Hospital and the County Health Department are interested in getting people walking for exercise and better health, while at the same time keeping them safe from traffic and other causes of serious physical injury. Transylvania County Parks and Recreation is also interested in getting people active. The Transylvania County School System wants to ensure safe walking routes to school for their students. Downtown businesses that are part of the Heart of Brevard appreciate walking tourists. All of these entities are working and must continue to work together to guarantee a more complete network of pedestrian facilities.

SECTION 4: DEVELOP PEDESTRIAN SYSTEM PLAN

• System Overview

Currently, the pedestrian system in Brevard is good downtown, but worsens with distance from that area. Exceptions occur in areas that have been developed fairly recently, under more stringent pedestrian facility requirements. In the future, it is anticipated that a full network of multi-use paths and sidewalks will connect all of Brevard and will extend into the surrounding areas, providing opportunities for inter-municipal and countywide connections.

• Corridor Identification

Every roadway within Brevard's planning jurisdiction should be served with some sort of pedestrian facility, whether sidewalks, paved multi-use paths, or unpaved but maintained trails. There are several pedestrian generators and attractors in and around Brevard, with one unique attractor being the Pisgah National Forest. A multi-use path is being planned that will connect downtown Brevard to the National Forest. Other generators and attractors include schools and colleges, residential areas, parks and recreational areas, businesses, retail areas, and the hospital. Each map in Section 8 illustrates select pedestrian generators and attractors. These areas are high priorities for construction of new and repair of older pedestrian facilities.

• Identify Opportunities

Brevard has been devoted to increasing the number of pedestrian facilities around the city. New sidewalk sections are being constructed on a regular basis, and others are being upgraded. Phases 2 and 3 of the Brevard Multi-use Path are in the design stage and construction is expected to begin in late 2006. In the near future, the NCDOT will be adding a turn lane on Gallimore Road at its intersection with Greenville Highway. Since Brevard holds an easement in front of the Boy's and Girl's Club and a multi-use path on Gallimore Road is planned, this is an opportunity to begin construction. When new development or redevelopment needs planning approval in the area, the opportunity for pedestrian facility construction and easement dedication is afforded, as such conditions may be placed on plan approval. For example, when redevelopment is considered in such high impact areas as Ecusta Road, around Transylvania Community Hospital, Chestnut Square, and Brevard College, sidewalks and easements should be sought. The city's ownership of the Bracken's Creek watershed and intentions to establish a park there generates an additional need for a multi-use path connecting downtown to the west side of town. The existing railway is expected to be used for transit to connect Brevard, Transylvania County, Hendersonville, Henderson County and beyond with positive economic impacts for the area, but if that does not transpire as anticipated, the opportunity should be taken to create long distance trails.

Concurrent with the NCDOT's installation of new synchronized traffic signals and pedestrian signal heads, crosswalk striping will be put in place by the DOT, as well as

signage reminding drivers to yield to pedestrians. Also, in conjunction with the Downtown Comprehensive Signage Program, pedestrian signage could be established that lets people know where other pedestrian facilities and places of interest are that they may or may not know exist.

While new construction is necessary to expand the network of pedestrian facilities in and around Brevard, upgrades and maintenance of existing facilities is also important to increase the service life of the existing system. Many of the curb ramps in and around town are not ADA compliant. Some sidewalks are partially obstructed and are difficult or impossible for the disabled to maneuver. These curb ramps and sidewalks must be upgraded or replaced so that they can be used by everyone. Truncated domes, for instance, must be placed on all curb ramps that abut a street, and a one-quarter inch lip or less must be provided. Where sidewalks are partially obstructed, a four-foot (4') minimum clear space is recommended, and three-feet (3') of clear space are required, for wheelchair clearance.



Fig. 4-1: Truncated domes help the visually impaired determine where the sidewalk meets the street.

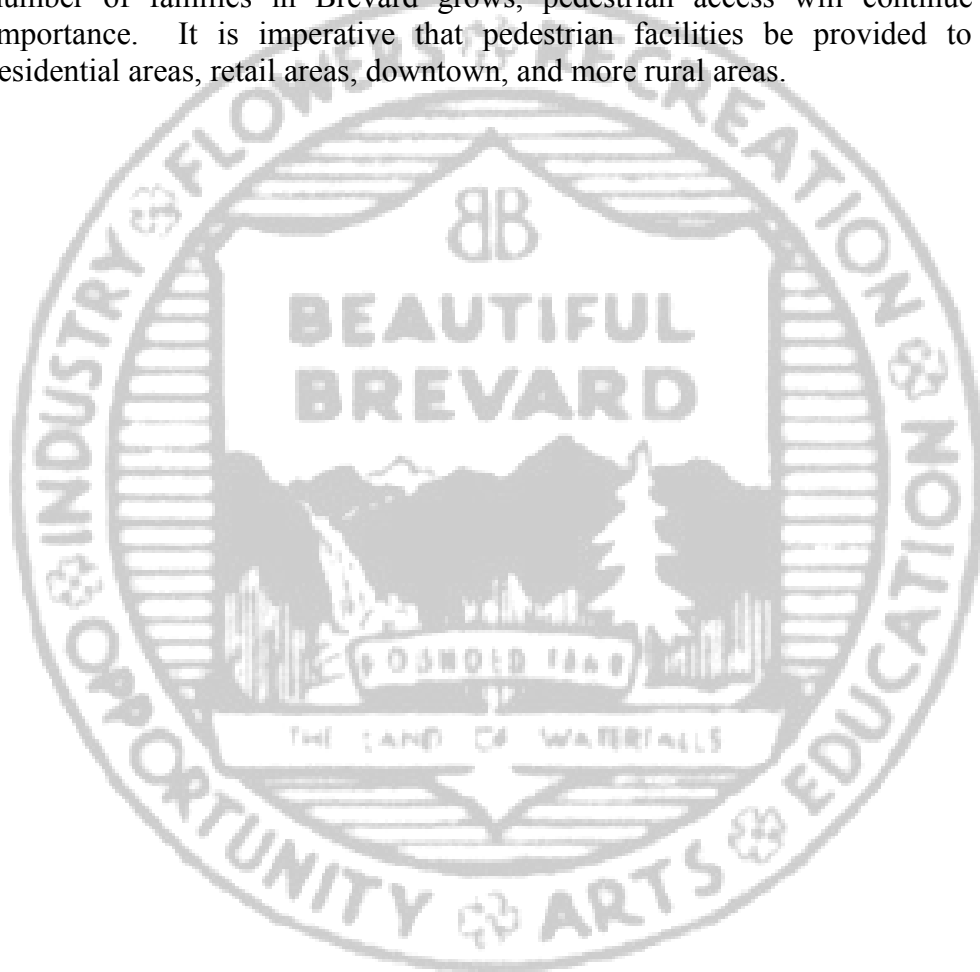
Regular maintenance extends the life and preserves the functionality of existing facilities. Property owners who have sidewalks on their property should be encouraged to help maintain the system. By trimming bushes and grass or weeds that encroach upon, sweeping or blowing leaves and debris from, and checking the general condition of their section of sidewalk, they prolong the service life of the facility. In addition, they free up the Public Works Department to repair other facilities or construct new segments. When possible, sidewalk improvements could be scheduled to be done in concurrence with utility repairs when the sidewalk is damaged.

Public rights-of-way and utility easements can be used for connections in the pedestrian network. The Southern Railway property that runs through town allows for multi-use paths to connect distant destinations. Even in areas where the train still runs, adjacent property owners may be willing to grant easements for multi-use paths to be constructed, creating a buffer between the railway and their home or business. Greenways may also be established along these routes to provide a natural buffer of trees and other greenery on both sides of a path. Sewer easements could also be used for trails and paths.

In future, pedestrian facilities may be included in projects that are listed in the TIP or Land-of-Sky Priority Needs List (PNL). In the more distant future, pedestrian facilities may also be included as stand-alone projects. The construction of the proposed West Loop, which is planned to follow the path of and displace a portion of Phase 3 of the Brevard Multi-use Path, should include an MUP along one side or sidewalks and bike lanes on both sides. While acquiring easements for the widening of Caldwell Street to three lanes, sidewalk construction on both sides should be considered and designed.

- **Special Focus Areas**

Senior citizens, retirees, tourists, and families with children are all drawn to Brevard, many by the beautiful scenery afforded by the mountains. The proximity of Brevard to the Pisgah National Forest, the Davidson River, and the French Broad River offer opportunities not shared by other locations. These natural attractions need to be accessible to pedestrians in addition to having vehicular access. Brevard College, the Brevard Music Center, and Brevard Academy draw in another group of people, many of whom do not have access to a car. They need to be able to travel throughout Brevard safely as well. As the population of senior citizens, Brevard College, and the number of families in Brevard grows, pedestrian access will continue to gain importance. It is imperative that pedestrian facilities be provided to connect residential areas, retail areas, downtown, and more rural areas.



SECTION 5: FACILITY STANDARDS AND GUIDELINES

• General Pedestrian Planning and Design Guidelines

The Pedestrian Planning process began long before this document was drafted. One of the earliest steps was the formation of the Pedestrian/Bicycle Pathways Study Committee in 1996, whose goal was to develop a plan to provide a network of pedestrian and bicycle facilities adequate to provide safe, accessible, non-motorized transportation links to schools, parks and recreation centers, public facilities, commercial areas and residential neighborhoods.



Fig. 5-1: Public Input was a key to producing the Brevard Pedestrian Plan.

When City Council decided to apply for an NCDOT Pedestrian Planning Grant in February 2004, members of the Pedestrian/Bicycle Pathways Study Committee were chosen to run the Project Steering Committee. Other documents and plans that have helped to lay the groundwork for this Pedestrian Plan include the Focus 2020 Report, City of Brevard Land Use Plan revision, and Downtown Master Plan. Adopted in March 2003, the Focus 2020 Report emphasized issues such as pedestrian safety and access and the coordination of modalities throughout the city. The Land Use Plan called for pedestrian/bicycle pathways to link as many key sites as possible in the urban area while allowing for safe transportation for all and was adopted in August 2002. The Downtown Master Plan was adopted in May 2003 and called for a full sidewalk inventory, noting that sidewalks were in good condition in the downtown area, but were progressively worse outside of that area.

A wide variety of people are pedestrians, and a successful network of facilities will be accessible to all. Universal design incorporating ADA compliance will insure facilities are designed for wheelchair accessibility and for use by the blind, seniors, children, and parents with young children, to name a few. Curb ramps in particular must allow for a fairly smooth transition between sidewalk and roadway for wheelchair and walker users, but must also provide some signal to the blind that a transition is being made. Several curb ramps throughout the city need to be upgraded or retrofitted to meet these needs. To prevent heat-related illnesses, to which young children and senior citizens are most susceptible, benches and shady areas should be provided to allow a place to rest and cool down. Local officials have adopted the design guidelines provided by the state, such as roadway cross-sections and sidewalk design criteria. Federal, state, and local guidelines all require ADA compliance.

• Typical Cross-Sections and Pedestrian Design Considerations

The Brevard Bikeway / Multi-Use Path is the first pedestrian connector to attempt to connect Downtown Brevard and the Pisgah National Forest. Phases 1 and 3 of this off-road facility incorporate sections that are adjacent to the roadway and other sections that are located on separate rights-of-way. Cross-sections designed for these phases of the Brevard Multi-Use Path will dictate the standard for future similar facilities.

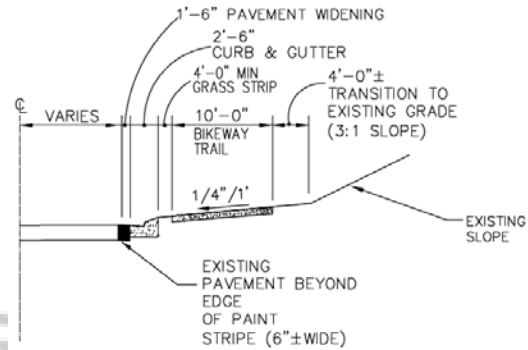


Fig. 5-2: Cross-section of a portion of Phase 1 of the Brevard MUP.

The UDO includes street schematics that illustrate cross-sectional dimensions of the various roadway classifications. Sidewalks are shown to range from 5'-wide to 8'-wide. Almost all of them are shown with a 6' planting strip serving as a buffer between the travel lanes and the sidewalk. Illustrations can be seen in Appendix E.

Intersections are dangerous places for pedestrians regardless of treatment, but steps can be taken to reduce the risk to people crossing the roadway. Pedestrians must be visible and expected in the area. Lighting is an important consideration in making pedestrians more visible, and therefore safer, at night. Crosswalks are intended to raise drivers' expectation of pedestrians. In the downtown area, crosswalks have been painted with diagonal and transverse lines, so this pattern should be carried on throughout the downtown area to provide continuity. Outside of downtown, longitudinal markings may be used instead. Intersections with large volumes of pedestrian and vehicular traffic should be considered for pedestrian activated signals, or at least have pedestrian intervals scheduled into the signal timing.

Mid-block crossings should not surprise drivers. They should be signed, painted, and/or called out in some other way. Bulb-outs can be used in conjunction with parallel parking to allow drivers to see pedestrians past parked cars much sooner than they would be able to without bulb-outs. Median refuges should be created where mid-block crossings are permitted to allow safer crossings of multiple lanes of traffic. For example, if access management is desired on Broad Street (US 64/US 276), replacement of the two-way left-turn lane with a median and left-turn lanes is recommended. U-turns could be allowed at signalized intersections, and pedestrian crossing intervals would need to take that into account. Landscaped islands serve as streetscape improvements and provide safer median refuges.

Roundabouts are an effective way to increase safety at intersections (Fig. 5-3)². The locations where a pedestrian and vehicle might cross are called pedestrian/vehicular conflict points and are decreased in number from 16 to 8 for a 4-leg intersection with one lane traveling in each direction (Fig. 5-4)². Splitter islands at each approach to

² Roundabouts: An Informational Guide. June 2000. Federal Highway Administration. 09 February 2005 <<http://www.fhrc.gov/safety/00068.htm>>.

the roundabout provide pedestrians a haven between travel lanes, allowing a pedestrian to cross one direction of traffic and then the other. The central island is not for use by pedestrians. Brevard currently has no roundabouts, but if installation occurs in the future, crosswalks would need to be provided 20' back from the yield line, with any on-street parking prohibited within 20' of the crosswalk. Drivers and pedestrians would need to be educated on how to properly navigate a roundabout.

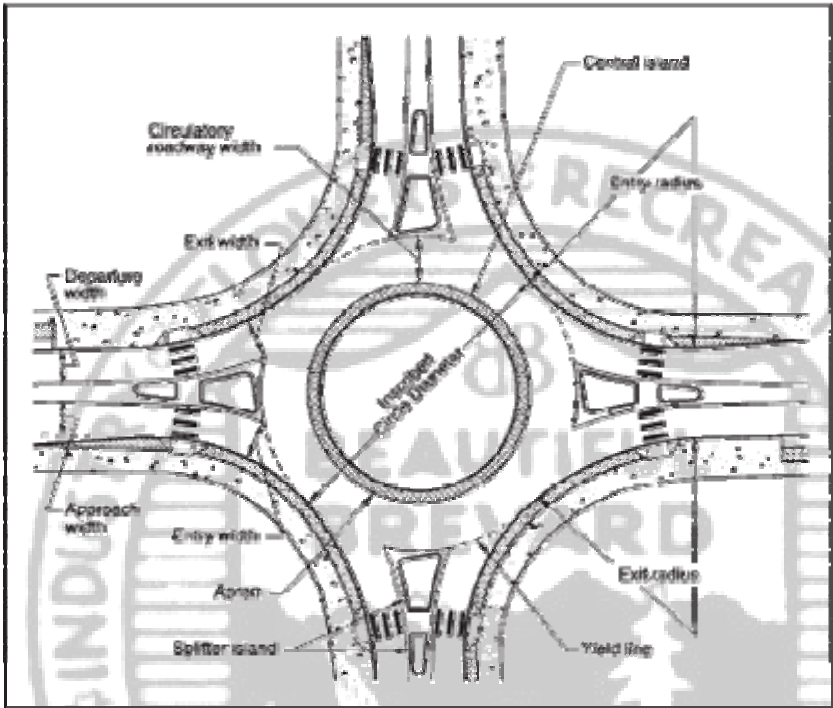
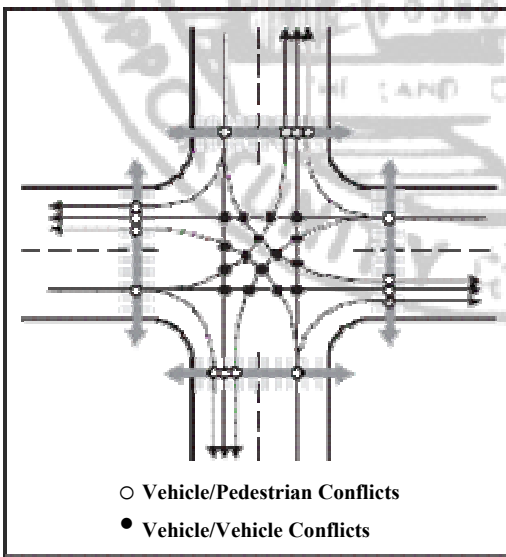
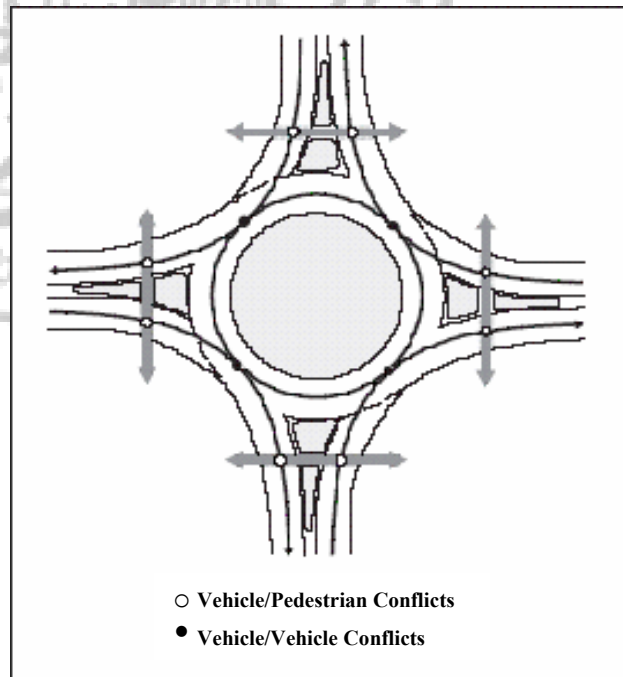


Fig. 5-3: Basic geometry of a roundabout.



4-Leg Intersection

Fig. 5-4: Possible Vehicle/Pedestrian Conflicts (white circles) are decreased by 50% in the roundabout.



4-Leg Roundabout

Regardless of treatment, all pedestrian facilities should at minimum meet NCDOT Design Standards. NCDOT's Design Standards can be found online at http://www.ncdot.org/doh/construction/ps/std_draw/06english/default.html. Signage should meet the warrants set forth in the FHWA's "Manual on Uniform Traffic Control Devices" (MUTCD). The manual is available in both PDF and HTML format online at <http://mutcd.fhwa.dot.gov/kno-2003r1.htm>.

• **Sample Cost Estimates**

Costs of construction materials change almost daily. The price estimates below (Fig. 5-5, 5-6, and 5-7) are provided only for reference.

Handicap Accessible Curb Ramp:	<u>\$1000 / EA</u>
Crosswalk with Paint Striping:	<u>\$5 / LF</u>
Speed Hump / Imprinted Crosswalk:	<u>\$45 / SY</u>
Parking Bumper:	<u>\$100 / EA</u>
Signs:	<u>\$500 / EA</u>
Grading:	<u>\$4 / CY</u>
Wooden Fence:	<u>\$8 / LF</u>
Retaining Wall:	<u>\$125 / LF</u>
Adjust Top of Existing Utility Manhole:	<u>\$500 / EA</u>
Pavement Repair:	<u>\$5 / SY</u>

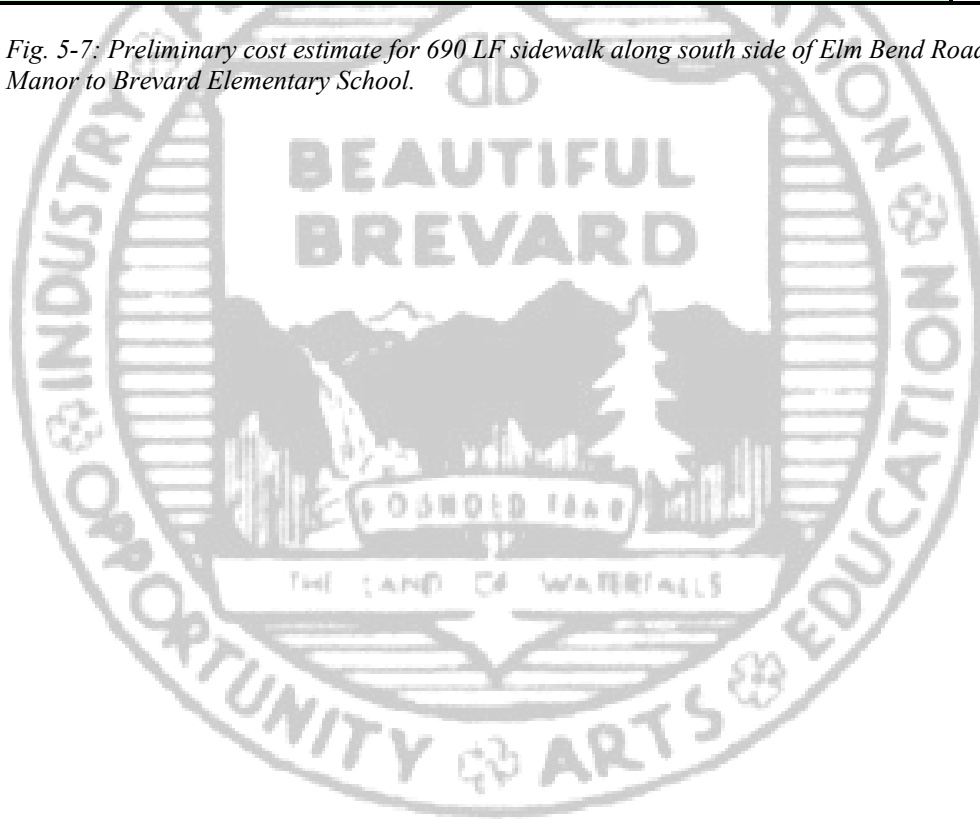
Fig. 5-5: Estimated prices of pedestrian facility appurtenances.

No.	DESCRIPTION	QTY.	UNIT	UNIT PRICE	EXTENSION
1	10'-0" wide Asphalt Multi-Use Path, including stone base	5500	LF	\$ 20	\$ 110,000
2	Painted Crosswalk, including signage	8	EA	\$ 5,000	\$ 40,000
3	Grading	1	LS	\$ 20,000	\$ 20,000
4	Culvert Extension	2	EA	\$ 10,000	\$ 20,000
5	Utility Relocation/Adjustment	1	LS	\$ 40,000	\$ 40,000
6	Wood Fence	1500	LF	\$ 8	\$ 12,000
7	2'-6" Concrete Curb & Gutter Along Road	4000	LF	\$ 25	\$ 100,000
8	18" Reinforced Concrete Pipe	2000	LF	\$ 30	\$ 60,000
9	Catch Basin	10	EA	\$ 2,000	\$ 20,000
SUBTOTAL CONSTRUCTION COST					\$ 422,000
Engineering, Surveying, Administrative (15%)					\$ 63,300
Contingencies (10%)					\$ 42,200
TOTAL COST ESTIMATE					\$ 527,500

Fig. 5-6: Preliminary cost estimate for 5500 LF multi-use path along south side of Gallimore Road, from Brevard Elementary School to Brevard High School.

No.	DESCRIPTION	QTY.	UNIT	UNIT PRICE	EXTENSION
1	5'-0" wide Concrete Sidewalk, including stone base	690	LF	\$ 30	\$ 20,700
2	Painted Crosswalk, including signage	1	EA	\$ 2,000	\$ 2,000
3	Grading	1	LS	\$ 3,000	\$ 3,000
4	2'-6" Concrete Curb & Gutter Along Road	690	LF	\$ 25	\$ 17,250
SUBTOTAL CONSTRUCTION COST					\$ 42,950
Engineering, Surveying, Administrative (15%)					\$ 6,443
Contingencies (10%)					\$ 4,295
TOTAL COST ESTIMATE					\$ 53,688

Fig. 5-7: Preliminary cost estimate for 690 LF sidewalk along south side of Elm Bend Road, from Sapphire Manor to Brevard Elementary School.



SECTION 6: ANCILLARY FACILITIES AND PROGRAMS

• Introduction

Additional sidewalks and multi-use paths are typically what are thought of when pedestrian improvements are discussed. Brevard can also be improved for use by pedestrians through spot improvement and maintenance programs, traffic calming initiatives, transit interface planning, safety education programs, enforcement programs, and encouragement and promotion. Examples of each are outlined below.

• Spot Improvement Programs

Certain tasks can be taken care of immediately to improve the condition of the pedestrian facility network. These spot improvements can be done without involving a lot of capital or labor at once. Removing and replacing severely broken sections of the network are tasks that will require more time and money than some other surface improvements, such as removing fallen leaves and debris from the sidewalks and edging grass and weeds along the network, but each task will do much to enhance the system. Pedestrian signals should be provided in high traffic areas, and existing signals throughout the planning area should be checked periodically to insure that all are functioning properly. Lighting at intersections and along pedestrian facilities should also be installed where needed and checked and replaced if necessary. Crosswalks that need painting or repainting could be done fairly quickly and will provide residents with visual evidence that pedestrian concerns are being addressed. Simultaneously, curb cuts could be checked for ADA compliance and retrofitted or replaced if necessary. Pedestrian-oriented signage should be installed to allow for greater use of the system. Overhanging branches and overgrown shrubs should be trimmed along the network to increase visibility of and for pedestrians. Where culvert crossings or bridges are too narrow to allow pedestrians to cross without entering the traveled way, culverts should be extended, shoulders should be widened, or separate pedestrian bridges should be constructed.

• Maintenance Programs

Maintenance is required to keep any system operating, including a network of pedestrian facilities. The spot improvements listed above could easily be converted into a maintenance program. Each of the tasks – removal and replacement of broken sections, removal of leaves and other debris, edging and trimming, signal and lighting inspections and replacements, crosswalk painting, and curb cut retrofitting – should be put into a scheduled rotation and kept up. Maintenance of the multi-use paths in the area also needs to be taken into account when the annual budget is drafted.

• Traffic Calming Initiatives

By redirecting some vehicular traffic and reducing the speed of remaining traffic, pedestrian safety is increased. Many of the improvements installed as traffic calming measures also assist pedestrians in more direct ways. A few examples are speed

humps / raised crosswalks and bulb-outs. Raised crosswalks and bulb-outs both increase driver expectation of pedestrians while making them more visible.



Fig. 6-1: Bulb-outs (left) and raised crosswalks (right) can be used together or separately to increase visibility of pedestrians to drivers.



• Transit Interface

Though transit in Brevard is limited now, it is smart to design for the future. As Brevard continues to grow, the demand for transit will likely increase, and the transit interface will need to be accessible to and accommodate pedestrians who are passing by, boarding, or exiting transit. Sidewalks will need to be provided to transit stops and be widened to provide a pad 8'-wide by 5'-long to front and rear bus doors to accommodate all of these uses at once³. Benches, bicycle racks, and shelters are welcome additions to any transit stop, and waste receptacles allow users to keep the area clean and inviting, but all of these must be placed so as not to block pedestrian flow.

• Safety Education Programs



Fig. 6-2: Schools and pedestrian safety should go hand-in-hand.

It is important to provide safety education to drivers and pedestrians, as they both have responsibilities when they encounter each other. School-based programs would train a wider group of impressionable minds than any other type of program, and the City of Brevard should work with Transylvania County Schools to promote pedestrian safety. The School Resource Officer or another officer could put on an informative and entertaining program for elementary school students. A more involved agenda could be scheduled for middle school students, while an increased focus on how to react to pedestrians should be incorporated into the Driver's Education courses

offered in high school. Instruction for elementary and middle school students could be given in conjunction with physical education, since this could result in kids being more apt to walking reasonable distances and being in better shape, or as a separate curriculum. Safety education programs for elementary and middle school students could be funded in part or in whole by the Safe Routes to School Program (SRTS).

Repetition of a message increases the chance that it will be remembered. Other opportunities for teaching pedestrians and drivers should be sought out and utilized.

³ American Association of State Highway and Transportation Officials. *Guide for the Planning, Design, and Operation of Pedestrian Facilities*. Washington, DC: American Association of State Highway and Transportation Officials, 2004.

The Boys and Girls Club is another venue that could be used to educate children on how to be safe pedestrians. Senior centers and communities provide the opportunity and location for programs directed more toward seniors. Young and middle-age adults also need to be reminded of or taught about pedestrian laws. Television, radio, newspaper, and Brevard's quarterly newsletter could each provide that information.

• **Enforcement Programs**

Traffic laws are more likely to be obeyed if they are enforced. Some people do not realize certain actions are against the law because they have never heard of someone being "caught in the act." Even enforcement of an area for one day will make people who drive or walk in that area remember and pay more attention to their actions. This selective concern, though, is also why enforcement needs to be spread throughout the city.

Outreach programs also increase the chances that people obey the laws. When someone feels like they have a positive personal relationship with a law enforcement official, they tend to show more respect, in turn driving or walking more in accordance with the law. These outreach programs also afford law enforcement officials the opportunity to educate the community about laws and safety. Officers could have a booth at the White Squirrel Festival or at other community festivals and gatherings. A Safety Fair could be held at the local elementary schools. Middle school and high school students could receive materials that were more directed at laws and ordinances and the consequences associated with failing to comply with each. SRTS funds could be used to fund enforcement programs for elementary and middle school students.

• **Encouragement and Promotion**

Many would-be pedestrians need some incentive to get them moving. Others are unaware of their options. Incentive programs could be sponsored by the Transylvania County Health Department, Transylvania County Schools, Transylvania County Recreation Department, and/or Heart of Brevard. Rewards could be offered to participants who walked and recorded their times and distances for trips, in addition to the health benefits of exercise received as a result of those trips. Other agencies would be welcome to join the effort. Employers should be encouraged to offer some benefit to employees who walk instead of drive to work. The same groups could put together informational brochures and display them in racks or displays near pedestrian generators.

Events should also be held that would increase awareness and draw more attention to pedestrian needs and incentives. Some cities have week-long programs that encourage people not to drive, and a similar program could be initiated in Brevard. "Walk to School Day" is another approach that can be taken and involves younger pedestrians. Funds set aside for SRTS could be used to finance encouragement and promotion programs for middle and elementary school students. Display booths could also be set up at existing events, such as the White Squirrel Festival.

SECTION 7: PROJECT DEVELOPMENT

• Introduction

Expansion of the pedestrian network in and around Brevard must take place project by project. Potential projects and preferred treatments must first be identified, and then prioritized. Funding sources and opportunities should then be reviewed, special funding opportunities for high priority projects and supporting policies and guidelines identified, and monitoring processes developed to insure successful growth.

• Identify Potential Projects and Preferred Treatments

There are several potential projects throughout the city, and many options for construction or upgrade. Many of these are discussed more in this section. On-road construction projects include those within the street right-of-way. Concrete sidewalks provided for pedestrian use should be wide enough to allow two pedestrians to comfortably pass each other and should be far enough from vehicular traffic so that pedestrians feel secure. Dedicated bicycle lanes can be provided where there is no other form of separation between the pedestrian facility and the travel lane. Planting strips of any width greater than two-feet are preferred. At roadway intersections where pedestrians may not be expected or where there are a large number of users, painted crosswalks that are visible to drivers should be used. Pedestrian activated signal heads should also be provided at intersections where there is a large number of pedestrian users, special user groups, and/or a large volume of vehicular traffic. Culvert crossings and bridges should be wide enough to provide room for pedestrians and vehicles to cross without conflict or a feeling of insecurity. Signage may be required in some locations to inform drivers of the presence of pedestrians and the driver's responsibility to yield. Portable flashing beacons that may be moved from one location to another to call special attention to certain locations could be used for short periods of time. A yearly inventory of the sidewalk system will allow maintenance to be scheduled and completed in a timely and efficient manner.



Fig. 7-1: Signs like the one above have become popular for reminding drivers of the pedestrians present.



Fig. 7-2: A section of the completed first phase of Brevard's MUP System.

Off-road construction projects are those which are not limited to the street right-of-way. 10-foot wide, asphalt or concrete paved multi-use pathways are the preferred off-road pedestrian alternative in Brevard and also serve bicyclists. Greenways are defined as corridors of undeveloped land reserved for transportation and recreational use or environmental preservation, and can be used to enhance multi-use paths. The terms “greenway”, “bikeway”, and “multi-use path”

are often used interchangeably. Rail trails are the portions of multi-use paths that follow abandoned railroad beds and are already at acceptable grades, aiding in the design process and lowering the cost to build facilities. Paths and trails can be used to connect neighborhoods throughout Brevard and increase the pedestrian network where sidewalks are not feasible. These could be crushed limestone, stone dust, or some other surface and would not necessarily be ADA compliant, but would provide the groundwork for possible future projects. Maintenance of these trails would be crucial. Signage of pedestrian facilities is also necessary, and standards have been set forth by the NCDOT. A link to their website can be found in Section 5 of this document.

In addition to the construction projects that are called for throughout this document, there are many supplementary facilities and programs that will improve the pedestrian network. Mapping and signing, for instance, will aid pedestrians in their exploration of the area. Once Phase 2 of the Brevard Multi-use Path is completed, mileage markers can be placed along the trail to let users know how far they are from the Pisgah National Forest and where they are in the system. City-wide pedestrian maps should be created and made available to the public through the city website, at the Chamber of Commerce, and other areas. Rest facilities, overlooks, and benches would be welcome additions to the multi-use path system. Pedestrian safety education could be provided to many different age groups by the Brevard Police Department. Pedestrian-related laws should also be enforced around the city to increase the chances that pedestrians and drivers will respect those laws. A collection of pedestrian-related laws can be found on the NCDOT website at <http://www.ncdot.org/transit/bicycle/laws/resources/lawsguidebook.html>. To increase the number of pedestrians who use the updated facilities, the City and County should continue to encourage the Employee Walking Club. The “100 Miles in 100 Days” program sponsored by the Blue Cross and Blue Shield Insurance Company is also an effective way to encourage people to walk more. The City should also work with Transylvania Community Hospital and the Transylvania County Health Department to continue to promote the myriad of health benefits of walking. The annual White Squirrel Festival held in Downtown Brevard provides a unique venue to promote many of the aforementioned strategies. Up-to-date maps of the pedestrian system could be displayed and distributed, as could safety information. Pedestrian transportation could be promoted and encouraged, and suggestions for improvements could be solicited. In the future, if transit becomes available in Brevard, connectivity to pedestrian facilities will have to be ensured.

• **Prioritize Projects**

Appendix H lists existing pedestrian facility lengths, while the following tables list proposed pedestrian facilities by type and priority. Proposed projects do not have to be built in the order shown. If an opportunity to construct presents itself, the opportunity should be taken. Each project has been deemed either a long range, a mid-range, or a short-term improvement. Short-term improvements are scheduled to be completed in five years, mid-range projects in five to ten years, and long range projects will likely take more than ten years. At this time, all immediate spot improvements are maintenance related, with the exception of an improved pedestrian

crossing at Gallimore Road and US 276/Greenville Highway. This intersection connects the Boys’ and Girls’ Club and Brevard Elementary School across US 276. A road-widening project is being planned, including turn lanes on Gallimore Road and Greenville Highway. A safe pedestrian crossing needs to be installed when these projects are constructed.

On-road construction projects consist primarily of sidewalks. Minor sidewalk projects include any construction of less than 1500 linear feet, and major sidewalk projects are 1500 linear feet or greater. Short-term projects can be seen in the table below. There are many mid- and long range improvements planned, which are shown on Map 8-4. It is intended that all of the streets shown on Map 8-4 as “unprioritized” will have sidewalks built in the future. As opportunities for funding or constructing projects present themselves, such projects should be undertaken.

Sidewalk	Minor / Major	Short-term, Mid-range
Elm Bend Road	Minor	Short-term
Probart Street	Major	Short-term
Neely Road, East French Broad Street to Chestnut Street	Major	Short-term
Morris Road	Minor	Short-term
McLean Road	Minor	Short-term
French Broad Street (south)	Minor	Short-term
Hospital Road / Temple Church Road	Minor	Short-term
Johnson Street	Minor	Short-term
Deer Lake Road	Minor	Short-term
Greenville Highway	Minor	Short-term

Fig. 7-3: Top ten priority locations for new sidewalk construction.

Multi-use paths account for the majority of off-road pedestrian facilities. High priority short-term projects include Phases 2 and 3 of the Brevard Multi-use Path, a facility on Gallimore Road, and construction along Rail Road Avenue. Mid-range projects connect downtown to English Hills, provide a connection from English Hills to Rosman Highway, connect Rosman Highway to BHS, provide an off-road connection between downtown and the Brevard Music Center, follow Asheville Highway from Hospital Road to the Pisgah National Forest, and follow Old Hendersonville Highway from Chestnut Street to Ecusta Road. Other unprioritized MUP projects have been suggested to complete a large loop and connect beyond Brevard and are shown on Map 8-4.

Multi-Use Path		Short-term, Mid-range
US 64/276/280, off-road, Poplar Street, Fisher Road, off-road (Bikeway – Phase 3)	Allison Road to McLean Road	Short-term

Ecusta Road, off-road (Bikeway – Phase 2)	Sports Complex to Pisgah National Forest	Short-term
Gallimore Road	US 276/Greenville Highway to Country Club Road	Short-term
Asheville Highway	Hospital Drive to Ecusta Road	Short-term
Railroad Avenue, off-road	McLean Road to West Main Street	Short-term
West Main Street, off-road	South Oaklawn Avenue to English Hills/WCCA/Head Start Facility	Mid-range
Off-road, Picklesimer Avenue, off-road, McMinn Avenue, off-road	English Hills/WCCA/Head Start Facility to Rosman Highway	Mid-range
Rosman Highway, Forest Hill Drive, off-road, Country Club Road	Rosman Highway to Country Club Road/High School	Mid-range
Off-road, Holcombe Road, Probart Street, Music Camp Road, Pinnacle Road	West Main Street to Bracken Mountain Boundary/Brevard Music Center	Mid-range
Old Hendersonville Highway	Chestnut Street to Ecusta Road	Long range

Fig. 7-4: Top ten priority locations for new multi-use path construction.

The Public Works Department is responsible for the maintenance of existing sidewalks throughout the City of Brevard. Improvement of city sidewalks that have been rated as fair or poor is a high priority. Sidewalk repair priorities are listed below, and are typically ranked for completion with regard to their seriousness. Public works should coordinate these improvements with construction of new sidewalks so that benefits are distributed fairly evenly throughout the city when possible. If funding allows, at least 2500 LF of sidewalk should be repaired each year.

Sidewalk Repair Priority List		
Sidewalk Location		Surface Area (square feet)
Whitmire Street	#325	30
	#315	60
	#307	60
	#303	120
	#312	30
	#326	60
	Brevard Moving & Storage	240
South Caldwell Street	#201	120
	#240 - #242	360
	#248	212
	#316 - #318	180
	#322	60

	Rear of McCosh's #412 - #414 #420 #502 – Jamar Plaza Funeral Home	60 212 90 300 125
West Main Street	#220	120
King Street	Brevard Lumber Company	1,600
South Gaston Street	County Sheriff's Department	240
Maple Street	#206 #124 #262 #268 #270 #278 #254	30 30 30 270 30 30 30
East Main Street	#539 No Number #542 #555	120 90 120 90
Probart Street	By K&M Auto #216	180 50
West Morgan Street	Duckworth Avenue to BHA	768
Greenville Highway	Near Park View Drive	120
West Jordan Street	#239	60
Carver Street	From Cashiers Valley to Oaklawn Avenue	4,635

Fig. 7-5: Top priority locations for sidewalk repairs.

• Review Funding Opportunities

Several funding sources at the federal, state, and local levels are available to aid in the implementation of this plan. Possible sources and applications are listed below.

Federal Funding

- Funds allocated to the Surface Transportation Program (STP) can be used to construct pedestrian walkways or to create maps and brochures or public service announcements (PSAs) promoting safe walking. Public sidewalks can also be retrofitted to comply with ADA with these funds. *(Use as listed above)*
- Each state must earmark 10% of their annual STP funds for Transportation Enhancement Activities (TEAs). Conversion of abandoned railway corridors to multi-use paths can be paid for with these funds, as can the construction of pedestrian facilities or safety- and educational-related activities for pedestrians. *(Use as listed above)*

- A separate 10% of STP funds is allocated for the Hazard Elimination and Railway-Highway Crossing program to address pedestrian and bicycle safety issues. Publicly owned pedestrian and/or bicycle pathways can be funded under this program. *(Use as listed above)*
- Many of the same projects that can be funded by the STP – including pedestrian walkways, maps, brochures, and PSAs – can also receive funds as part of the Environmental Protection Agency’s (EPA’s) Congestion Mitigation and Air Quality Improvement Program. *(Use as listed above)*
- The Highway Safety Improvement Program (HSIP) has been established by SAFETEA-LU to allow states to target their most critical safety needs. States are required to develop and implement a safety plan and submit safety reports that describe hazardous locations, progress in implementation of highway safety improvement projects, and the effectiveness of those projects in reducing injuries and fatalities. *(Use to make hazardous areas safer for pedestrians and vehicular traffic in such areas as Broad Street / Chestnut Street, Neely Road, and Old Hendersonville Highway / Broad Street)*
- SRTS is intended to enable and encourage elementary and middle school students to walk safely to school. Funding is available to facilitate planning, development, and implementation of activities and projects that reduce traffic, fuel consumption, and air pollution and improve safety within approximately two (2) miles of elementary and middle schools. *(Use to create sidewalks and MUPs around Brevard Elementary, Pisgah Forest Elementary, and Brevard Middle School, as well as to fund pedestrian education and safety programs for those students)*
- The Recreational Trails Program has 30% of its funds set aside for motorized trail projects, 30% for nonmotorized trail uses, and the remainder can be spent on either. As a result of SAFETEA-LU legislation, these funds can now be used to cover the costs of construction and maintenance equipment, real estate, educational programs, state administration, and assessment of trail conditions. *(Use as listed above)*
- When providing pedestrian access to the Pisgah National Forest, especially if in conjunction with roads, Brevard is eligible to apply for funds from the Federal Lands Highway Program (FLHP). For most types of Federal-aid highway funded projects, FHLP funds can be used as the state or local match. *(Use to create trails / MUPs to Pisgah National Forest)*
- Land and Water Conservation Fund (LWCF) Grants are designed to fund projects that bring parks and recreation, including multi-use trails, closer to people’s homes. The fund is financed through the sale or lease of nonrenewable resources owned by the government. *(Use to create MUPs near neighborhoods)*

- Watershed Protection and Flood Prevention Grants for Small Watersheds are a source of funding offered to state and local agencies or nonprofit organizations to create and maintain watershed improvements of less than 250,000 acres. Financial and technical assistance are available – a 50% local match is required for public recreation projects. *(Use to create MUPs near streams and rivers)*
- The US Department of Housing and Urban Development (HUD), through the Community Development Block Grant (CDBG), provides grants to communities to revitalize their neighborhoods, for economic development, and to improve community facilities and services, particularly in low to moderate-income areas. These funds can be used to create safe multi-use trails or sidewalks. *(Use as listed above)*
- Sidewalks or multi-use paths from low-income residential developments to employment areas can be funded in part by Job Access and Reverse Commute Grants. *(Use as listed above)*
- With the development of a street-tree inventory, municipal tree ordinance, an urban forestry-management plan, and a tree commission, committee, or department, a community qualifies for the Urban and Community Forestry Assistance Program. This program provides grants of up to \$10,000 so that communities can purchase trees to be planted along streets and multi-use paths or in parks. *(Use to enhance sidewalks or MUPs)*
- “Clean Air Transportation Communities: Innovative Projects to Improve Air Quality and Reduce Greenhouse Gases” is a program of the EPA’s Office of Transportation and Air Quality that provides funds for innovative projects that reduce emissions, such as multi-use paths, to state and local agencies involved with transportation and air quality issues. *(Use for sidewalks or MUPs)*
- The Brownfields Redevelopment Initiative provides loan guarantees and funding for the cleanup and redevelopment of environmentally contaminated commercial and industrial sites. Multi-use paths and sidewalks can easily be integrated into the new site designs. *(Use as listed above in abandoned commercial / industrial areas)*
- Private landowners who agree to place sensitive wetlands under permanent easements, such as placing greenways with multi-use paths within riparian zones, can receive payment through the Wetlands Reserve Program. *(Use to promote contribution of land for access to trails / MUPs)*

State Funding

- The Transportation Improvement Program (TIP) is a prioritized list of transportation facility construction projects throughout the State of North Carolina. Funding for the projects comes from Federal Aid and the State Highway Trust Fund. The Department of Bicycle and Pedestrian

Transportation compiles a 4-year scheduled list from Metropolitan and Rural Planning Organization (MPO and RPO) TIPs, individual project requests, and internal assessments of candidate pedestrian projects to be considered for the TIP. *(Use for incidental sidewalk projects along state roads, incidental MUP projects along state roads, or independent MUPs)*

- State Roadway Construction Funds can be used to fund pedestrian facilities as incidental to roadway or independent projects. Pedestrian accommodations built as part of scheduled improvement projects are typically funded by a combination of federal and state funds. Independent projects, such as off-road multi-use paths or sidewalk construction on roads that are not scheduled for improvement, have a shot at a portion of the \$6 million set aside annually for such projects. \$100,000 is offered per NCDOT Highway Division for hazard elimination projects. *(Use for sidewalks or MUPs along state roads)*
- The Governor's Highway Safety Program (GHSP) provides funds to carry out various pedestrian and bicycle safety initiatives, including educational programs and materials. Based on specific amounts requested for projects, funds allocated to the GHSP fluctuate annually. *(Use for pedestrian safety programs)*
- The North Carolina Parks and Recreation Trust Fund (PARTF) provides local governments with dollar-for-dollar matching grants to purchase land or develop recreational projects for the general public. *(Use as listed above for MUPs)*
- \$135,000 in Adopt-A-Trail Grants is awarded each year through the North Carolina Trails Program. Government agencies, non-profit organizations, and private groups are eligible to apply for the grants, and funds can be used to build, sign, maintain, renovate, or map and create brochures of facilities. *(Use as listed above for MUPs)*
- State Water Management Funds can be used to fund the purchase of land along waterways to protect or improve the water quality. Greenways and multi-use paths could then be developed. *(Use as listed above)*
- Funding will soon be available to connect residential areas to schools through the Safe Routes to School program. *(Use as listed above)*
- Landowners who donate property for conservation purposes, whether as easements or by sale, are eligible for the North Carolina Conservation Tax Credit. The goal of the program is to provide incentive to protect water supply watersheds, manage stormwater, retain forests and working farms, and to allow for ecological communities through the formation of trails and wildlife corridors. *(Use to promote contribution of land to provide public access to trails / MUPs)*

Local Funding

- Brevard's City Budget typically allots enough funds to cover the cost of pedestrian facilities listed on the Capital Improvements Project Sidewalk Repair Priority List or Recommended New Sidewalk Installation list. Separate line items should be listed in the budget to provide funding for land acquisition, planning, and construction. Programs should be in place to track spending. *(Use as listed above)*
- To create additional revenue specifically for the construction of new pedestrian facilities, a Bond Referendum could be placed on the local ballot in the next general election. *(Use to pay for construction of new or maintenance of existing pedestrian facilities)*
- A Trust Fund could be set up to collect money from a variety of sources, including gifts, private grants, estate donations, and municipal and county general funds. The fund could be administered by a private advocacy group or local commission and could aid in land acquisition, especially of large high-priority parcels that may be unavailable if not obtained by the initiative of members of the private sector. *(Use to acquire land and rights-of-way for various pedestrian facilities)*

Private Funding – Public Involvement

- Local private-sector assistance may be provided through monetary donations, donation of services such as equipment use and labor, or discounts on material costs from local businesses. *(Various uses, as listed above)*
- Project sponsors can purchase amenities such as benches, trash receptacles, mile markers, entry signage and bollards to assist in funding while enhancing the overall project. Another option is to sell linear feet of a multi-use path at the unit cost for said path. Some sort of recognition should be provided for sponsors, possibly through a plaque or certificate. *(Various uses, as listed above)*
- Volunteers from within the community can aid in the expansion of the pedestrian facility network by conducting fundraisers or by donating labor to construction, landscaping, and maintenance after the facility is in place. Community volunteers can be drawn from civic groups, scouting groups, and outdoor clubs. Volunteers could adopt trails or sidewalks or portions of them to keep clean and beautify through the years, saving the city money over time. *(Various uses, as listed above)*
- The Small Business Association has set up the Small Business Tree-Planting Program – grants of up to \$10,000 are provided to purchase trees to be planted along streets, along multi-use paths, and in parks. The grants are used to develop contracts with local businesses for the plantings. *(Use to enhance sidewalks or MUPs)*

- Through the Design Arts Program, the National Endowment for the Arts can provide states, local agencies, individuals, and nonprofit organizations with grants if their project incorporates urban design, planning, historic preservation, architecture, landscape architecture, or other community improvement activities, i.e. multi-use path development. Grants can provide up to \$50,000 and require a 50% local match. *(Use for sidewalks or MUPs in areas of historic or architectural significance)*
- The Robert Wood Johnson Foundation is dedicated to enhancing the health and health care of every American. Grants are prioritized into four goal areas, one of which is the promotion of healthy communities and lifestyles. Multi-use paths and sidewalks allow for social interaction and provide more opportunities for people to walk to their destinations safely rather than relying on motorized transportation. *(Use for any sidewalk or MUP)*
- Small grants of \$250-\$2,000 are offered for planning, design, and development of greenways through a partnership between the Conservation Fund's American Greenways Program, Eastman Kodak Corporation, and the National Geographic Society. *(Use for off-road MUPs)*
- The World Wildlife Fund Innovative Grants Program awards small grants to local, regional, and statewide nonprofit organizations to implement the conservation of natural resources. Projects must accomplish at least one of the following: conserve wetlands, conserve coastal resources, protect endangered species, preserve migratory birds, or establish and sustain protected natural areas, i.e. greenways. The maximum award through a single grant is \$10,000, and funds can be used to pay administrative costs including planning, technical assistance, legal costs, and other costs to acquire critical lands; fees of consultants; and preparation of visual presentations and brochures. *(Use for greenway easements for off-road MUPs)*
- Local community and environmental activities and educational programs for children that are put on by charitable organizations may be funded through the Wal-Mart Foundation. Organizations must work with the local store manager to discuss application. *(Use for pedestrian safety education)*
- REI, Recreational Equipment Incorporated, calls its employees to nominate nonprofit organizations who are interested in enhancing and protecting natural resources for outdoor recreation to receive grants ranging from \$500-\$8,000. Grants can be used to protect lands and waterways and make them accessible to more people, promote proper care for outdoor resources, increase access to outdoor activities, better utilize or preserve natural resources for recreation, encourage involvement in muscle-powered recreation, and promote safe participation in outdoor muscle-powered recreation. *(Use for any off-road MUP)*

• Identify Special Funding Opportunities for High Priority Projects

All of the funding opportunities listed above, and others that are not listed that may become available in the future, should be applied when possible. Mid- and long range projects may be included in later editions of the TIP as enhancement projects. If a roadway improvement project is scheduled for a road that currently has no pedestrian facilities, NCDOT may be approached in an effort to get pedestrian facilities installed incidental to the project. Mapping and signing projects may also be included in the TIP. Safety projects may be funded by the Governor's Highway Safety Program.

Projects scheduled for construction along major and minor thoroughfares throughout the city may be funded by a bond referendum. Grant programs are the preferred method of payment for large scale projects, as they do not have to be repaid by the city or its citizens. Public/private partnerships are another good way to make pedestrian facility improvements, as they allow the public to take an extra sense of pride out of the facilities.

• Identify Supporting Policies / Guidelines

The NCDOT has compiled a report on Planning and Designing Local Pedestrian Facilities. These guidelines are provided to localities throughout North Carolina and cover several, but not all, factors relating to the construction or restoration of pedestrian facilities. The Federal Highway Administration (FHWA) has declared that all transportation projects will incorporate bicycling and walking facilities, unless exceptional circumstances exist. The FHWA also calls for a minimum five-foot sidewalk on both sides of collector and arterial streets, with wider sidewalks and/or landscaped buffers where high volumes of vehicular and pedestrian traffic are present. Wider sidewalks may be necessary at intersections to accommodate accessible curb ramps. The American Association of State Highway and Transportation Officials (AASHTO) has also published the "Guide for the Planning, Design, and Operation of Pedestrian

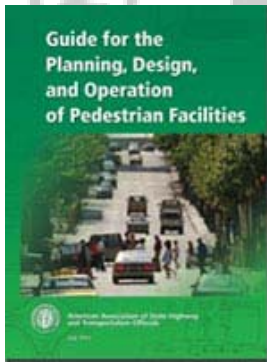


Fig. 7-6: AASHTO's "Guide for the Planning, Design, and Operation of Pedestrian Facilities" offers a great deal of information regarding pedestrian facilities. (<https://bookstore.transportation.org>)

Facilities," which provides guidelines on how to incorporate safe accommodations for pedestrian users. These guidelines will be used when constructing new or rehabilitating existing sidewalks, where feasible.

There are also guidelines for designing pedestrian facilities in Brevard's UDO (Appendix E). Section 13.5N requires pedestrian and bicycle infrastructure on both sides of every street within the city limits unless the Technical Review Committee grants an exception. Sidewalks are not currently required in the ETJ. The Subdivision Ordinance lists development standards that dictate the locations, dimensions, and types of pedestrian access that must be installed. Sections 62-141 through 62-145 relate to pedestrian and bicycle pathways, though no specific design

guidelines are discussed. Business owners in the Central Business District are to keep the sidewalks around their business free of trash and debris, and are to remove snow and ice as soon as feasible. Homeowners are free to do the same. Approval to do any other modifications to the sidewalk must be obtained from the city manager.

• **Develop Evaluation / Monitoring Process**

Each year, the City of Brevard should strive to complete at least one segment of multi-use path, one segment of major sidewalk improvements, and 2500 linear feet of minor sidewalk improvements. Sidewalk improvements include construction of new sidewalks and the repair of existing facilities. An annual assessment should be done on the pedestrian system to ensure that it stays up-to-date, based on needs. This can be done in conjunction with planning for the CIP each year, and the Pedestrian Plan can be updated at the same time to keep the document fresh by removing completed projects.



SECTION 8: MAPS AND ILLUSTRATIONS

• System Maps

The following pages are maps of the current and proposed pedestrian facilities in Brevard. Each of the first three maps shows where pedestrian generators such as grocery stores and schools are located. A map of the existing system of public and private pedestrian facilities is shown on Map 8-1. Map 8-2 presents the condition of existing facilities, rated excellent, good, fair, or poor. Possible locations for future sidewalks, paths, and trails are illustrated on Map 8-3. Priority corridors for improvement include all major and minor thoroughfares.

• Proposed Projects

Maps 8-4 and 8-5 illustrate new sidewalk and multi-use path priorities, respectively. The ideal goal for pedestrian facilities includes multi-use paths installed on one side of all major and minor thoroughfares and arterials. Sidewalks on both sides of all collectors and local roadways and arterials should also be constructed where feasible. Major thoroughfares serve through traffic with restricted or limited access and include such highways as US 64, NC 280, and Greenville Highway. Minor thoroughfares such as Ecusta Road, French Broad Street, and Country Club Road emphasize vehicular flow with limited access. Local access streets (collector and local roadways) provide more access than thoroughfares. Collector roadways, such as Osborne Road, Fisher Road, and Maple Street, collect traffic from local streets and carry them to arterials with equal emphasis on vehicular flow and access. An emphasis is placed on property access on local roadways such as Oak Park Drive, Salem Street, and Gillespie Circle.

• Project Priorities

Other high priority projects include those that have been included on the sidewalk construction list for years, such as Probart Street to the Music Center, Gallimore Road, Park Avenue from East French Broad Street to the Kings Creek Bridge, and Elm Bend Road from Sapphire Manor to the rear entrance of Brevard Elementary School. Prioritized improvements are discussed in Section 7.

Maintenance will need to be done continually to lengthen the service life of the facilities and make them safe, clean, and inviting. Examples include trimming overgrown bushes, removing overgrowth within 10 feet above the sidewalk or path, and repairing broken sections. Some of the landscaping tasks could and should be done by property owners.

SECTION 9: RECOMMENDATIONS

• Project Descriptions

A large number of construction projects were recommended in Section 7. Much coordination will need to be done to ensure these projects are undertaken in a timely and efficient manner. Projects were prioritized in an attempt to have new construction throughout the city rather than concentrated pockets of improvement while other areas received nothing.

Steps will also need to be taken to insure that opportunities for expansion of the existing pedestrian network within Brevard are taken. For example, in the event that rail transit between Transylvania and Henderson counties is not realized for many years, the rail beds could still be kept open to the public for use as a pedestrian facility. The state and the Land-of-Sky should be contacted to carry this project out.

Police should take advantage of opportunities to educate the citizens of Brevard about pedestrian safety. The annual Bicycle Rodeo could be expanded to include pedestrian safety issues. The City of Brevard Newsletter could include articles or public service announcements and safety tips to help educate even more people or reiterate the message. Signs telling drivers to “Yield here to pedestrians,” if not overused, could be helpful reminders. Portable flashing beacons could be used in conjunction with signs in various areas to call attention to pedestrians for a period of time and then moved to other locations.

Programs to educate children and parents about pedestrian safety and the benefits of walking should be established in cooperation with local law enforcement, the Transylvania County Public School System, and Parent-Teacher Organizations. Students and parents should be encouraged to walk to school, and the City should work with NCDOT, Transylvania County, and the school system to improve pedestrian access to the schools. The North Carolina “Safe Routes to School” program is one potential resource for these activities.

Increased enforcement of pedestrian-related laws and ordinances will do much to raise awareness of pedestrians in the area. Codes, such as those regarding sidewalk obstructions, need to be enforced so that facilities provide the capacity they were designed to handle. Failure of drivers to yield to pedestrians in crosswalks leads to many injuries in Brevard. The penalty for failure to yield is four (4) points on a non-commercial driver’s license and five (5) points on a commercial driver’s license. Accumulation of 12 points in three (3) years results in the loss of one’s license. Effective December 1, 2004, NC House Bill 965 increased penalties for “Failure to Yield Causing Death or Injuries”. When turning right, failure to yield to a pedestrian could cost a driver \$100 to \$500, plus points on the driver’s license. If serious bodily injury is caused, a fine of \$500 can be assessed and the driver’s license can be suspended for 90 days. Enforcement of this law in various parts of the city will remind drivers of their responsibilities. Pedestrians also need to be held responsible for their actions when they break the law.

Booths could be set up at the White Squirrel Festival and other festivals to promote pedestrian transportation. Maps and safety information could be distributed, and suggestions for improvements could be taken. The Employee Walking Club and Blue Cross and Blue Shield's "100 Miles in 100 days" programs could be granted more publicity, possibly reaching an audience that does not know about the programs. In addition, the Transylvania County Health Department and Transylvania Community Hospital should continue to promote the many benefits of walking.

• Pedestrian Friendly Policy Recommendations

Sidewalks should be required in conjunction with any new development or redevelopment of property within the city limits. In ten years without this requirement in place, many opportunities have been passed up on Asheville Highway between Osborne Road and the northern city limits alone. Over that period of time, 4930 linear feet, more than nine-tenths of a mile, of sidewalk that could have been required have gone unconstructed through a combination of variances and weak policy enforcement. Sidewalks internal to private developments should also be required, and they should provide connections to existing sidewalks beyond the property line.



Fig. 9-1: A residential development in Brevard with nice pedestrian facilities.

• Pedestrian Friendly Standards and Guidelines Recommendations

Standards and guidelines relating to sidewalks, multi-use paths, trails, signage, and other supplements to the pedestrian facility network should be clearly described and illustrated. Cross-sections are provided in Section 5 and Appendix E.

• Funding Recommendations

Every available funding source must be pursued to ensure that the projects recommended in Section 7 are carried out. Brevard's Capital Improvement Projects budget currently funds many of the pedestrian facility projects in and around the city. To pay for large-scale projects to be built in the next 10 years, including multi-use paths along major corridors and sidewalks to be constructed throughout the city, a bond referendum is recommended. It is also recommended that every feasible grant opportunity be pursued for each project. Many projects would meet the criteria for multiple funding sources as listed in Section 7. NCDOT Small Urban Construction Project funds and Moving Ahead! funds may be applicable for sidewalks and paths designed as a portion of development or redevelopment projects. Funding should be sought for higher priority projects first, but in the event that funding is found for a lower priority project before it is found for a higher priority project, the funded project should be completed, regardless of priority.

• **Staffing / Committee Recommendations**

Updating the pedestrian plan every two to five years should be a high priority for staff. Prices will need to be changed, projects will need to be removed and added, and needs may be reassessed. The responsibility of thoroughly updating the plan every two to five years should be assigned to the Planning Board. There should be some member of the Public Works Department in charge of documenting improvements to existing and construction of new pedestrian facilities to make updating the Pedestrian Plan less complicated.

• **Local Ordinance Recommendations**

The following changes to or clarifications of the local development ordinance are recommended:

- Consolidation of pedestrian related ordinances in the UDO would help developers who are interested in building in Brevard access the information they need regarding pedestrian facilities more easily. This would also make it simpler for code enforcers to find necessary information. An index for the document would serve a similar purpose.
- Sidewalks should be required on all major and minor arterials, collector streets, and local roadways, where feasible. This will compel the construction of sidewalk sections as new development and redevelopment moves into the area, saving the city money when the decision is made to construct a major pedestrian facility. Code enforcement is necessary.
- In locations where sidewalks are present, property owners should be required to keep the sidewalk free and clear of trash, leaves, debris, dirt, ice, and snow so that it is usable by all. This is already required in the Central Business District, but should include the rest of the city, as well.
- Off-road easements should be attained in conjunction with the approval of the development of a piece of property on which a multi-use path is proposed to be built. Right-of-access easements would allow for a piece of land to be used for the installation of utilities and a greenway/MUP.

SECTION 10: IMPLEMENTING THE PLAN

• Plan Approval / Adoption Process

Following the final public input session on December 6, 2005, the Pedestrian Plan PSC helped finalize the document. NCDOT preliminary approval and Brevard City Council approval was sought before July 1, 2006. The document was forwarded to the Land-of-Sky RPO for approval before August 1, 2006 and then sent to the NCDOT for final approval before September 1, 2006.

• Project Planning / Implementation Process

Policies, standards, and guidelines will have to be changed for many of the projects mentioned before to be put in place. Changing existing policies that do not require pedestrian facilities, easements, or a fee-in-lieu will be a solid start. The new UDO has set standards and design guidelines for determining minimum requirements for pedestrian facilities in different zoning districts and for different roadway classifications. It will also help to incorporate pedestrian facilities or easement for pedestrian facilities into any new construction plans submitted to the city.

A bond referendum may be the only way to pay for many of the large-scale, high-priority construction projects mentioned before. A referendum for sidewalks and multi-use paths could be considered as part of a larger streets and sidewalks bond referendum to cover such issues as paving and other necessary improvements to the existing transportation network. Much planning will need to be done to ensure that the bond referendum makes the deadline to be placed on the ballot in the next major election. Several months of advertisement will be necessary, meaning the composition of the referendum needs to begin as soon as possible.

Construction projects that have been completed must be removed and new projects will need to be added as needs in the area change. As time goes by, transit may become a factor in Brevard, the city will likely grow, and the population may shift. The types of supplemental programs and projects that are required in and around Brevard will change. A yearly update by planning staff and a committee update every five years will help keep the pedestrian plan applicable.

• Project Summary and Assessment

The following tables summarize recommended projects and programs, assess the level of difficulty in implementing each, and note key impediments to new facilities.

Recommended New Pedestrian Facility Priorities

Pedestrian Facility	Type	Length	Cost	Right-of-Way Cost	Construction Difficulty	Primary Impediments to Construction	TIP Project	Funded	Complies with Plan	Public Support
Allison Road to McLean Road	MUP	4,750	\$ 475,000	ROW Secure	Low	Utility Poles, Curb Cuts	Yes	Yes	Yes	Yes
Sports Complex to Pisgah National Forest	MUP	4,300	\$ 430,000	NCDOT Encroachment / Private Easement Required	Low	Curb Cuts, Storm Drainage, Flood Hazards	Yes	Yes	Yes	Yes
Gallimore Road - US 276 / Greenville Highway to Country Club Road	MUP	4,420	\$ 442,000	Additional ROW / NCDOT Encroachment / Private Easement Required	Moderate	Utility Poles, Curb Cuts, Slope, Storm Drainage, Stream Crossings	No	Partially	Yes	Yes
Asheville Highway - Hospital Drive to Ecusta Road	MUP	6,650	\$ 665,000	NCDOT Encroachment Required	Moderate	Utility Poles, Curb Cuts, Slope, Storm Drainage	No	No	Yes	Yes
Railroad Avenue - McLean Road to West Main Street	MUP	3,010	\$ 301,000	City Encroachment / Additional ROW Required	Moderate	Utility Poles, Stream Crossing	No	No	Yes	Yes
West Main Street - South Oaklawn Avenue to English Hills / WCCA / Head Start Facility	MUP	2,840	\$ 284,000	Additional ROW / NCDOT Encroachment / Private Easement Required	High	Utility Poles, Curb Cuts, Slope, Storm Drainage, Structures	No	No	Yes	Yes
English Hills / WCCA / Head Start Facility to Rosman Highway	MUP	5,300	\$ 530,000	Additional ROW / NCDOT Encroachment / Private Easement Required	High	Utility Poles, Storm Drainage	No	No	Yes	Yes
Rosman Highway to Country Club Road / High School	MUP	5,530	\$ 553,000	Additional ROW / NCDOT Encroachment / Private Easement Required	High	Flood Hazards	No	No	Yes	Yes
West Main Street to Bracken Mountain Boundary / Brevard Music Center	MUP	2,840	\$ 284,000	Additional ROW / Private Easement Required	High	Utility Poles, Curb Cuts, Slope, Storm Drainage, Structures	No	No	Yes	Yes

Pedestrian Facility	Type	Length	Cost	Right-of-Way Cost	Construction Difficulty	Primary Impediments to Construction	TIP Project	Funded	Complies with Plan	Public Support
Old Hendersonville Highway - Chestnut Street to Ecusta Road	MUP	7,180	\$ 718,000	Additional ROW / NCDOT Encroachment Required	Moderate	Utility Poles, Storm Drainage, Curb Cuts	No	No	Yes	Yes
Elm Bend Road - Sapphire Manor to BES	SW	690	\$ 53,820	NCDOT Encroachment / Private Easement Required	Low	Utility Poles, Storm Drainage, Curb Cuts	No	Partially	Yes	Yes
Probart Street	SW	3,260	\$ 254,280	Additional ROW / NCDOT Encroachment Required	High	Utility Poles, Curb Cuts, Slope, Storm Drainage, Structures	No	Partially	Yes	Yes
Neely Road, East French Broad Street to Chestnut Street	SW	5,160	\$ 402,480	Additional ROW / NCDOT Encroachment Required	High	Utility Poles, Curb Cuts, Storm Drainage, Stream Crossing, Structures	No	Partially	Yes	Yes
Morris Road	SW	3,040	\$ 237,120	NCDOT Encroachment Required	Moderate	Utility Poles, Storm Drainage, Curb Cuts	No	No	Yes	Yes
McLean Road	SW	770	\$ 60,060	Additional ROW / NCDOT Encroachment Required	High	Utility Poles, Curb Cuts, Slope, Storm Drainage	No	No	Yes	Yes
Hospital Road / Temple Church Road	SW	4,030	\$ 314,340	City Encroachment Required	Low	Utility Poles, Storm Drainage, Curb Cuts	No	No	Yes	Yes
Johnson Street	SW	1,030	\$ 80,340	City Encroachment / Additional ROW Required	Moderate	Utility Poles, Storm Drainage, Curb Cuts	No	No	Yes	Yes
Deer Lake Road	SW	1,080	\$ 84,240	City Encroachment Required	Moderate	Utility Poles, Storm Drainage, Curb Cuts	No	No	Yes	Yes
Greenville Highway	SW	1,340	\$ 104,520	NCDOT Encroachment Required	Moderate	Utility Poles, Storm Drainage, Curb Cuts	No	No	Yes	Yes
Pedestrian Crossing Improvements at US 276 / Gallimore Road	X-ing	80	Unknown at this time	NCDOT Encroachment Required	High	Absence of traffic signal at intersection	No	No	Yes	Yes

Recommended Maintenance Priorities for Existing Facilities

Sidewalk Repairs		
Sidewalk Location	Length	Cost
French Broad Street (West)	50	\$1,500
Whitmire Street	3,669	\$110,062
Caldwell Street (South)	870	\$26,109
Main Street (West)	12	\$360
King Street	1,175	\$35,236
South Gaston (South)	48	\$1,440
Maple Street	1,515	\$45,460
Main Street (East)	1,767	\$53,000
Probart Street	40	\$1,200
Morgan Street (West)	429	\$12,862
Greenville Hwy Total	903	\$27,090
Jordan Street (West)	12	\$360
Carver Street	648	\$19,441
Oakdale Lane	1,924	\$57,726
Total	13,062	\$391,846

Recommended Pedestrian Policies / Programs

Policy/Program	Difficulty to Implement	Startup Cost	Continuing Cost
Pedestrian Safety for Elementary School Students	Moderate	\$1,000-\$2,000	\$1,000-\$2,000 printing costs and staff time

Policy/Program	Difficulty to Implement	Startup Cost	Continuing Cost
Advanced Pedestrian Safety for Middle School Students	Moderate	\$1,000-\$2,000	\$1,000-\$2,000 printing costs and staff time
How to Interact with Pedestrians for Drivers - High School	Moderate	\$1,000-\$2,000	\$1,000-\$2,000 printing costs and staff time
How to Interact with Pedestrians for Drivers - Beyond High School	Moderate	\$1,000-\$2,000	\$1,000-\$2,000 printing costs and staff time
Pedestrian Safety Program for Residents of Brevard	Moderate	\$1,000-\$2,000	\$1,000-\$2,000 printing costs and staff time
Pedestrian Safety Booth at White Squirrel Festival	Low	\$1,000	\$1,000 per year, primarily printing costs
Promote Pedestrian Encouragement Programs at White Squirrel Festival	Low	\$1,000	\$1,000 per year, primarily printing costs
Promote Pedestrian Encouragement Programs via Media Outlets	Low	\$500	\$500 per year for ad costs
Pedestrian Safety Blurbs via Media Outlets	Low	\$500	\$500 per year for ad costs
Mapping of Pedestrian Facilities - Available to Public via Website (PDF)	Low	None	Limited Staff time
Mapping of Pedestrian Facilities - Available to Public through Heart of Brevard	Low	\$1,000	\$1,000 per year, primarily printing costs
Downtown Pedestrian Signage / Safety / Crosswalks	Low	To Be Determined	To Be Determined

APPENDICES

- APPENDIX A: NCDOT Pedestrian Policy Guidelines**
- APPENDIX B: Existing Sidewalk Conditions**
- APPENDIX C: Contact Lists**
- APPENDIX D: Survey Results**
- APPENDIX E: Summary of Existing Sidewalk Regulations**
- APPENDIX F: Comprehensive Transportation Plan Project Priority List**
- APPENDIX G: Capital Improvement Projects**
- APPENDIX H: Existing Pedestrian Facility Length (by street and by type)**
- APPENDIX I: Steering Committee Member List**
- APPENDIX J: Authorization Documents**
- APPENDIX K: Resolution Adopting Pedestrian Plan**
- APPENDIX L: NCDOT Letter Approving Plan**

APPENDIX A:
NCDOT Pedestrian Policy Guidelines

APPENDIX B:
Existing Sidewalk Conditions

APPENDIX C:

Contact Lists

APPENDIX D:

Survey Results

APPENDIX E:

Summary of Existing Sidewalk Regulations

APPENDIX F:

**Comprehensive Transportation Plan
Project Priority List**

APPENDIX G:
Capital Improvement Projects

APPENDIX H:
Existing Pedestrian Facility Length

APPENDIX I:
Steering Committee List

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Resolution Adopting Pedestrian Plan

APPENDIX L:
NCDOT Letter Approving Plan

DEPARTMENT OF TRANSPORTATION PEDESTRIAN POLICY GUIDELINES EFFECTIVE OCTOBER 1, 2000

These guidelines provide an updated procedure for implementing the Pedestrian Policy adopted by the Board of Transportation August 1993 and the Board of Transportation Resolution September 8, 2000. The resolution reaffirms the Department's commitment to improving conditions for bicycling and walking, and recognizes non-motorized modes of transportation as critical elements of the local, regional, and national transportation system. The resolution encourages North Carolina cities and towns to make bicycling and pedestrian improvements an integral part of their transportation planning and programming.

REQUIREMENTS FOR DOT FUNDING:

REPLACEMENT OF EXISTING SIDEWALKS:

The Department will pay 100% of the cost to replace an existing sidewalk that is removed to facilitate the widening of a road.

TIP INCIDENTAL PROJECTS:

DEFINED: Incidental pedestrian projects are defined as TIP projects where pedestrian facilities are included as part of the roadway project.

REQUIREMENTS:

1. The municipality and/or county notifies the Department in writing of its desire for the Department to incorporate pedestrian facilities into project planning and design. Notification states the party's commitment to participate in the cost of the facility as well as being responsible for all maintenance and liability. Responsibilities are defined by agreement. Execution is required prior to contract let.

The municipality is responsible for evaluating the need for the facility (ie: generators, safety, continuity, integration, existing or projected traffic) and public involvement.

2. Written notification must be received by the **Project Final Field Inspection (FFI) date**. Notification should be sent to the Deputy Highway Administrator - Preconstruction with a copy to the Project Engineer and the Agreements Section of the Program Development Branch. Requests received after the project FFI date will be incorporated into the TIP project, if feasible, and only if the requesting party commits by agreement to pay 100% of the cost of the facility.

3. The Department will review the feasibility of including the facility in our project and will try to accommodate all requests where the Department has acquired appropriate right of way on curb and gutter sections and the facility can be installed in the current project berm width. The standard project section is a 10-ft berm (3.0-meter) that accommodates a 5-ft sidewalk.

In accordance with AASHTO standards, the Department will construct 5-ft sidewalks with wheelchair ramps. Betterment cost (ie: decorative pavers) will be a Municipal responsibility.

4. If the facility is not contained within the project berm width, the Municipality is responsible for providing the right of way and/or construction easements as well as utility relocations, at no cost to the Department. This provision is applicable to all pedestrian facilities including multi-use trails and greenways.

5. A cost sharing approach is used to demonstrate the Department’s and the municipality’s / county’s commitment to pedestrian transportation (sidewalks, multi-use trails and greenways). The matching share is a sliding scale based on population as follows:

MUNICIPAL POPULATION	DOT PARTICIPATION	LOCAL PARTICIPATION
> 100,000	50%	50%
50,000 to 100,000	60%	40%
10,000 to 50,000	70%	30%
< 10,000	80%	20%

Note: The cost of bridges will not be included in the shared cost of the pedestrian installation if the Department is funding the installation under provision 6 - pedestrian facilities on bridges.

6. For bridges on streets with curb and gutter approaches, the Department will fund and construct sidewalks on both sides of the bridge facility if the bridge is less than 200 feet in length. If the bridge is greater than 200 feet in length, the Department will fund and construct a sidewalk on one side of the bridge structure. The bridge will also be studied to determine the costs and benefits of constructing sidewalks on both sides of the structure. If in the judgement of the Department sidewalks are justified, funding will be provided for installation. The above provision is also applicable to dual bridge structures. For dual bridges greater than 200 ft in length, a sidewalk will be constructed on the outside of one bridge structure. The bridges will also be studied to determine if sidewalks on the outside of both structures are justified.

7. FUNDING CAPS are no longer applicable.

8. This policy does not commit the Department to the installation of facilities in the Department’s TIP projects where the pedestrian facility causes an unpractical design modification, is not in accordance with AASHTO standards, creates an unsafe situation, or in the judgement of the Department is not practical to program.

INDEPENDENT PROJECTS

DEFINED: The DOT has a separate category of funds for all independent pedestrian facility projects in North Carolina where installation is unrelated to a TIP roadway project. An independent pedestrian facility project will be administered in accordance with Enhancement Program Guidelines.

Owner_Name	Street	Section	Present Function	Material
City of Brevard	Allison Creek Bridge - City Softball Fields	Allison Creek Bridge - City Softball Fields	Multi-Use Path	Asphalt
City of Brevard	Asheville Hwy	(Fisher - Green Acre	Sidewalk	Concrete
City of Brevard	Asheville Hwy	(K-Mart - Fisher	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Allison-Straus Parkway	Multi-Use Path	Asphalt
City of Brevard	Asheville Hwy	Asheville Hwy at Enmark	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Caldwell - K-Mart	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Caldwell / Broad / College - Food Lion	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Deerlake - Osborne	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Green Acre - Jackson / Chestnut	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Green Acre - Old 64	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Jackson / Chestnut - Deer Lake (N)	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Jackson / Chestnut - Deer Lake (S)	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Osborne - Allison	Sidewalk	Concrete
City of Brevard	Asheville Hwy	Straus Parkway - Hospital Drive	Multi-Use Path	Asphalt
City of Brevard	Asheville Hwy / Ecusta	At Lowes	Multi-Use Path	Asphalt
City of Brevard	Blueberry Hill	Blueberry Hill	Sidewalk	Concrete
Transylvania County Public Schools	Brevard Elementary		Recreation / Fitness	Asphalt
Transylvania County Schools	Brevard High School		Recreation / Fitness	Asphalt
City of Brevard	Broad N	Appletree - French Broad (E)	Sidewalk	Concrete
City of Brevard	Broad N	Appletree - French Broad (W)	Sidewalk	Concrete
City of Brevard	Broad N	Brevard College - Brevard College / Caldwell (E)	Sidewalk	Concrete
City of Brevard	Broad N	French Broad - Brevard College (E)	Sidewalk	Concrete
City of Brevard	Broad N	Kellys - Appletree (E)	Sidewalk	Concrete
City of Brevard	Broad N	Kellys - Appletree / Professional (W)	Sidewalk	Concrete
City of Brevard	Broad N	Main - Probart (E)	Sidewalk	Concrete
City of Brevard	Broad N	Main - Probart (W)	Sidewalk	Concrete
City of Brevard	Broad N	Probart - Kellys (E)	Sidewalk	Concrete
City of Brevard	Broad N	Probart - Kellys (W)	Sidewalk	Concrete
City of Brevard	Broad S	Country Club - Miner (E)	Sidewalk	Concrete
City of Brevard	Broad S	Jordan - Main (E)	Sidewalk	Concrete
City of Brevard	Broad S	Jordan - Main (W)	Sidewalk	Concrete
City of Brevard	Broad S	Miner - Varsity (E)	Sidewalk	Concrete
City of Brevard	Broad S	Miner - Varsity (W)	Sidewalk	Concrete
City of Brevard	Broad S	Morgan - Jordan (E)	Sidewalk	Concrete
City of Brevard	Broad S	Morgan - Jordan (W)	Sidewalk	Concrete
City of Brevard	Broad S	Oakdale - Morgan (E)	Sidewalk	Concrete
City of Brevard	Broad S	Oakdale - Morgan (W)	Sidewalk	Concrete
City of Brevard	Broad S	Rosman - Miner (W)	Sidewalk	Concrete
City of Brevard	Broad S	Varsity - Oakdale (E)	Sidewalk	Concrete
City of Brevard	Broad S	Varsity - Oakdale (W)	Sidewalk	Concrete
City of Brevard	Caldwell	Colwell - Wa Bracken (W)	Sidewalk	Concrete

Owner_Name	Street	Section	Present Function	Material
City of Brevard	Caldwell	Food Lion - N Broad / College / Asheville Hwy (W)	Sidewalk	Concrete
City of Brevard	Caldwell	French Broad - Methodist (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Jordan - Main (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Jordan - Main (W)	Sidewalk	Concrete
City of Brevard	Caldwell	King / Kelly's Aly - Professional Aly (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Main - Probart (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Main - Probart (W)	Sidewalk	Concrete
City of Brevard	Caldwell	Morgan - Jordan (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Morgan - Jordan (W)	Sidewalk	Concrete
City of Brevard	Caldwell	Oakdale - Morgan (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Oakdale - Morgan (W)	Sidewalk	Concrete
City of Brevard	Caldwell	Probart - Kelly's Aly / King (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Professional Aly - French Broad (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Silverstein - Oakdale (E)	Sidewalk	Concrete
City of Brevard	Caldwell	Silverstein - Oakdale (W)	Sidewalk	Concrete
City of Brevard	Caldwell	WA Bracken - Silverstein (W)	Sidewalk	Concrete
City of Brevard	Caldwell	Whitmire - Food Lion (W)	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Cambridge (E) - Deer Lake	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Cambridge (N) - Canterbury Trl	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Cambridge (S) - Canterbury	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Cambridge (W) - Deer Lake	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Canterbury - Sheffield	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Canterbury - Sheffield	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Deer Lake - Sheffield	Sidewalk	Concrete
City of Brevard	Cambridge Dr.	Deer Lake - Sheffield	Sidewalk	Concrete
City of Brevard	Camp Sapphire	Meadow View - Camp Town	Sidewalk	Concrete
City of Brevard	Camp Town	Camp Sapphire - Meadow View	Sidewalk	Concrete
City of Brevard	Camp Town	Camp Sapphire / Blueberry - Pine Mountain	Sidewalk	Concrete
City of Brevard	Camp Town	Meadow View - Stone House	Sidewalk	Concrete
City of Brevard	Camp Town	Stone House - END	Sidewalk	Concrete
City of Brevard	Canterbury	Canterbury (E) - Cambridge	Sidewalk	Concrete
City of Brevard	Canterbury	Canterbury (W) - Cambridge	Sidewalk	Concrete
City of Brevard	Carver		Sidewalk	Concrete
City of Brevard	Carver		Sidewalk	Concrete
City of Brevard	Cashiers Valley		Sidewalk	Concrete
City of Brevard	Cashiers Valley		Sidewalk	Concrete
City of Brevard	City Softball Fields - Ecusta Road	City Softball Fields - Ecusta Road	Multi-Use Path	Asphalt
City of Brevard	College Station	Caldwell - Food Lion	Sidewalk	Concrete
City of Brevard	Country Club	(Southview - Turnpike)	Sidewalk	Concrete
City of Brevard	Country Club	(Turnpike - Monroe)	Sidewalk	Concrete
City of Brevard	Country Club	Ashworth - Southview	Sidewalk	Concrete

Owner_Name	Street	Section	Present Function	Material
City of Brevard	Country Club	End of Sidewalk - Bridge at H.S.	Sidewalk	Concrete
City of Brevard	Country Club	Gallimore - Brevard High	Sidewalk	Concrete
City of Brevard	Country Club	Gallimore - Gillespie	Sidewalk	Concrete
City of Brevard	Country Club	Gillespie - Ashworth	Sidewalk	Concrete
City of Brevard	Country Club	Gillispie Circle - Hayes	Sidewalk	Concrete
City of Brevard	Country Club	Hayes - South Broad	Sidewalk	Concrete
City of Brevard	Country Club	Monroe - Gillispie Circle	Sidewalk	Concrete
City of Brevard	Danbury	Hopewell - Tutor	Sidewalk	Concrete
City of Brevard	Danbury	Terminus - Hopewell	Sidewalk	Concrete
City of Brevard	Danbury	Tutor - Terminus	Sidewalk	Concrete
City of Brevard	Deer Lake	West terminal - Stone Dr	Sidewalk	Concrete
City of Brevard	Deer Run	Deer Lake - Chipmunk	Sidewalk	Concrete
City of Brevard	Deer Run	Deer Lake - Chipmunk	Sidewalk	Concrete
City of Brevard	Deerlake	(Stone - Fox Cross)	Sidewalk	Concrete
City of Brevard	Deerlake	Deer Run - Broad	Sidewalk	Concrete
City of Brevard	Deerlake	Eldridge - Broad	Sidewalk	Concrete
City of Brevard	Deerlake	Fox Cross - Deer Run	Sidewalk	Concrete
City of Brevard	East Main St. Ext.	Wilson / Main / Greenville - Terminus	Sidewalk	Concrete
City of Brevard	Elm Bend	Greenville Hwy - Throwbridge / Magnolia	Sidewalk	Concrete
City of Brevard	Elm Bend	Throwbridge / Magnolia - Sapphire Manor	Sidewalk	Concrete
City of Brevard	England	(Main - Probart)	Sidewalk	Concrete
City of Brevard	English Hills	Picklesimer Ave. - English Hills Project	Sidewalk	Concrete
City of Brevard	Fisher	(between Montview intersections)	Sidewalk	Concrete
City of Brevard	Fisher	(Montview - Broad)	Sidewalk	Concrete
City of Brevard	Fisher	(Poplar - Montview)	Sidewalk	Concrete
Transylvania County Public Schools	Fisher	Brevard Middle School	Sidewalk	Concrete
City of Brevard	Franklin	Lakeview - French Broad (N)	Sidewalk	Concrete
City of Brevard	Franklin	Lakeview - Lakeview (S)	Sidewalk	Concrete
City of Brevard	Franklin	Main - Lakeview (N)	Sidewalk	Concrete
City of Brevard	Franklin	Main - Lakeview (S)	Sidewalk	Concrete
City of Brevard	French Broad	(Appletree - Franklin)	Sidewalk	Concrete
City of Brevard	French Broad	(Broad - Johnson)	Sidewalk	Concrete
City of Brevard	French Broad	(Caldwell - Broad)	Sidewalk	Concrete
City of Brevard	French Broad	(Franklin - Hilt)	Sidewalk	Concrete
City of Brevard	French Broad	(Johnson - Appletree)	Sidewalk	Concrete
City of Brevard	French Broad	(Railroad - Salem)	Sidewalk	Concrete
City of Brevard	French Broad	(Railroad - Salem)	Sidewalk	Concrete
City of Brevard	French Broad	(Salem - Caldwell)	Sidewalk	Concrete
City of Brevard	French Broad	(Salem - Caldwell)	Sidewalk	Concrete
City of Brevard	French Broad	Hilt - Woodlawn	Sidewalk	Concrete
City of Brevard	French Broad	Woodlawn - Parkview	Sidewalk	Concrete

Owner_Name	Street	Section	Present Function	Material
City of Brevard	Gaston	(Jordan - Main)	Sidewalk	Concrete
City of Brevard	Gaston	(Jordan - Main)	Sidewalk	Concrete
City of Brevard	Gaston	(Main - Probart)	Sidewalk	Concrete
City of Brevard	Gaston	(Main - Probart)	Sidewalk	Concrete
City of Brevard	Gaston	(Maple - Morgan)	Sidewalk	Concrete
City of Brevard	Gaston	(Maple - Morgan)	Sidewalk	Concrete
City of Brevard	Gaston	(Morgan - Jordan)	Sidewalk	Concrete
City of Brevard	Gaston	(Morgan - Jordan)	Sidewalk	Concrete
City of Brevard	Greenville Hwy	(Elm Bend - Hrowbridge)	Sidewalk	Concrete
City of Brevard	Greenville Hwy	(Hrowbridge - sidewalk to Brevard Elementary)	Sidewalk	Concrete
City of Brevard	Greenville Hwy	E. Main - Elm Bend	Sidewalk	Concrete
Transylvania County Public Schools	Greenville Hwy	sidewalk to Brevard Elementary	Sidewalk	Concrete
City of Brevard	Hillview Ave	(Private Dr. - Picklesminer Ave)	Sidewalk	Concrete
Brevard Housing Authority	Hillview Ave		Sidewalk	Concrete
Brevard Housing Authority	Hillview Ave		Sidewalk	Concrete
City of Brevard	Hopewell	Knight - Danbury	Sidewalk	Concrete
City of Brevard	Hopewell	London - Knight	Sidewalk	Concrete
City of Brevard	Hopewell	Osborne - London	Sidewalk	Concrete
City of Brevard	Hospital Drive	Hospital Drive - Mid-Block	Multi-Use Path	Asphalt
City of Brevard	Hospital Drive - Allison Creek Bridge	Hospital Drive - Allison Creek Bridge	Multi-Use Path	Asphalt
City of Brevard	Johnson	(Appletree - Fr. Broad)	Sidewalk	Concrete
City of Brevard	Johnson	(Jordan - Main)	Sidewalk	Concrete
City of Brevard	Johnson	(Main - Probart)	Sidewalk	Concrete
City of Brevard	Johnson	(Probart - Appletree)	Sidewalk	Concrete
City of Brevard	Jordan	(Broad - Gaston) (N)	Sidewalk	Concrete
City of Brevard	Jordan	(Broad - Gaston) (S)	Sidewalk	Concrete
City of Brevard	Jordan	(Caldwell - Broad) (N)	Sidewalk	Concrete
City of Brevard	Jordan	(Caldwell - Broad) (S)	Sidewalk	Concrete
City of Brevard	Jordan	(England - Caldwell) (N)	Sidewalk	Concrete
City of Brevard	Jordan	(England - Caldwell) (S)	Sidewalk	Concrete
City of Brevard	Jordan	(Gaston - Johnson) (N)	Sidewalk	Concrete
City of Brevard	Jordan	(Oaklawn - S. England St.) (N)	Sidewalk	Concrete
City of Brevard	Jordan	(Oaklawn - S. England St.) (S)	Sidewalk	Concrete
City of Brevard	King	Floyd - Caldwell	Sidewalk	Concrete
City of Brevard	King	Railroad - Floyd (N)	Sidewalk	Concrete
City of Brevard	King	Railroad - Floyd (S)	Sidewalk	Concrete
City of Brevard	King	Railroad - Floyd (S)	Sidewalk	Concrete
City of Brevard	Knight Ct.	Hopewell - Terminus	Sidewalk	Concrete
City of Brevard	London	Hopewell - Terminus	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Broad - Gaston) (N)	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Broad - Gaston) (S)	Sidewalk	Concrete

Owner_Name	Street	Section	Present Function	Material
City of Brevard	Main Street (E)	(Franklin - Park) (N)	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Franklin - Park) (S)	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Gaston - Johnson) (N)	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Gaston - Johnson) (S)	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Johnson - Rice St.) (N)	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Johnson - Rice St.) (S)	Sidewalk	Concrete
City of Brevard	Main Street (E)	(Rice - Franklin) (N)	Sidewalk	Concrete
City of Brevard	Main Street (E)	Franklin - Woodside (S)	Sidewalk	Concrete
City of Brevard	Main Street (E)	Park - Greenville Hwy / E. Main	Sidewalk	Concrete
City of Brevard	Main Street (E)	Park - Greenville Hwy / E. Main	Sidewalk	Concrete
City of Brevard	Main Street (E)	Rice - St. Phillips (S)	Sidewalk	Concrete
City of Brevard	Main Street (E)	St. Phillips - Franklin (S)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(Caldwell - Broad) (N)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(Caldwell - Broad) (S)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(England - Caldwell) (N)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(England - Caldwell) (S)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(Galloway - Oaklawn Ave)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(Galloway - Oaklawn Ave) (N)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(Oaklawn - S. England St.)	Sidewalk	Concrete
City of Brevard	Main Street (W)	(Oaklawn - S. England St.) (N)	Sidewalk	Concrete
City of Brevard	Maple	(Miner - Johnson) (E)	Sidewalk	Concrete
City of Brevard	Maple	Batson - Willow / Grove (W)	Sidewalk	Concrete
City of Brevard	Maple	Grove - Miner (E)	Sidewalk	Concrete
City of Brevard	Maple	Johnson - Gaston (E)	Sidewalk	Concrete
City of Brevard	Maple	Johnson - Gaston (W)	Sidewalk	Concrete
City of Brevard	Maple	Memory - Batson	Sidewalk	Concrete
City of Brevard	Maple	Miner - Johnson (W)	Sidewalk	Concrete
City of Brevard	Maple	Turnpike - Miner (W)	Sidewalk	Concrete
City of Brevard	Maple	Willow / Grove (E) - Grove (midblock)	Sidewalk	Concrete
City of Brevard	Maple	Willow / Grove (W) - Grove (midblock)	Sidewalk	Concrete
City of Brevard	Meadow View	Camp Sapphire - Straus Parkway (E)	Sidewalk	Concrete
City of Brevard	Meadow View	Camp Sapphire - Straus Parkway (W)	Sidewalk	Concrete
City of Brevard	Meadow View	Camp town - Camp Sapphire (E)	Sidewalk	Concrete
City of Brevard	Meadow View	Camp town - Camp Sapphire (W)	Sidewalk	Concrete
City of Brevard	Mills	(Cashiers Valley - Private Dr.)	Sidewalk	Concrete
City of Brevard	Mills	(Picklesminer Ave - Cashiers Valley)	Sidewalk	Concrete
City of Brevard	Mills	(Private Dr. - Galloway)	Sidewalk	Concrete
City of Brevard	Morgan	Broad - Gaston (N)	Sidewalk	Concrete
City of Brevard	Morgan	Broad - Gaston (S)	Sidewalk	Concrete
City of Brevard	Morgan	Caldwell - Broad (N)	Sidewalk	Concrete
City of Brevard	Morgan	Caldwell - Broad (S)	Sidewalk	Concrete

Owner_Name	Street	Section	Present Function	Material
City of Brevard	Morgan	Carver / Hamlin / Oaklawn - Duckworth (S)	Sidewalk	Concrete
City of Brevard	Morgan	Duckworth - Caldwell (N)	Sidewalk	Concrete
City of Brevard	Morgan	Duckworth - Caldwell (S)	Sidewalk	Concrete
City of Brevard	Neely Rd / Park View	(Greenville Hwy. - Private Dr.)	Sidewalk	Concrete
City of Brevard	Neely Rd / Park View	(Park - Fr. Broad)	Sidewalk	Concrete
City of Brevard	Neely Rd / Park View	Private Dr. - Park (NW)	Sidewalk	Concrete
City of Brevard	Neely Rd / Park View	Private Dr. - Park (SE)	Sidewalk	Concrete
City of Brevard	Oakdale Ln	(Caldwell - Broad)	Sidewalk	Concrete
City of Brevard	Oakdale Ln	Carver - Jenkins	Sidewalk	Concrete
City of Brevard	Oakdale Ln	Carver - Jenkins (N)	Sidewalk	Concrete
City of Brevard	Oakdale Ln	Duckworth Ave. - Orange	Sidewalk	Concrete
City of Brevard	Oakdale Ln	Hemphill Cr. - Duckworth Ave	Sidewalk	Concrete
City of Brevard	Oakdale Ln	Jenkins - Hemphill (S)	Sidewalk	Concrete
City of Brevard	Oakdale Ln	Orange - Caldwell	Sidewalk	Concrete
City of Brevard	Oaklawn Ave	(Main - Probart)	Sidewalk	Concrete
City of Brevard	Oaklawn Ave	(Morgan - Fortune)	Sidewalk	Concrete
City of Brevard	Oaklawn Ave	Fortune - Main	Sidewalk	Concrete
City of Brevard	Oaklawn Ave	Jordan - Morgan (W)	Sidewalk	Concrete
City of Brevard	Old Cottage Ln	Mid-Block - Cul-de-sac	Sidewalk	Concrete
City of Brevard	Old Cottage Ln	Straus Parkway - Midblock	Sidewalk	Concrete
City of Brevard	Park Ave	(Boxwood - Harold)	Sidewalk	Concrete
City of Brevard	Park Ave	(Harold - Parkview)	Sidewalk	Concrete
City of Brevard	Park Ave	(Main - Box Wood)	Sidewalk	Concrete
City of Brevard	Park Ave	Boxwood - Harold (N)	Sidewalk	Concrete
City of Brevard	Park Ave	Main - Box Wood (N)	Sidewalk	Concrete
City of Brevard	Pine Mountain	(Camp Town - Camp Straus)	Sidewalk	Concrete
Transylvania County Schools	Pisgah Forest Elementary		Recreation / Fitness	Asphalt
City of Brevard	Probart St.	(Caldwell - Broad) (N)	Sidewalk	Concrete
City of Brevard	Probart St.	(Caldwell - Broad) (S)	Sidewalk	Concrete
City of Brevard	Probart St.	(England - Caldwell) (N)	Sidewalk	Concrete
City of Brevard	Probart St.	(England - Caldwell) (S)	Sidewalk	Concrete
City of Brevard	Probart St.	(Railroad - Oaklawn) (N)	Sidewalk	Concrete
City of Brevard	Probart St.	(Railroad - Oaklawn) (S)	Sidewalk	Concrete
City of Brevard	Probart St.	Broad - Courthouse (S)	Sidewalk	Concrete
City of Brevard	Probart St.	Oaklawn - England) (N)	Sidewalk	Concrete
City of Brevard	Probart St.	Oaklawn - England) (S)	Sidewalk	Concrete
City of Brevard	Rail Road / Asheville Hwy Connector	Along Kings Creek	Recreation / Fitness	Mulch
City of Brevard	Rail Road Ave	Probart - King	Sidewalk	Concrete
City of Brevard	Sheffield Pl	Sheffield (N) - Cambridge	Sidewalk	Concrete
City of Brevard	Sheffield Pl	Sheffield (S) - Cambridge	Sidewalk	Concrete
City of Brevard	Silversteen		Sidewalk	Concrete

Owner_Name	Street	Section	Present Function	Material
City of Brevard	Silversteen		Sidewalk	Concrete
City of Brevard	Springhouse Trail		Sidewalk	Concrete
City of Brevard	Straus Parkway	(Meadow - Stone House)	Sidewalk	Concrete
City of Brevard	Straus Parkway	Asheville Hwy - Old Cottage Ln	Sidewalk	Concrete
City of Brevard	Straus Parkway	Pine Mtn - Meadow	Sidewalk	Concrete
City of Brevard	Straus Parkway	Private Drive - Asheville Hwy	Sidewalk	Concrete
City of Brevard	Straus Parkway	Stone House - Private Drive	Sidewalk	Concrete
City of Brevard	Throwbridge	Elm Bend - Manor (W))	Sidewalk	Concrete
City of Brevard	Throwbridge	Elm Bend - Sapphire (E)	Sidewalk	Concrete
City of Brevard	Throwbridge	Manor - Greenville Hwy (W)	Sidewalk	Concrete
City of Brevard	Throwbridge	Sapphire - Greenville Hwy (E)	Sidewalk	Concrete
City of Brevard	Timber Trace		Sidewalk	Concrete
City of Brevard	Tutor	Danbury - Terminus	Sidewalk	Concrete
Brevard Housing Authority	Unity		Sidewalk	Concrete
Brevard Housing Authority	Unity		Sidewalk	Concrete
Brevard Housing Authority	Unity		Sidewalk	Concrete
Brevard Housing Authority	Unity		Sidewalk	Concrete
Brevard Housing Authority	Unity		Sidewalk	Concrete
Brevard Housing Authority	Unity		Sidewalk	Concrete
Brevard Housing Authority	Unity		Sidewalk	Concrete
City of Brevard	Whitmire	(Palmer - Tinsley)	Sidewalk	Concrete
City of Brevard	Whitmire	(Railroad - Salem)	Sidewalk	Concrete
City of Brevard	Whitmire	(Tinsley - Railroad)	Sidewalk	Concrete
City of Brevard	Whitmire	College Station - Caldwell (N)	Sidewalk	Concrete
City of Brevard	Whitmire	Kingsmill / Burrel Mtn - Palmer	Sidewalk	Concrete
City of Brevard	Whitmire	Railroad - Salem (S)	Sidewalk	Concrete
City of Brevard	Whitmire	Salem - Caldwell (S)	Sidewalk	Concrete
City of Brevard	Whitmire	Salem - College Station	Sidewalk	Concrete
Brevard Housing Authority			Building Access	Concrete
Transylvania County Public Schools			Building Access	Concrete
Transylvania County Public Schools			Building Access	Concrete
Transylvania County Schools			Building Access	Concrete
Transylvania County Schools			Building Access	Concrete

Street	Section	Width	Surface Condition	Vegetation	Debris
Allison Creek Bridge - City Softball Fields	Allison Creek Bridge - City Softball Fields	10.00	excellent	excellent	excellent
Asheville Hwy	(Fisher - Green Acre	5.00	excellent	excellent	excellent
Asheville Hwy	(K-Mart - Fisher	5.00	excellent	excellent	excellent
Asheville Hwy	Allison-Straus Parkway	10.00	excellent	excellent	excellent
Asheville Hwy	Asheville Hwy at Enmark	5.00	excellent	excellent	excellent
Asheville Hwy	Caldwell - K-Mart	5.00	fair	excellent	excellent
Asheville Hwy	Caldwell / Broad / College - Food Lion	0.00			
Asheville Hwy	Deerlake - Osborne	5.00	fair	poor	excellent
Asheville Hwy	Green Acre - Jackson / Chestnut	5.00	good	good	excellent
Asheville Hwy	Green Acre - Old 64	5.00	good	fair	excellent
Asheville Hwy	Jackson / Chestnut - Deer Lake (N)	5.00	poor	fair	good
Asheville Hwy	Jackson / Chestnut - Deer Lake (S)	5.00	poor	fair	good
Asheville Hwy	Osborne - Allison	5.00	fair	excellent	excellent
Asheville Hwy	Straus Parkway - Hospital Drive	10.00	excellent	excellent	excellent
Asheville Hwy / Ecusta	At Lowes	0.00			
Blueberry Hill	Blueberry Hill	4.00	excellent	excellent	excellent
Brevard Elementary		0.00			
Brevard High School		0.00			
Broad N	Appletree - French Broad (E)	7.00	poor	excellent	fair
Broad N	Appletree - French Broad (W)	5.00	poor	excellent	fair
Broad N	Brevard College - Brevard College / Caldwell (E)	5.00	fair	excellent	fair
Broad N	French Broad - Brevard College (E)	5.00	fair	excellent	fair
Broad N	Kellys - Appletree (E)	7.00	fair	excellent	fair
Broad N	Kellys - Appletree / Professional (W)	5.00	poor	excellent	fair
Broad N	Main - Probart (E)	7.00	fair	excellent	fair
Broad N	Main - Probart (W)	7.00	fair	excellent	fair
Broad N	Probart - Kellys (E)	7.00	fair	excellent	fair
Broad N	Probart - Kellys (W)	7.00	good	excellent	fair
Broad S	Country Club - Miner (E)	5.00	fair	excellent	fair
Broad S	Jordan - Main (E)	5.00	good	excellent	excellent
Broad S	Jordan - Main (W)	5.00	excellent	excellent	excellent
Broad S	Miner - Varsity (E)	5.00	fair	excellent	fair
Broad S	Miner - Varsity (W)	5.00	fair	excellent	fair
Broad S	Morgan - Jordan (E)	7.00	good	excellent	fair
Broad S	Morgan - Jordan (W)	5.00	good	excellent	excellent
Broad S	Oakdale - Morgan (E)	5.00	excellent	excellent	excellent
Broad S	Oakdale - Morgan (W)	5.00	excellent	excellent	excellent
Broad S	Rosman - Miner (W)	5.00	fair	excellent	fair
Broad S	Varsity - Oakdale (E)	5.00	excellent	excellent	excellent
Broad S	Varsity - Oakdale (W)	5.00	excellent	excellent	excellent
Caldwell	Colwell - Wa Bracken (W)	4.00	poor	poor	poor

Street	Section	Width	Surface Condition	Vegetation	Debris
Caldwell	Food Lion - N Broad / College / Asheville Hwy (W)	5.00	excellent	excellent\	excellent
Caldwell	French Broad - Methodist (E)	5.00	excellent	excellent	excellent
Caldwell	Jordan - Main (E)	5.00	poor	excellent	excellent
Caldwell	Jordan - Main (W)	5.00	excellent	excellent	excellent
Caldwell	King / Kelly's Aly - Professional Aly (E)	5.00	excellent	excellent	excellent
Caldwell	Main - Probart (E)	5.00	poor	excellent	excellent
Caldwell	Main - Probart (W)	5.00	excellent	excellent	excellent
Caldwell	Morgan - Jordan (E)	5.00	good	excellent	excellent
Caldwell	Morgan - Jordan (W)	4.00	fair	fair	good
Caldwell	Oakdale - Morgan (E)	4.00	poor	fair	good
Caldwell	Oakdale - Morgan (W)	4.00	poor	fair	good
Caldwell	Probart - Kelly's Aly / King (E)	5.00	excellent	excellent	excellent
Caldwell	Professional Aly - French Broad (E)	5.00	excellent	excellent	excellent
Caldwell	Silverstein - Oakdale (E)	4.00	poor	fair	fair
Caldwell	Silverstein - Oakdale (W)	4.00	poor	fair	fair
Caldwell	WA Bracken - Silverstein (W)	4.00	poor	poor	poor
Caldwell	Whitmire - Food Lion (W)	5.00	excellent	excellent	excellent
Cambridge Dr.	Cambridge (E) - Deer Lake	4.00	excellent	fair	excellent
Cambridge Dr.	Cambridge (N) - Canterbury Trl	4.00	excellent	fair	excellent
Cambridge Dr.	Cambridge (S) - Canterbury	4.00	excellent	fair	excellent
Cambridge Dr.	Cambridge (W) - Deer Lake	4.00	excellent	fair	excellent
Cambridge Dr.	Canterbury - Sheffield	4.00	excellent	fair	excellent
Cambridge Dr.	Canterbury - Sheffield	4.00	excellent	fair	excellent
Cambridge Dr.	Deer Lake - Sheffield	4.00	excellent	fair	excellent
Cambridge Dr.	Deer Lake - Sheffield	4.00	excellent	fair	excellent
Camp Sapphire	Meadow View - Camp Town	4.00	excellent	excellent	excellent
Camp Town	Camp Sapphire - Meadow View	4.00	excellent	excellent	excellent
Camp Town	Camp Sapphire / Blueberry - Pine Mountain	4.00	excellent	excellent	excellent
Camp Town	Meadow View - Stone House	4.00	excellent	excellent	excellent
Camp Town	Stone House - END	4.00	excellent	excellent	excellent
Canterbury	Canterbury (E) - Cambridge	4.00	excellent	fair	excellent
Canterbury	Canterbury (W) - Cambridge	4.00	excellent	fair	excellent
Carver		4.00	fair	fair	excellent
Carver		4.00	fair	fair	excellent
Cashiers Valley		4.00	good	good	excellent
Cashiers Valley		4.00	fair	poor	fair
City Softball Fields - Ecusta Road	City Softball Fields - Ecusta Road	10.00	excellent	excellent	excellent
College Station	Caldwell - Food Lion	0.00			
Country Club	(Southview - Turnpike)	4.00	excellent	good	excellent
Country Club	(Turnpike - Monroe)	4.00	excellent	good	excellent
Country Club	Ashworth - Southview	4.00	excellent	good	excellent

Street	Section	Width	Surface Condition	Vegetation	Debris
Country Club	End of Sidewalk - Bridge at H.S.	0.00			
Country Club	Gallimore - Brevard High	4.00	excellent	excellent	excellent
Country Club	Gallimore - Gillespie	4.00	excellent	good	excellent
Country Club	Gillespie - Ashworth	4.00	excellent	good	excellent
Country Club	Gillispie Circle - Hayes	4.00	fair	good	fair
Country Club	Hayes - South Broad	4.00	excellent	excellent	good
Country Club	Monroe - Gillispie Circle	4.00	fair	good	fair
Danbury	Hopewell - Tutor	0.00			
Danbury	Terminus - Hopewell	0.00			
Danbury	Tutor - Terminus	0.00			
Deer Lake	West terminal - Stone Dr	5.00	excellent	excellent	excellent
Deer Run	Deer Lake - Chipmunk	4.00	excellent	excellent	excellent
Deer Run	Deer Lake - Chipmunk	4.00	excellent	excellent	excellent
Deerlake	(Stone - Fox Cross)	5.00	good	excellent	excellent
Deerlake	Deer Run - Broad	5.00	excellent	excellent	excellent
Deerlake	Eldridge - Broad	5.00	excellent	excellent	excellent
Deerlake	Fox Cross - Deer Run	5.00	good	excellent	excellent
East Main St. Ext.	Wilson / Main / Greenville - Terminus	4.00	fair	good	good
Elm Bend	Greenville Hwy - Throwbridge / Magnolia	4.00	poor	good	good
Elm Bend	Throwbridge / Magnolia - Sapphire Manor	4.00	poor	good	good
England	(Main - Probart)	5.00	excellent	excellent	excellent
English Hills	Picklesimer Ave. - English Hills Project	5.00	excellent	excellent	excellent
Fisher	(between Montview intersections)	5.00	excellent	excellent	excellent
Fisher	(Montview - Broad)	5.00	excellent	excellent	excellent
Fisher	(Poplar - Montview)	5.00	excellent	excellent	excellent
Fisher	Brevard Middle School	0.00			
Franklin	Lakeview - French Broad (N)	5.00	fair	poor	good
Franklin	Lakeview - Lakeview (S)	5.00	excellent	excellent	excellent
Franklin	Main - Lakeview (N)	5.00	fair	poor	poor
Franklin	Main - Lakeview (S)	5.00	fair	poor	good
French Broad	(Appletree - Franklin)	5.00	excellent	good	excellent
French Broad	(Broad - Johnson)	4.00	good	excellent	excellent
French Broad	(Caldwell - Broad)	4.00	fair	excellent	good
French Broad	(Franklin - Hilt)	5.00	excellent	good	good
French Broad	(Johnson - Appletree)	5.00	excellent	excellent	fair
French Broad	(Railroad - Salem)	5.00	poor	excellent	poor
French Broad	(Railroad - Salem)	5.00	poor	excellent	poor
French Broad	(Salem - Caldwell)	5.00	poor	poor	good
French Broad	(Salem - Caldwell)	5.00	poor	poor	good
French Broad	Hilt - Woodlawn	5.00	excellent	fair	good
French Broad	Woodlawn - Parkview	5.00	excellent	fair	fair

Street	Section	Width	Surface Condition	Vegetation	Debris
Gaston	(Jordan - Main)	4.00	fair	good	excellent
Gaston	(Jordan - Main)	4.00	fair	good	excellent
Gaston	(Main - Probart)	4.00	fair	good	excellent
Gaston	(Main - Probart)	4.00	fair	good	excellent
Gaston	(Maple - Morgan)	4.00	fair	good	excellent
Gaston	(Maple - Morgan)	4.00	fair	good	excellent
Gaston	(Morgan - Jordan)	4.00	fair	good	excellent
Gaston	(Morgan - Jordan)	4.00	fair	good	excellent
Greenville Hwy	(Elm Bend - Hrowbridge)	4.00	good	excellent	fair
Greenville Hwy	(Hrowbridge - sidewalk to Brevard Elementary)	4.00	poor	good	fair
Greenville Hwy	E. Main - Elm Bend	5.00	poor	poor	poor
Greenville Hwy	sidewalk to Brevard Elementary	5.00	excellent	excellent	excellent
Hillview Ave	(Private Dr. - Picklesminer Ave)	4.00	fair	poor	fair
Hillview Ave		4.00	fair	good	good
Hillview Ave		4.00	fair	good	good
Hopewell	Knight - Danbury	0.00			
Hopewell	London - Knight	0.00			
Hopewell	Osborne - London	0.00			
Hospital Drive	Hospital Drive - Mid-Block	10.00	excellent	excellent	excellent
Hospital Drive - Allison Creek Bridge	Hospital Drive - Allison Creek Bridge	10.00	excellent	excellent	excellent
Johnson	(Appletree - Fr. Broad)	4.00	excellent	excellent	excellent
Johnson	(Jordan - Main)	5.00	fair	excellent	good
Johnson	(Main - Probart)	4.00	excellent	excellent	excellent
Johnson	(Probart - Appletree)	4.00	good	excellent	fair
Jordan	(Broad - Gaston) (N)	5.00	fair	excellent	excellent
Jordan	(Broad - Gaston) (S)	5.00	fair	excellent	excellent
Jordan	(Caldwell - Broad) (N)	5.00	poor	excellent	excellent
Jordan	(Caldwell - Broad) (S)	5.00	poor	excellent	excellent
Jordan	(England - Caldwell) (N)	5.00	excellent	excellent	excellent
Jordan	(England - Caldwell) (S)	5.00	excellent	excellent	excellent
Jordan	(Gaston - Johnson) (N)	5.00			
Jordan	(Oaklawn - S. England St.) (N)	5.00	fair	good	excellent
Jordan	(Oaklawn - S. England St.) (S)	5.00	fair	good	excellent
King	Floyd - Caldwell	5.00	poor	fair	excellent
King	Railroad - Floyd (N)	5.00	poor	fair	excellent
King	Railroad - Floyd (S)	0.00			
King	Railroad - Floyd (S)	0.00			
Knight Ct.	Hopewell - Terminus	5.00	excellent	excellent	excellent
London	Hopewell - Terminus	5.00	excellent	excellent	excellent
Main Street (E)	(Broad - Gaston) (N)	0.00	excellent	excellent	excellent
Main Street (E)	(Broad - Gaston) (S)	0.00	excellent	excellent	excellent

Street	Section	Width	Surface Condition	Vegetation	Debris
Main Street (E)	(Franklin - Park) (N)	5.00	good	good	poor
Main Street (E)	(Franklin - Park) (S)	5.00	good	good	poor
Main Street (E)	(Gaston - Johnson) (N)	0.00	excellent	excellent	excellent
Main Street (E)	(Gaston - Johnson) (S)	0.00	excellent	excellent	excellent
Main Street (E)	(Johnson - Rice St.) (N)	5.00	fair	excellent	excellent
Main Street (E)	(Johnson - Rice St.) (S)	5.00	fair	excellent	excellent
Main Street (E)	(Rice - Franklin) (N)	5.00	fair	fair	fair
Main Street (E)	Franklin - Woodside (S)	0.00			
Main Street (E)	Park - Greenville Hwy / E. Main	5.00	poor	fair	fair
Main Street (E)	Park - Greenville Hwy / E. Main	5.00	poor	poor	poor
Main Street (E)	Rice - St. Phillips (S)	5.00	fair	fair	fair
Main Street (E)	St. Phillips - Franklin (S)	5.00	fair	fair	fair
Main Street (W)	(Caldwell - Broad) (N)	0.00	good	excellent	good
Main Street (W)	(Caldwell - Broad) (S)	0.00	excellent	excellent	excellent
Main Street (W)	(England - Caldwell) (N)	0.00			
Main Street (W)	(England - Caldwell) (S)	0.00	excellent	excellent	excellent
Main Street (W)	(Galloway - Oaklawn Ave)	0.00	excellent	good	good
Main Street (W)	(Galloway - Oaklawn Ave) (N)	0.00	excellent	good	good
Main Street (W)	(Oaklawn - S. England St.)	5.00	excellent	excellent	excellent
Main Street (W)	(Oaklawn - S. England St.) (N)	0.00	excellent	excellent	excellent
Maple	(Miner - Johnson (E)	4.00	fair	good	fair
Maple	Batson - Willow / Grove (W)	4.00	poor	good	excellent
Maple	Grove - Miner (E)	4.00	fair	good	excellent
Maple	Johnson - Gaston (E)	4.00	fair	good	fair
Maple	Johnson - Gaston (W)	4.00	fair	good	fair
Maple	Memory - Batson	4.00	poor	good	excellent
Maple	Miner - Johnson (W)	4.00	fair	good	fair
Maple	Turnpike - Miner (W)	4.00	fair	good	excellent
Maple	Willow / Grove (E) - Grove (midblock)	4.00	fair	good	excellent
Maple	Willow / Grove (W) - Grove (midblock)	4.00	excellent	excellent	excellent
Meadow View	Camp Sapphire - Straus Parkway (E)	4.00	excellent	excellent	excellent
Meadow View	Camp Sapphire - Straus Parkway (W)	4.00	excellent	excellent	excellent
Meadow View	Camp town - Camp Sapphire (E)	4.00	excellent	excellent	excellent
Meadow View	Camp town - Camp Sapphire (W)	4.00	excellent	excellent	excellent
Mills	(Cashiers Valley - Private Dr.)	4.00	excellent	good	excellent
Mills	(Picklesminer Ave - Cashiers Valley)	4.00	poor	poor	excellent
Mills	(Private Dr. - Galloway)	4.00	poor	poor	excellent
Morgan	Broad - Gaston (N)	5.00	fair	excellent	excellent
Morgan	Broad - Gaston (S)	5.00	excellent	excellent	excellent
Morgan	Caldwell - Broad (N)	5.00	fair	excellent	excellent
Morgan	Caldwell - Broad (S)	5.00	fair	excellent	excellent

Street	Section	Width	Surface Condition	Vegetation	Debris
Morgan	Carver / Hamlin / Oaklawn - Duckworth (S)	5.00	fair	excellent	excellent
Morgan	Duckworth - Caldwell (N)	5.00	fair	excellent	excellent
Morgan	Duckworth - Caldwell (S)	5.00	fair	excellent	excellent
Neely Rd / Park View	(Greenville Hwy. - Private Dr.)	5.00	good	good	good
Neely Rd / Park View	(Park - Fr. Broad)	5.00	good	good	excellent
Neely Rd / Park View	Private Dr. - Park (NW)	4.00	poor	fair	poor
Neely Rd / Park View	Private Dr. - Park (SE)	4.00	poor	fair	poor
Oakdale Ln	(Caldwell - Broad)	4.00	fair	good	fair
Oakdale Ln	Carver - Jenkins	4.00	fair	poor	excellent
Oakdale Ln	Carver - Jenkins (N)	4.00	fair	poor	excellent
Oakdale Ln	Duckworth Ave. - Orange	4.00	fair	fair	good
Oakdale Ln	Hemphill Cr. - Duckworth Ave	4.00	good	poor	poor
Oakdale Ln	Jenkins - Hemphill (S)	4.00	fair	poor	excellent
Oakdale Ln	Orange - Caldwell	4.00	fair	fair	good
Oaklawn Ave	(Main - Probart)	5.00	poor	excellent	poor
Oaklawn Ave	(Morgan - Fortune)	5.00	excellent	fair	excellent
Oaklawn Ave	Fortune - Main	5.00	excellent	fair	excellent
Oaklawn Ave	Jordan - Morgan (W)	0.00			
Old Cottage Ln	Mid-Block - Cul-de-sac	5.00	excellent	excellent	excellent
Old Cottage Ln	Straus Parkway - Midblock	5.00	excellent	excellent	excellent
Park Ave	(Boxwood - Harold)	4.00	poor	fair	good
Park Ave	(Harold - Parkview)	4.00	poor	poor	fair
Park Ave	(Main - Box Wood)	5.00	good	good	fair
Park Ave	Boxwood - Harold (N)	4.00	poor	fair	good
Park Ave	Main - Box Wood (N)	5.00	good	good	fair
Pine Mountain	(Camp Town - Camp Straus)	4.00	excellent	excellent	excellent
Pisgah Forest Elementary		0.00			
Probart St.	(Caldwell - Broad) (N)	5.00	excellent	fair	good
Probart St.	(Caldwell - Broad) (S)	5.00	excellent	poor	good
Probart St.	(England - Caldwell) (N)	5.00	excellent	excellent	poor
Probart St.	(England - Caldwell) (S)	5.00	excellent	poor	poor
Probart St.	(Railroad - Oaklawn) (N)	5.00	excellent	poor	poor
Probart St.	(Railroad - Oaklawn) (S)	5.00	excellent	poor	poor
Probart St.	Broad - Courthouse (S)	5.00	excellent	excellent	excellent
Probart St.	Oaklawn - England) (N)	5.00	excellent	poor	poor
Probart St.	Oaklawn - England) (S)	5.00	excellent	poor	poor
Rail Road / Asheville Hwy Connector	Along Kings Creek	0.00			
Rail Road Ave	Probart - King	5.00	poor	poor	poor
Sheffield Pl	Sheffield (N) - Cambridge	4.00	excellent	fair	excellent
Sheffield Pl	Sheffield (S) - Cambridge	4.00	excellent	fair	excellent
Silversteen		5.00	Excellent	Excellent	Excellent

Street	Section	Width	Surface Condition	Vegetation	Debris
Silversteen		5.00	Excellent	Excellent	Excellent
Springhouse Trail		4.00	excellent	excellent	excellent
Straus Parkway	(Meadow - Stone House)	5.00	excellent	excellent	excellent
Straus Parkway	Asheville Hwy - Old Cottage Ln	5.00	excellent	excellent	excellent
Straus Parkway	Pine Mtn - Meadow	4.00	excellent	excellent	excellent
Straus Parkway	Private Drive - Asheville Hwy	5.00	excellent	excellent	excellent
Straus Parkway	Stone House - Private Drive	5.00	excellent	excellent	excellent
Throwbridge	Elm Bend - Manor (W))	4.00	fair	good	good
Throwbridge	Elm Bend - Sapphire (E)	4.00	fair	good	good
Throwbridge	Manor - Greenville Hwy (W)	4.00	fair	good	good
Throwbridge	Sapphire - Greenville Hwy (E)	4.00	fair	good	good
Timber Trace		4.00	excellent	excellent	excellent
Tutor	Danbury - Terminus	0.00	excellent	excellent	excellent
Unity		3.00	good	good	excellent
Unity		4.00	good	excellent	excellent
Unity		4.00	good	excellent	excellent
Unity		4.00	good	excellent	excellent
Unity		0.00			
Unity		4.00	good	excellent	excellent
Unity		4.00	good	excellent	excellent
Unity		0.00			
Whitmire	(Palmer - Tinsley)	4.00	poor	poor	fair
Whitmire	(Railroad - Salem)	4.00	poor	poor	fair
Whitmire	(Tinsley - Railroad)	4.00	poor	poor	fair
Whitmire	College Station - Caldwell (N)	4.00	poor	poor	fair
Whitmire	Kingsmill / Burrel Mtn - Palmer	4.00	poor	poor	fair
Whitmire	Railroad - Salem (S)	4.00	poor	poor	fair
Whitmire	Salem - Caldwell (S)	4.00	poor	poor	fair
Whitmire	Salem - College Station	4.00	poor	poor	fair
		0.00			
		0.00			
		0.00			
		0.00			
		0.00			

Street	Section	Obstruction	Traffic Separation	ADA Curb Condition
Allison Creek Bridge - City Softball Fields	Allison Creek Bridge - City Softball Fields	excellent	poor	excellent
Asheville Hwy	(Fisher - Green Acre	fair	poor	poor
Asheville Hwy	(K-Mart - Fisher	good	poor	excellent
Asheville Hwy	Allison-Straus Parkway	excellent	poor	excellent
Asheville Hwy	Asheville Hwy at Enmark	excellent	excellent	excellent
Asheville Hwy	Caldwell - K-Mart	good	poor	good
Asheville Hwy	Caldwell / Broad / College - Food Lion			
Asheville Hwy	Deerlake - Osborne	excellent	excellent	good
Asheville Hwy	Green Acre - Jackson / Chestnut	excellent	poor	poor
Asheville Hwy	Green Acre - Old 64	excellent	excellent	excellent
Asheville Hwy	Jackson / Chestnut - Deer Lake (N)	excellent	poor	fair
Asheville Hwy	Jackson / Chestnut - Deer Lake (S)	excellent	poor	fair
Asheville Hwy	Osborne - Allison	excellent	excellent	poor
Asheville Hwy	Straus Parkway - Hospital Drive	excellent	poor	excellent
Asheville Hwy / Ecusta	At Lowes			
Blueberry Hill	Blueberry Hill	excellent	excellent	excellent
Brevard Elementary				
Brevard High School				
Broad N	Appletree - French Broad (E)	fair	fair	fair
Broad N	Appletree - French Broad (W)	fair	fair	fair
Broad N	Brevard College - Brevard College / Caldwell (E)	fair	fair	fair
Broad N	French Broad - Brevard College (E)	fair	fair	fair
Broad N	Kellys - Appletree (E)	fair	fair	fair
Broad N	Kellys - Appletree / Professional (W)	fair	fair	fair
Broad N	Main - Probart (E)	fair	fair	fair
Broad N	Main - Probart (W)	fair	fair	fair
Broad N	Probart - Kellys (E)	fair	fair	fair
Broad N	Probart - Kellys (W)	fair	fair	fair
Broad S	Country Club - Miner (E)	excellent	good	
Broad S	Jordan - Main (E)	excellent	good	good
Broad S	Jordan - Main (W)	excellent	excellent	good
Broad S	Miner - Varsity (E)	excellent	good	
Broad S	Miner - Varsity (W)	excellent	good	
Broad S	Morgan - Jordan (E)	fair	fair	fair
Broad S	Morgan - Jordan (W)	excellent	good	good
Broad S	Oakdale - Morgan (E)	excellent	excellent	good
Broad S	Oakdale - Morgan (W)	excellent	excellent	good
Broad S	Rosman - Miner (W)	excellent	good	
Broad S	Varsity - Oakdale (E)	excellent	good	good
Broad S	Varsity - Oakdale (W)	excellent	good	good
Caldwell	Colwell - Wa Bracken (W)	excellent	fair	

Street	Section	Obstruction	Traffic Separation	ADA Curb Condition
Caldwell	Food Lion - N Broad / College / Asheville Hwy (W)	excellent	excellent	excellent
Caldwell	French Broad - Methodist (E)	excellent	excellent	excellent
Caldwell	Jordan - Main (E)	excellent	poor	good
Caldwell	Jordan - Main (W)	excellent	poor	excellent
Caldwell	King / Kelly's Aly - Professional Aly (E)	excellent	excellent	excellent
Caldwell	Main - Probart (E)	excellent	poor	good
Caldwell	Main - Probart (W)	excellent	excellent	fair
Caldwell	Morgan - Jordan (E)	excellent	excellent	good
Caldwell	Morgan - Jordan (W)	excellent	excellent	fair
Caldwell	Oakdale - Morgan (E)	excellent	good	
Caldwell	Oakdale - Morgan (W)	excellent	good	
Caldwell	Probart - Kelly's Aly / King (E)	excellent	excellent	fai
Caldwell	Professional Aly - French Broad (E)	excellent	excellent	excellent
Caldwell	Silverstein - Oakdale (E)	excellent	good	
Caldwell	Silverstein - Oakdale (W)	excellent	good	
Caldwell	WA Bracken - Silverstein (W)	excellent	fair	
Caldwell	Whitmire - Food Lion (W)	excellent	excellent	excellent
Cambridge Dr.	Cambridge (E) - Deer Lake	excellent	fair	excellent
Cambridge Dr.	Cambridge (N) - Canterbury Trl	excellent	fair	excellent
Cambridge Dr.	Cambridge (S) - Canterbury	excellent	fair	excellent
Cambridge Dr.	Cambridge (W) - Deer Lake	excellent	fair	excellent
Cambridge Dr.	Canterbury - Sheffield	excellent	fair	excellent
Cambridge Dr.	Canterbury - Sheffield	excellent	fair	excellent
Cambridge Dr.	Deer Lake - Sheffield	excellent	fair	excellent
Cambridge Dr.	Deer Lake - Sheffield	excellent	fair	excellent
Camp Sapphire	Meadow View - Camp Town	poor	excellent	excellent
Camp Town	Camp Sapphire - Meadow View	excellent	excellent	excellent
Camp Town	Camp Sapphire / Blueberry - Pine Mountain	excellent	excellent	excellent
Camp Town	Meadow View - Stone House	excellent	excellent	excellent
Camp Town	Stone House - END	poor	excellent	excellent
Canterbury	Canterbury (E) - Cambridge	excellent	fair	excellent
Canterbury	Canterbury (W) - Cambridge	excellent	fair	excellent
Carver		excellent	poor	
Carver		excellent	poor	
Cashiers Valley		excellent	poor	
Cashiers Valley		excellent	poor	
City Softball Fields - Ecusta Road	City Softball Fields - Ecusta Road	excellent	poor	excellent
College Station	Caldwell - Food Lion			
Country Club	(Southview - Turnpike)	excellent	poor	
Country Club	(Turnpike - Monroe)	excellent	poor	
Country Club	Ashworth - Southview	excellent	poor	

Street	Section	Obstruction	Traffic Separation	ADA Curb Condition
Country Club	End of Sidewalk - Bridge at H.S.			
Country Club	Gallimore - Brevard High	excellent	poor	
Country Club	Gallimore - Gillespie	excellent	poor	
Country Club	Gillespie - Ashworth	excellent	poor	
Country Club	Gillispie Circle - Hayes	excellent	good	
Country Club	Hayes - South Broad	excellent	excellent	
Country Club	Monroe - Gillispie Circle	excellent	good	
Danbury	Hopewell - Tutor			
Danbury	Terminus - Hopewell			
Danbury	Tutor - Terminus			
Deer Lake	West terminal - Stone Dr	excellent	poor	excellent
Deer Run	Deer Lake - Chipmunk	excellent	poor	fair
Deer Run	Deer Lake - Chipmunk	excellent	poor	fair
Deerlake	(Stone - Fox Cross)	excellent	poor	fair
Deerlake	Deer Run - Broad	excellent	poor	poor
Deerlake	Eldridge - Broad	excellent	excellent	excellent
Deerlake	Fox Cross - Deer Run	excellent	poor	fair
East Main St. Ext.	Wilson / Main / Greenville - Terminus	excellent	good	poor
Elm Bend	Greenville Hwy - Throwbridge / Magnolia	excellent	excellent	fair
Elm Bend	Throwbridge / Magnolia - Sapphire Manor	excellent	excellent	poor
England	(Main - Probart)	excellent	poor	excellent
English Hills	Picklesimer Ave. - English Hills Project	excellent	good	good
Fisher	(between Montview intersections)	excellent	poor	excellent
Fisher	(Montview - Broad)	excellent	poor	excellent
Fisher	(Poplar - Montview)	excellent	poor	excellent
Fisher	Brevard Middle School			
Franklin	Lakeview - French Broad (N)	fair	good	fair
Franklin	Lakeview - Lakeview (S)	poor	good	poor
Franklin	Main - Lakeview (N)	fair	good	fair
Franklin	Main - Lakeview (S)	poor	fair	fair
French Broad	(Appletree - Franklin)	good	excellent	fair
French Broad	(Broad - Johnson)	excellent	excellent	fair
French Broad	(Caldwell - Broad)	poor	poor	poor
French Broad	(Franklin - Hilt)	excellent	poor	fair
French Broad	(Johnson - Appletree)	excellent	excellent	fair
French Broad	(Railroad - Salem)	poor	excellent	poor
French Broad	(Railroad - Salem)	poor	excellent	poor
French Broad	(Salem - Caldwell)	poor	excellent	poor
French Broad	(Salem - Caldwell)	poor	excellent	poor
French Broad	Hilt - Woodlawn	excellent	poor	poor
French Broad	Woodlawn - Parkview	excellent	poor	poor

Street	Section	Obstruction	Traffic Separation	ADA Curb Condition
Gaston	(Jordan - Main)	excellent	fair	good
Gaston	(Jordan - Main)	excellent	fair	good
Gaston	(Main - Probart)	excellent	fair	good
Gaston	(Main - Probart)	excellent	fair	good
Gaston	(Maple - Morgan)	excellent	fair	good
Gaston	(Maple - Morgan)	excellent	fair	good
Gaston	(Morgan - Jordan)	excellent	fair	good
Gaston	(Morgan - Jordan)	excellent	fair	good
Greenville Hwy	(Elm Bend - Hrowbridge)	excellent	excellent	
Greenville Hwy	(Hrowbridge - sidewalk to Brevard Elementary)	excellent	excellent	
Greenville Hwy	E. Main - Elm Bend	excellent	fair	
Greenville Hwy	sidewalk to Brevard Elementary	excellent	excellent	
Hillview Ave	(Private Dr. - Picklesminer Ave)	excellent	poor	
Hillview Ave		excellent	poor	
Hillview Ave		excellent	poor	
Hopewell	Knight - Danbury			
Hopewell	London - Knight			
Hopewell	Osborne - London			
Hospital Drive	Hospital Drive - Mid-Block	excellent	poor	excellent
Hospital Drive - Allison Creek Bridge	Hospital Drive - Allison Creek Bridge	excellent	poor	excellent
Johnson	(Appletree - Fr. Broad)	excellent	excellent	fair
Johnson	(Jordan - Main)	excellent	poor	fair
Johnson	(Main - Probart)	excellent	excellent	
Johnson	(Probart - Appletree)	excellent	excellent	fair
Jordan	(Broad - Gaston) (N)	excellent	fair	fair
Jordan	(Broad - Gaston) (S)	excellent	fair	fair
Jordan	(Caldwell - Broad) (N)	excellent	fair	fair
Jordan	(Caldwell - Broad) (S)	excellent	poor	fair
Jordan	(England - Caldwell) (N)	excellent	fair	good
Jordan	(England - Caldwell) (S)	excellent	fair	good
Jordan	(Gaston - Johnson) (N)			
Jordan	(Oaklawn - S. England St.) (N)	excellent	fair	good
Jordan	(Oaklawn - S. England St.) (S)	excellent	fair	good
King	Floyd - Caldwell	excellent	poor	poor
King	Railroad - Floyd (N)	excellent	poor	poor
King	Railroad - Floyd (S)			
King	Railroad - Floyd (S)			
Knight Ct.	Hopewell - Terminus	excellent	excellent	fair
London	Hopewell - Terminus	excellent	excellent	fair
Main Street (E)	(Broad - Gaston) (N)	excellent	excellent	good
Main Street (E)	(Broad - Gaston) (S)	excellent	excellent	good

Street	Section	Obstruction	Traffic Separation	ADA Curb Condition
Main Street (E)	(Franklin - Park) (N)	excellent	good	good
Main Street (E)	(Franklin - Park) (S)	excellent	good	good
Main Street (E)	(Gaston - Johnson) (N)	excellent	excellent	good
Main Street (E)	(Gaston - Johnson) (S)	excellent	excellent	good
Main Street (E)	(Johnson - Rice St.) (N)	excellent	excellent	
Main Street (E)	(Johnson - Rice St.) (S)	excellent	excellent	good
Main Street (E)	(Rice - Franklin) (N)	excellent	excellent	fair
Main Street (E)	Franklin - Woodside (S)			fair
Main Street (E)	Park - Greenville Hwy / E. Main	fair	good	good
Main Street (E)	Park - Greenville Hwy / E. Main	excellent	fair	good
Main Street (E)	Rice - St. Phillips (S)	excellent	excellent	good
Main Street (E)	St. Phillips - Franklin (S)	excellent	excellent	fair
Main Street (W)	(Caldwell - Broad) (N)	fair		excellent
Main Street (W)	(Caldwell - Broad) (S)	excellent	excellent	good
Main Street (W)	(England - Caldwell) (N)			
Main Street (W)	(England - Caldwell) (S)	excellent	excellent	good
Main Street (W)	(Galloway - Oaklawn Ave)	fair	good	poor
Main Street (W)	(Galloway - Oaklawn Ave) (N)	fair	good	poor
Main Street (W)	(Oaklawn - S. England St.)	excellent	excellent	excellent
Main Street (W)	(Oaklawn - S. England St.) (N)	excellent	excellent	excellent
Maple	(Miner - Johnson (E)	excellent	good	
Maple	Batson - Willow / Grove (W)	excellent	excellent	
Maple	Grove - Miner (E)	excellent	excellent	
Maple	Johnson - Gaston (E)	excellent	good	
Maple	Johnson - Gaston (W)	excellent	good	
Maple	Memory - Batson	excellent	excellent	
Maple	Miner - Johnson (W)	excellent	good	
Maple	Turnpike - Miner (W)	excellent	excellent	
Maple	Willow / Grove (E) - Grove (midblock)	excellent	excellent	
Maple	Willow / Grove (W) - Grove (midblock)	excellent	excellent	
Meadow View	Camp Sapphire - Straus Parkway (E)	excellent	excellent	excellent
Meadow View	Camp Sapphire - Straus Parkway (W)	excellent	excellent	excellent
Meadow View	Camp town - Camp Sapphire (E)	excellent	excellent	excellent
Meadow View	Camp town - Camp Sapphire (W)	excellent	excellent	excellent
Mills	(Cashiers Valley - Private Dr.)	excellent	poor	poor
Mills	(Picklesminer Ave - Cashiers Valley)	excellent	poor	poor
Mills	(Private Dr. - Galloway)	excellent	poor	excellent
Morgan	Broad - Gaston (N)	excellent	good	poor
Morgan	Broad - Gaston (S)	excellent	good	poor
Morgan	Caldwell - Broad (N)	excellent	good	fair
Morgan	Caldwell - Broad (S)	excellent	excellent	fair

Street	Section	Obstruction	Traffic Separation	ADA Curb Condition
Morgan	Carver / Hamlin / Oaklawn - Duckworth (S)	excellent	poor	poor
Morgan	Duckworth - Caldwell (N)	excellent	excellent	poor
Morgan	Duckworth - Caldwell (S)	excellent	excellent	fair
Neely Rd / Park View	(Greenville Hwy. - Private Dr.)	excellent	excellent	
Neely Rd / Park View	(Park - Fr. Broad)	excellent	poor	
Neely Rd / Park View	Private Dr. - Park (NW)	excellent	excellent	
Neely Rd / Park View	Private Dr. - Park (SE)	excellent	excellent	
Oakdale Ln	(Caldwell - Broad)	excellent	good	
Oakdale Ln	Carver - Jenkins	excellent	poor	
Oakdale Ln	Carver - Jenkins (N)	excellent	poor	
Oakdale Ln	Duckworth Ave. - Orange	excellent	poor	
Oakdale Ln	Hemphill Cr. - Duckworth Ave	excellent	poor	
Oakdale Ln	Jenkins - Hemphill (S)	excellent	poor	
Oakdale Ln	Orange - Caldwell	excellent	poor	
Oaklawn Ave	(Main - Probart)	poor	poor	fair
Oaklawn Ave	(Morgan - Fortune)	excellent	fair	fair
Oaklawn Ave	Fortune - Main	excellent	fair	fair
Oaklawn Ave	Jordan - Morgan (W)			
Old Cottage Ln	Mid-Block - Cul-de-sac	excellent	excellent	excellent
Old Cottage Ln	Straus Parkway - Midblock	excellent	excellent	excellent
Park Ave	(Boxwood - Harold)	excellent	good	
Park Ave	(Harold - Parkview)	excellent	good	
Park Ave	(Main - Box Wood)	excellent	good	
Park Ave	Boxwood - Harold (N)	excellent	good	
Park Ave	Main - Box Wood (N)	excellent	good	
Pine Mountain	(Camp Town - Camp Straus)	excellent	excellent	excellent
Pisgah Forest Elementary				
Probart St.	(Caldwell - Broad) (N)	fair	poor	fair
Probart St.	(Caldwell - Broad) (S)	fair	fair	fair
Probart St.	(England - Caldwell) (N)	excellent	excellent	fair
Probart St.	(England - Caldwell) (S)	excellent	excellent	fair
Probart St.	(Railroad - Oaklawn) (N)	excellent	excellent	fair
Probart St.	(Railroad - Oaklawn) (S)	fair	excellent	fair
Probart St.	Broad - Courthouse (S)	excellent	poor	fair
Probart St.	Oaklawn - England) (N)	excellent	excellent	fair
Probart St.	Oaklawn - England) (S)	excellent	excellent	fair
Rail Road / Asheville Hwy Connector	Along Kings Creek			
Rail Road Ave	Probart - King	poor	excellent	poor
Sheffield Pl	Sheffield (N) - Cambridge	excellent	fair	excellent
Sheffield Pl	Sheffield (S) - Cambridge	excellent	fair	excellent
Silversteen		None	Fair	Excellent

Street	Section	Obstruction	Traffic Separation	ADA Curb Condition
Silversteen		None	Fair	Excellent
Springhouse Trail		excellent	excellent	excellent
Straus Parkway	(Meadow - Stone House)	excellent	excellent	excellent
Straus Parkway	Asheville Hwy - Old Cottage Ln	excellent	excellent	excellent
Straus Parkway	Pine Mtn - Meadow	excellent	excellent	excellent
Straus Parkway	Private Drive - Asheville Hwy	excellent	excellent	excellent
Straus Parkway	Stone House - Private Drive	excellent	excellent	excellent
Throwbridge	Elm Bend - Manor (W))	excellent	poor	
Throwbridge	Elm Bend - Sapphire (E)	excellent	poor	
Throwbridge	Manor - Greenville Hwy (W)	excellent	poor	
Throwbridge	Sapphire - Greenville Hwy (E)	excellent	poor	
Timber Trace		excellent	excellent	excellent
Tutor	Danbury - Terminus	excellent	fair	good
Unity		excellent	poor	
Unity		excellent	fair	
Unity		excellent	fair	
Unity		excellent	fair	
Unity				
Unity		excellent	fair	
Unity		excellent	fair	
Unity				
Whitmire	(Palmer - Tinsley)	poor	poor	poor
Whitmire	(Railroad - Salem)	fair	excellent	poor
Whitmire	(Tinsley - Railroad)	poor	poor	poor
Whitmire	College Station - Caldwell (N)	fair	excellent	poor
Whitmire	Kingsmill / Burrel Mtn - Palmer	poor	poor	poor
Whitmire	Railroad - Salem (S)	fair	excellent	poor
Whitmire	Salem - Caldwell (S)	fair	excellent	poor
Whitmire	Salem - College Station	fair	excellent	poor

Street	Section	Lighting	Benches	Shade	Trash Cans
Allison Creek Bridge - City Softball Fields	Allison Creek Bridge - City Softball Fields	poor	absent	present	absent
Asheville Hwy	(Fisher - Green Acre	good	absent	present	absent
Asheville Hwy	(K-Mart - Fisher	excellent	absent	present	absent
Asheville Hwy	Allison-Straus Parkway	poor	absent	present	absent
Asheville Hwy	Asheville Hwy at Enmark	excellent	absent	present	absent
Asheville Hwy	Caldwell - K-Mart	excellent	absent	present	absent
Asheville Hwy	Caldwell / Broad / College - Food Lion				
Asheville Hwy	Deerlake - Osborne	excellent	absent	absent	absent
Asheville Hwy	Green Acre - Jackson / Chestnut	fair	absent	absent	absent
Asheville Hwy	Green Acre - Old 64	fair	absent	present	absent
Asheville Hwy	Jackson / Chestnut - Deer Lake (N)	fair	absent	absent	absent
Asheville Hwy	Jackson / Chestnut - Deer Lake (S)	fair	absent	absent	absent
Asheville Hwy	Osborne - Allison	fair	absent	absent	absent
Asheville Hwy	Straus Parkway - Hospital Drive	poor	absent	present	absent
Asheville Hwy / Ecusta	At Lowes				
Blueberry Hill	Blueberry Hill	excellent	absent	present	absent
Brevard Elementary					
Brevard High School					
Broad N	Appletree - French Broad (E)	excellent	absent	absent	absent
Broad N	Appletree - French Broad (W)	excellent	absent	absent	absent
Broad N	Brevard College - Brevard College / Caldwell (E)	excellent	absent	absent	absent
Broad N	French Broad - Brevard College (E)	excellent	absent	absent	absent
Broad N	Kellys - Appletree (E)	excellent	absent	absent	absent
Broad N	Kellys - Appletree / Professional (W)	excellent	absent	absent	absent
Broad N	Main - Probart (E)	excellent	absent	absent	absent
Broad N	Main - Probart (W)	excellent	absent	absent	absent
Broad N	Probart - Kellys (E)	excellent	absent	absent	absent
Broad N	Probart - Kellys (W)	excellent	absent	absent	absent
Broad S	Country Club - Miner (E)	good	absent	absent	absent
Broad S	Jordan - Main (E)	excelleng	absent	absent	absent
Broad S	Jordan - Main (W)	good	absent	absent	absent
Broad S	Miner - Varsity (E)	good	absent	absent	absent
Broad S	Miner - Varsity (W)	good	absent	absent	absent
Broad S	Morgan - Jordan (E)	excellent	absent	absent	absent
Broad S	Morgan - Jordan (W)	good	absent	absent	absent
Broad S	Oakdale - Morgan (E)	good	absent	absent	absent
Broad S	Oakdale - Morgan (W)	good	absent	absent	absent
Broad S	Rosman - Miner (W)	good	absent	absent	absent
Broad S	Varsity - Oakdale (E)	good	absent	absent	absent
Broad S	Varsity - Oakdale (W)	good	absent	absent	absent
Caldwell	Colwell - Wa Bracken (W)	fair	absent	present	absent

Street	Section	Lighting	Benches	Shade	Trash Cans
Caldwell	Food Lion - N Broad / College / Asheville Hwy (W)	good	absent	absent	absent
Caldwell	French Broad - Methodist (E)	excellent	absent	absent	absent
Caldwell	Jordan - Main (E)	excellent	absent	present	absent
Caldwell	Jordan - Main (W)	excellent	absent	absent	absent
Caldwell	King / Kelly's Aly - Professional Aly (E)	excellent	absent	absent	absent
Caldwell	Main - Probart (E)	excellent	absent	absent	absent
Caldwell	Main - Probart (W)	good	absent	absent	absent
Caldwell	Morgan - Jordan (E)	excellent	absent	present	absent
Caldwell	Morgan - Jordan (W)	good	absent	absent	absent
Caldwell	Oakdale - Morgan (E)	good	absent	absent	absent
Caldwell	Oakdale - Morgan (W)	good	absent	absent	absent
Caldwell	Probart - Kelly's Aly / King (E)	good	absent	absent	absent
Caldwell	Professional Aly - French Broad (E)	excellent	absent	absent	absent
Caldwell	Silverstein - Oakdale (E)	good	absent	absent	absent
Caldwell	Silverstein - Oakdale (W)	good	absent	absent	absent
Caldwell	WA Bracken - Silverstein (W)	fair	absent	present	absent
Caldwell	Whitmire - Food Lion (W)	excellent	absent	present	absent
Cambridge Dr.	Cambridge (E) - Deer Lake	fair	absent	present	absent
Cambridge Dr.	Cambridge (N) - Canterbury Trl	fair	absent	present	absent
Cambridge Dr.	Cambridge (S) - Canterbury	fair	absent	present	absent
Cambridge Dr.	Cambridge (W) - Deer Lake	fair	absent	present	absent
Cambridge Dr.	Canterbury - Sheffield	fair	absent	present	absent
Cambridge Dr.	Canterbury - Sheffield	fair	absent	present	absent
Cambridge Dr.	Deer Lake - Sheffield	fair	absent	present	absent
Cambridge Dr.	Deer Lake - Sheffield	fair	absent	present	absent
Camp Sapphire	Meadow View - Camp Town	excellent	absent	present	absent
Camp Town	Camp Sapphire - Meadow View	excellent	absent	present	absent
Camp Town	Camp Sapphire / Blueberry - Pine Mountain	excellent	absent	present	absent
Camp Town	Meadow View - Stone House	excellent	absent	present	absent
Camp Town	Stone House - END	excellent	absent	present	absent
Canterbury	Canterbury (E) - Cambridge	fair	absent	present	absent
Canterbury	Canterbury (W) - Cambridge	fair	absent	present	absent
Carver		good	absent	absent	absent
Carver		good	absent	absent	absent
Cashiers Valley		good	absent	absent	absent
Cashiers Valley		good	absent	absent	absent
City Softball Fields - Ecusta Road	City Softball Fields - Ecusta Road	poor	absent	present	absent
College Station	Caldwell - Food Lion				
Country Club	(Southview - Turnpike)	fair	absent	present	absent
Country Club	(Turnpike - Monroe)	fair	absent	present	absent
Country Club	Ashworth - Southview	good	absent	present	absent

Street	Section	Lighting	Benches	Shade	Trash Cans
Country Club	End of Sidewalk - Bridge at H.S.				
Country Club	Gallimore - Brevard High	poor	absent	absent	absent
Country Club	Gallimore - Gillespie	good	absent	present	absent
Country Club	Gillespie - Ashworth	good	absent	present	absent
Country Club	Gillispie Circle - Hayes	good	absent	present	absent
Country Club	Hayes - South Broad	good	absent	absent	absent
Country Club	Monroe - Gillispie Circle	good	absent	present	absent
Danbury	Hopewell - Tutor				
Danbury	Terminus - Hopewell				
Danbury	Tutor - Terminus				
Deer Lake	West terminal - Stone Dr	poor	absent	present	absent
Deer Run	Deer Lake - Chipmunk	poor	absent	present	absent
Deer Run	Deer Lake - Chipmunk	poor	absent	present	absent
Deerlake	(Stone - Fox Cross)	fair	absent	absent	absent
Deerlake	Deer Run - Broad	poor	absent	absent	absent
Deerlake	Eldridge - Broad	poor	absent	absent	absent
Deerlake	Fox Cross - Deer Run	fair	absent	absent	absent
East Main St. Ext.	Wilson / Main / Greenville - Terminus	poor	absent	present	absent
Elm Bend	Greenville Hwy - Throwbridge / Magnolia	fair	absent	present	absent
Elm Bend	Throwbridge / Magnolia - Sapphire Manor	fair	absent	present	absent
England	(Main - Probart)	poor	absent	absent	absent
English Hills	Picklesimer Ave. - English Hills Project	poor	absent	absent	absent
Fisher	(between Montview intersections)	poor	absent	absent	absent
Fisher	(Montview - Broad)	poor	absent	absent	absent
Fisher	(Poplar - Montview)	poor	absent	absent	absent
Fisher	Brevard Middle School				
Franklin	Lakeview - French Broad (N)	good	absent	present	absent
Franklin	Lakeview - Lakeview (S)	fair	absent	present	absent
Franklin	Main - Lakeview (N)	good	absent	present	absent
Franklin	Main - Lakeview (S)	good	absent	present	absent
French Broad	(Appletree - Franklin)	good	absent	absent	absent
French Broad	(Broad - Johnson)	good	absent	absent	absent
French Broad	(Caldwell - Broad)	good	absent	absent	absent
French Broad	(Franklin - Hilt)	fair	absent	present	absent
French Broad	(Johnson - Appletree)	good	absent	present	absent
French Broad	(Railroad - Salem)	good	absent	absent	absent
French Broad	(Railroad - Salem)	good	absent	absent	absent
French Broad	(Salem - Caldwell)	poor	absent	absent	absent
French Broad	(Salem - Caldwell)	poor	absent	absent	absent
French Broad	Hilt - Woodlawn	fair	absent	present	absent
French Broad	Woodlawn - Parkview	fair	absent	present	absent

Street	Section	Lighting	Benches	Shade	Trash Cans
Gaston	(Jordan - Main)	fair	absent	absent	absent
Gaston	(Jordan - Main)	fair	absent	absent	absent
Gaston	(Main - Probart)	fair	absent	absent	absent
Gaston	(Main - Probart)	fair	absent	absent	absent
Gaston	(Maple - Morgan)	fair	absent	absent	absent
Gaston	(Maple - Morgan)	fair	absent	absent	absent
Gaston	(Morgan - Jordan)	fair	absent	absent	absent
Gaston	(Morgan - Jordan)	fair	absent	absent	absent
Greenville Hwy	(Elm Bend - Hrowbridge)	fair	absent	present	absent
Greenville Hwy	(Hrowbridge - sidewalk to Brevard Elementary)	fair	absent	absent	absent
Greenville Hwy	E. Main - Elm Bend	fair	absent	present	absent
Greenville Hwy	sidewalk to Brevard Elementary	poor	absent	absent	absent
Hillview Ave	(Private Dr. - Picklesminer Ave)	fair	absent	absent	absent
Hillview Ave		good	absent	absent	absent
Hillview Ave		good	absent	absent	absent
Hopewell	Knight - Danbury				
Hopewell	London - Knight				
Hopewell	Osborne - London				
Hospital Drive	Hospital Drive - Mid-Block	poor	absent	present	absent
Hospital Drive - Allison Creek Bridge	Hospital Drive - Allison Creek Bridge	poor	absent	present	absent
Johnson	(Appletree - Fr. Broad)	good	absent	absent	absent
Johnson	(Jordan - Main)	poor	absent	present	absent
Johnson	(Main - Probart)	fair	absent	absent	absent
Johnson	(Probart - Appletree)	fair	absent	present	absent
Jordan	(Broad - Gaston) (N)	poor	absent	absent	absent
Jordan	(Broad - Gaston) (S)	excellent	absent	absent	absent
Jordan	(Caldwell - Broad) (N)	poor	absent	absent	absent
Jordan	(Caldwell - Broad) (S)	poor	absent	absent	absent
Jordan	(England - Caldwell) (N)	excellent	absent	present	absent
Jordan	(England - Caldwell) (S)	excellent	absent	present	absent
Jordan	(Gaston - Johnson) (N)				
Jordan	(Oaklawn - S. England St.) (N)	poor	absent	present	absent
Jordan	(Oaklawn - S. England St.) (S)	poor	absent	present	absent
King	Floyd - Caldwell	poor	absent	absent	absent
King	Railroad - Floyd (N)	poor	absent	absent	absent
King	Railroad - Floyd (S)				
King	Railroad - Floyd (S)				
Knight Ct.	Hopewell - Terminus	fair	absent	present	absent
London	Hopewell - Terminus	fair	absent	present	absent
Main Street (E)	(Broad - Gaston) (N)	excellent	present	absent	absent
Main Street (E)	(Broad - Gaston) (S)	excellent	present	absent	absent

Street	Section	Lighting	Benches	Shade	Trash Cans
Main Street (E)	(Franklin - Park) (N)	fair	absent	absent	absent
Main Street (E)	(Franklin - Park) (S)	fair	absent	absent	absent
Main Street (E)	(Gaston - Johnson) (N)	excellent	present	absent	absent
Main Street (E)	(Gaston - Johnson) (S)	excellent	present	absent	absent
Main Street (E)	(Johnson - Rice St.) (N)	fair	absent	present	absent
Main Street (E)	(Johnson - Rice St.) (S)	fair	absent	present	absent
Main Street (E)	(Rice - Franklin) (N)	fair	absent	present	absent
Main Street (E)	Franklin - Woodside (S)				
Main Street (E)	Park - Greenville Hwy / E. Main	poor	absent	absent	absent
Main Street (E)	Park - Greenville Hwy / E. Main	fair	absent	present	absent
Main Street (E)	Rice - St. Phillips (S)	fair	absent	present	absent
Main Street (E)	St. Phillips - Franklin (S)	fair	absent	present	absent
Main Street (W)	(Caldwell - Broad) (N)				
Main Street (W)	(Caldwell - Broad) (S)	excellent	present	absent	absent
Main Street (W)	(England - Caldwell) (N)				
Main Street (W)	(England - Caldwell) (S)	excellent	present	absent	absent
Main Street (W)	(Galloway - Oaklawn Ave)	fair	absent	good	absent
Main Street (W)	(Galloway - Oaklawn Ave) (N)	fair	absent	good	absent
Main Street (W)	(Oaklawn - S. England St.)	excellent	present	excellent	present
Main Street (W)	(Oaklawn - S. England St.) (N)	excellent	present	excellent	present
Maple	(Miner - Johnson (E)	fair	absent	present	absent
Maple	Batson - Willow / Grove (W)	good	absent	present	absent
Maple	Grove - Miner (E)	good	absent	absent	absent
Maple	Johnson - Gaston (E)	good	absent	absent	absent
Maple	Johnson - Gaston (W)	good	absent	absent	absent
Maple	Memory - Batson	good	absent	present	absent
Maple	Miner - Johnson (W)	fair	absent	present	absent
Maple	Turnpike - Miner (W)	good	absent	absent	absent
Maple	Willow / Grove (E) - Grove (midblock)	good	absent	absent	absent
Maple	Willow / Grove (W) - Grove (midblock)	good	absent	absent	absent
Meadow View	Camp Sapphire - Straus Parkway (E)	excellent	absent	present	absent
Meadow View	Camp Sapphire - Straus Parkway (W)	excellent	absent	present	absent
Meadow View	Camp town - Camp Sapphire (E)	excellent	absent	present	absent
Meadow View	Camp town - Camp Sapphire (W)	excellent	absent	present	absent
Mills	(Cashiers Valley - Private Dr.)	good	absent	absent	absent
Mills	(Picklesminer Ave - Cashiers Valley)	good	absent	absent	absent
Mills	(Private Dr. - Galloway)	poor	absent	absent	absent
Morgan	Broad - Gaston (N)	excellent	absent	absent	absent
Morgan	Broad - Gaston (S)	good	absent	absent	absent
Morgan	Caldwell - Broad (N)	excellent	absent	absent	absent
Morgan	Caldwell - Broad (S)	good	absent	absent	absent

Street	Section	Lighting	Benches	Shade	Trash Cans
Morgan	Carver / Hamlin / Oaklawn - Duckworth (S)	poor	absent	absent	absent
Morgan	Duckworth - Caldwell (N)	poor	absent	absent	absent
Morgan	Duckworth - Caldwell (S)	poor	absent	absent	absent
Neely Rd / Park View	(Greenville Hwy. - Private Dr.)	fair	absent	present	absent
Neely Rd / Park View	(Park - Fr. Broad)	fair	absent	present	absent
Neely Rd / Park View	Private Dr. - Park (NW)	fair	absent	absent	absent
Neely Rd / Park View	Private Dr. - Park (SE)	fair	absent	absent	absent
Oakdale Ln	(Caldwell - Broad)	poor	absent	present	absent
Oakdale Ln	Carver - Jenkins	good	absent	absent	absent
Oakdale Ln	Carver - Jenkins (N)	good	absent	absent	absent
Oakdale Ln	Duckworth Ave. - Orange	poor	absent	present	absent
Oakdale Ln	Hemphill Cr. - Duckworth Ave	poor	absent	absent	absent
Oakdale Ln	Jenkins - Hemphill (S)	good	absent	absent	absent
Oakdale Ln	Orange - Caldwell	poor	absent	present	absent
Oaklawn Ave	(Main - Probart)	poor	absent	present	absent
Oaklawn Ave	(Morgan - Fortune)	poor	absent	present	absent
Oaklawn Ave	Fortune - Main	poor	absent	present	absent
Oaklawn Ave	Jordan - Morgan (W)				
Old Cottage Ln	Mid-Block - Cul-de-sac	excellent	absent	present	absent
Old Cottage Ln	Straus Parkway - Midblock	excellent	absent	present	absent
Park Ave	(Boxwood - Harold)	good	absent	absent	absent
Park Ave	(Harold - Parkview)	fair	absent	absent	absent
Park Ave	(Main - Box Wood)	good	absent	absent	absent
Park Ave	Boxwood - Harold (N)	good	absent	absent	absent
Park Ave	Main - Box Wood (N)	good	absent	absent	absent
Pine Mountain	(Camp Town - Camp Straus)	excellent	absent	present	absent
Pisgah Forest Elementary					
Probart St.	(Caldwell - Broad) (N)	poor	absent	present	absent
Probart St.	(Caldwell - Broad) (S)	poor	absent	present	absent
Probart St.	(England - Caldwell) (N)	poor	absent	present	absent
Probart St.	(England - Caldwell) (S)	poor	absent	present	absent
Probart St.	(Railroad - Oaklawn) (N)	poor	absent	present	absent
Probart St.	(Railroad - Oaklawn) (S)	poor	absent	present	absent
Probart St.	Broad - Courthouse (S)	poor	absent	present	absent
Probart St.	Oaklawn - England) (N)	poor	absent	present	absent
Probart St.	Oaklawn - England) (S)	poor	absent	present	absent
Rail Road / Asheville Hwy Connector	Along Kings Creek				
Rail Road Ave	Probart - King	poor	absent	present	absent
Sheffield Pl	Sheffield (N) - Cambridge	fair	absent	present	absent
Sheffield Pl	Sheffield (S) - Cambridge	fair	absent	present	absent
Silversteen		Poor	None	Excellent	None

Street	Section	Lighting	Benches	Shade	Trash Cans
Silversteen		Poor	None	Excellent	None
Springhouse Trail		excellent	absent	present	absent
Straus Parkway	(Meadow - Stone House)	excellent	absent	present	absent
Straus Parkway	Asheville Hwy - Old Cottage Ln	excellent	absent	present	absent
Straus Parkway	Pine Mtn - Meadow	excellent	absent	present	absent
Straus Parkway	Private Drive - Asheville Hwy	excellent	absent	present	absent
Straus Parkway	Stone House - Private Drive	excellent	absent	present	absent
Throwbridge	Elm Bend - Manor (W))	fair	absent	present	absent
Throwbridge	Elm Bend - Sapphire (E)	fair	absent	present	absent
Throwbridge	Manor - Greenville Hwy (W)	fair	absent	present	absent
Throwbridge	Sapphire - Greenville Hwy (E)	fair	absent	present	absent
Timber Trace		excellent	absent	present	absent
Tutor	Danbury - Terminus	good	absent	present	present
Unity		fair	absent	present	absent
Unity		good	absent	absent	absent
Unity		good	absent	absent	absent
Unity		good	absent	absent	absent
Unity					
Unity		good	absent	absent	absent
Unity		good	absent	absent	absent
Unity					
Whitmire	(Palmer - Tinsley)	good	absent	present	absent
Whitmire	(Railroad - Salem)	fair	absent	present	absent
Whitmire	(Tinsley - Railroad)	good	absent	present	absent
Whitmire	College Station - Caldwell (N)	poor	absent	absent	absent
Whitmire	Kingsmill / Burrel Mtn - Palmer	good	absent	present	absent
Whitmire	Railroad - Salem (S)	fair	absent	present	absent
Whitmire	Salem - Caldwell (S)	fair	absent	present	absent
Whitmire	Salem - College Station	fair	absent	present	absent

Street	Section	Notes
Allison Creek Bridge - City Softball Fields	Allison Creek Bridge - City Softball Fields	Feels too close to road on bicycle given travel volume / speed
Asheville Hwy	(Fisher - Green Acre	
Asheville Hwy	(K-Mart - Fisher	
Asheville Hwy	Allison-Straus Parkway	Feels too close to road on bicycle given travel volume / speed
Asheville Hwy	Asheville Hwy at Enmark	Location of proposed bike path. SW isolated at present.
Asheville Hwy	Caldwell - K-Mart	Telephone pole near sidewalk
Asheville Hwy	Caldwell / Broad / College - Food Lion	
Asheville Hwy	Deerlake - Osborne	3 curb cuts. SW terminates at Osborne without ramp.
Asheville Hwy	Green Acre - Jackson / Chestnut	Ramps ADA but many very large curb cuts make crossing at driveways dangerous.
Asheville Hwy	Green Acre - Old 64	
Asheville Hwy	Jackson / Chestnut - Deer Lake (N)	Big gap in middle of section where shoulder replaces sidewalk.
Asheville Hwy	Jackson / Chestnut - Deer Lake (S)	Big gap in middle of section where shoulder replaces sidewalk.
Asheville Hwy	Osborne - Allison	Down ramps not exist or in bad condition
Asheville Hwy	Straus Parkway - Hospital Drive	Feels too close to road on bicycle given travel volume / speed
Asheville Hwy / Ecusta	At Lowes	
Blueberry Hill	Blueberry Hill	Sidewalk needs completion. Only on one side of street.
Brevard Elementary		
Brevard High School		
Broad N	Appletree - French Broad (E)	
Broad N	Appletree - French Broad (W)	
Broad N	Brevard College - Brevard College / Caldwell (E)	
Broad N	French Broad - Brevard College (E)	
Broad N	Kellys - Appletree (E)	
Broad N	Kellys - Appletree / Professional (W)	
Broad N	Main - Probart (E)	
Broad N	Main - Probart (W)	
Broad N	Probart - Kellys (E)	
Broad N	Probart - Kellys (W)	
Broad S	Country Club - Miner (E)	
Broad S	Jordan - Main (E)	
Broad S	Jordan - Main (W)	
Broad S	Miner - Varsity (E)	surface condition is good on right side of the street in direction from the city centre
Broad S	Miner - Varsity (W)	
Broad S	Morgan - Jordan (E)	
Broad S	Morgan - Jordan (W)	
Broad S	Oakdale - Morgan (E)	
Broad S	Oakdale - Morgan (W)	
Broad S	Rosman - Miner (W)	surface condition is good on right side of the street in direction from the city centre
Broad S	Varsity - Oakdale (E)	
Broad S	Varsity - Oakdale (W)	
Caldwell	Colwell - Wa Bracken (W)	separating strip in a bad condition

Street	Section	Notes
Caldwell	Food Lion - N Broad / College / Asheville Hwy (W)	
Caldwell	French Broad - Methodist (E)	
Caldwell	Jordan - Main (E)	
Caldwell	Jordan - Main (W)	
Caldwell	King / Kelly's Aly - Professional Aly (E)	
Caldwell	Main - Probart (E)	
Caldwell	Main - Probart (W)	
Caldwell	Morgan - Jordan (E)	
Caldwell	Morgan - Jordan (W)	
Caldwell	Oakdale - Morgan (E)	
Caldwell	Oakdale - Morgan (W)	
Caldwell	Probart - Kelly's Aly / King (E)	
Caldwell	Professional Aly - French Broad (E)	
Caldwell	Silverstein - Oakdale (E)	
Caldwell	Silverstein - Oakdale (W)	
Caldwell	WA Bracken - Silverstein (W)	separating strip in a bad condition
Caldwell	Whitmire - Food Lion (W)	
Cambridge Dr.	Cambridge (E) - Deer Lake	Low tree branches. Otherwise Perfect.
Cambridge Dr.	Cambridge (N) - Canterbury Trl	Low tree branches. Otherwise Perfect.
Cambridge Dr.	Cambridge (S) - Canterbury	Low tree branches. Otherwise Perfect.
Cambridge Dr.	Cambridge (W) - Deer Lake	Low tree branches. Otherwise Perfect.
Cambridge Dr.	Canterbury - Sheffield	Low tree branches. Otherwise Perfect.
Cambridge Dr.	Canterbury - Sheffield	Low tree branches. Otherwise Perfect.
Cambridge Dr.	Deer Lake - Sheffield	Low tree branches. Otherwise Perfect.
Cambridge Dr.	Deer Lake - Sheffield	Low tree branches. Otherwise Perfect.
Camp Sapphire	Meadow View - Camp Town	Very short section terminating on shoulder.
Camp Town	Camp Sapphire - Meadow View	Sidewalk only on one side.
Camp Town	Camp Sapphire / Blueberry - Pine Mountain	Sidewalk only on one side.
Camp Town	Meadow View - Stone House	Sidewalk only on one side.
Camp Town	Stone House - END	Sidewalk only on one side. Needs completion.
Canterbury	Canterbury (E) - Cambridge	Low tree branches. Otherwise Perfect.
Canterbury	Canterbury (W) - Cambridge	Low tree branches. Otherwise Perfect.
Carver		
Carver		
Cashiers Valley		
Cashiers Valley		
City Softball Fields - Ecusta Road	City Softball Fields - Ecusta Road	Feels too close to road on bicycle given travel volume / speed
College Station	Caldwell - Food Lion	
Country Club	(Southview - Turnpike)	
Country Club	(Turnpike - Monroe)	
Country Club	Ashworth - Southview	

Street	Section	Notes
Country Club	End of Sidewalk - Bridge at H.S.	
Country Club	Gallimore - Brevard High	
Country Club	Gallimore - Gillespie	
Country Club	Gillespie - Ashworth	
Country Club	Gillispie Circle - Hayes	
Country Club	Hayes - South Broad	
Country Club	Monroe - Gillispie Circle	
Danbury	Hopewell - Tutor	
Danbury	Terminus - Hopewell	
Danbury	Tutor - Terminus	
Deer Lake	West terminal - Stone Dr	Terminates without ramp.
Deer Run	Deer Lake - Chipmunk	Steep
Deer Run	Deer Lake - Chipmunk	Steep
Deerlake	(Stone - Fox Cross)	Millions of nasty geese
Deerlake	Deer Run - Broad	Short segment terminates without ramp.
Deerlake	Eldridge - Broad	
Deerlake	Fox Cross - Deer Run	Billions of nasty geese
East Main St. Ext.	Wilson / Main / Greenville - Terminus	
Elm Bend	Greenville Hwy - Throwbridge / Magnolia	
Elm Bend	Throwbridge / Magnolia - Sapphire Manor	
England	(Main - Probart)	
English Hills	Picklesimer Ave. - English Hills Project	Short segment needs extension
Fisher	(between Montview intersections)	
Fisher	(Montview - Broad)	
Fisher	(Poplar - Montview)	
Fisher	Brevard Middle School	
Franklin	Lakeview - French Broad (N)	household's dust bins crossing sidewalk
Franklin	Lakeview - Lakeview (S)	Short segment begins and ends same property
Franklin	Main - Lakeview (N)	hydrante blocking sidewalk
Franklin	Main - Lakeview (S)	Short sidewalk terminates in parking area
French Broad	(Appletree - Franklin)	dust bin crossing sidewalk
French Broad	(Broad - Johnson)	
French Broad	(Caldwell - Broad)	
French Broad	(Franklin - Hilt)	
French Broad	(Johnson - Appletree)	
French Broad	(Railroad - Salem)	
French Broad	(Railroad - Salem)	
French Broad	(Salem - Caldwell)	
French Broad	(Salem - Caldwell)	
French Broad	Hilt - Woodlawn	
French Broad	Woodlawn - Parkview	vegetation invasion from neighboring property

Street	Section	Notes
Gaston	(Jordan - Main)	
Gaston	(Jordan - Main)	
Gaston	(Main - Probart)	
Gaston	(Main - Probart)	
Gaston	(Maple - Morgan)	
Gaston	(Maple - Morgan)	
Gaston	(Morgan - Jordan)	
Gaston	(Morgan - Jordan)	
Greenville Hwy	(Elm Bend - Hrowbridge)	
Greenville Hwy	(Hrowbridge - sidewalk to Brevard Elementary)	
Greenville Hwy	E. Main - Elm Bend	trees and brushes are invading sidewalk's walking space
Greenville Hwy	sidewalk to Brevard Elementary	
Hillview Ave	(Private Dr. - Picklesminer Ave)	
Hillview Ave		
Hillview Ave		
Hopewell	Knight - Danbury	
Hopewell	London - Knight	
Hopewell	Osborne - London	
Hospital Drive	Hospital Drive - Mid-Block	Feels too close to road on bicycle given travel volume / speed
Hospital Drive - Allison Creek Bridge	Hospital Drive - Allison Creek Bridge	Feels too close to road on bicycle given travel volume / speed
Johnson	(Appletree - Fr. Broad)	
Johnson	(Jordan - Main)	
Johnson	(Main - Probart)	
Johnson	(Probart - Appletree)	
Jordan	(Broad - Gaston) (N)	
Jordan	(Broad - Gaston) (S)	
Jordan	(Caldwell - Broad) (N)	
Jordan	(Caldwell - Broad) (S)	
Jordan	(England - Caldwell) (N)	
Jordan	(England - Caldwell) (S)	
Jordan	(Gaston - Johnson) (N)	
Jordan	(Oaklawn - S. England St.) (N)	
Jordan	(Oaklawn - S. England St.) (S)	
King	Floyd - Caldwell	
King	Railroad - Floyd (N)	
King	Railroad - Floyd (S)	
King	Railroad - Floyd (S)	
Knight Ct.	Hopewell - Terminus	
London	Hopewell - Terminus	
Main Street (E)	(Broad - Gaston) (N)	
Main Street (E)	(Broad - Gaston) (S)	

Street	Section	Notes
Main Street (E)	(Franklin - Park) (N)	debris from Silvermont park - mainly leafs
Main Street (E)	(Franklin - Park) (S)	debris from Silvermont park - mainly leafs
Main Street (E)	(Gaston - Johnson) (N)	
Main Street (E)	(Gaston - Johnson) (S)	
Main Street (E)	(Johnson - Rice St.) (N)	one sidewalk is brick
Main Street (E)	(Johnson - Rice St.) (S)	one sidewalk is brick
Main Street (E)	(Rice - Franklin) (N)	vegetation invasion from neighboring private property
Main Street (E)	Franklin - Woodside (S)	
Main Street (E)	Park - Greenville Hwy / E. Main	household dust bins crossing sidewalks
Main Street (E)	Park - Greenville Hwy / E. Main	trees and brushes are invasing sidewalk's walking space
Main Street (E)	Rice - St. Phillips (S)	vegetation invasion from neighboring private property
Main Street (E)	St. Phillips - Franklin (S)	vegetation invasion from neighboring private property
Main Street (W)	(Caldwell - Broad) (N)	
Main Street (W)	(Caldwell - Broad) (S)	
Main Street (W)	(England - Caldwell) (N)	
Main Street (W)	(England - Caldwell) (S)	
Main Street (W)	(Galloway - Oaklawn Ave)	
Main Street (W)	(Galloway - Oaklawn Ave) (N)	
Main Street (W)	(Oaklawn - S. England St.)	
Main Street (W)	(Oaklawn - S. England St.) (N)	
Maple	(Miner - Johnson (E)	
Maple	Batson - Willow / Grove (W)	
Maple	Grove - Miner (E)	
Maple	Johnson - Gaston (E)	
Maple	Johnson - Gaston (W)	
Maple	Memory - Batson	
Maple	Miner - Johnson (W)	
Maple	Turnpike - Miner (W)	
Maple	Willow / Grove (E) - Grove (midblock)	Midblock switches sides with limited visibility and no indicators
Maple	Willow / Grove (W) - Grove (midblock)	Midblock switches sides with limited visibility and no indicators
Meadow View	Camp Sapphire - Straus Parkway (E)	Perfect streetscape
Meadow View	Camp Sapphire - Straus Parkway (W)	Perfect streetscape
Meadow View	Camp town - Camp Saphire (E)	Almost perfect sidewalk section / streetscape.
Meadow View	Camp town - Camp Saphire (W)	Almost perfect sidewalk section / streetscape.
Mills	(Cashiers Valley - Private Dr.)	
Mills	(Picklesminer Ave - Cashiers Valley)	playground next to road, 1 drum instead trash can
Mills	(Private Dr. - Galloway)	
Morgan	Broad - Gaston (N)	
Morgan	Broad - Gaston (S)	separation from traffic jus on one side of the street
Morgan	Caldwell - Broad (N)	
Morgan	Caldwell - Broad (S)	

Street	Section	Notes
Morgan	Carver / Hamlin / Oaklawn - Duckworth (S)	
Morgan	Duckworth - Caldwell (N)	
Morgan	Duckworth - Caldwell (S)	
Neely Rd / Park View	(Greenville Hwy. - Private Dr.)	
Neely Rd / Park View	(Park - Fr. Broad)	
Neely Rd / Park View	Private Dr. - Park (NW)	
Neely Rd / Park View	Private Dr. - Park (SE)	
Oakdale Ln	(Caldwell - Broad)	
Oakdale Ln	Carver - Jenkins	
Oakdale Ln	Carver - Jenkins (N)	
Oakdale Ln	Duckworth Ave. - Orange	
Oakdale Ln	Hemphill Cr. - Duckworth Ave	
Oakdale Ln	Jenkins - Hemphill (S)	
Oakdale Ln	Orange - Caldwell	
Oaklawn Ave	(Main - Probart)	
Oaklawn Ave	(Morgan - Fortune)	
Oaklawn Ave	Fortune - Main	
Oaklawn Ave	Jordan - Morgan (W)	
Old Cottage Ln	Mid-Block - Cul-de-sac	Proposed SW does not exist. Required via Mkt St. PBD Permit
Old Cottage Ln	Straus Parkway - Midblock	Proposed SW does not exist. Required via Mkt St. PBD Permit
Park Ave	(Boxwood - Harold)	
Park Ave	(Harold - Parkview)	
Park Ave	(Main - Box Wood)	
Park Ave	Boxwood - Harold (N)	
Park Ave	Main - Box Wood (N)	
Pine Mountain	(Camp Town - Camp Straus)	
Pisgah Forest Elementary		
Probart St.	(Caldwell - Broad) (N)	
Probart St.	(Caldwell - Broad) (S)	
Probart St.	(England - Caldwell) (N)	
Probart St.	(England - Caldwell) (S)	
Probart St.	(Railroad - Oaklawn) (N)	
Probart St.	(Railroad - Oaklawn) (S)	
Probart St.	Broad - Courthouse (S)	
Probart St.	Oaklawn - England) (N)	
Probart St.	Oaklawn - England) (S)	
Rail Road / Asheville Hwy Connector	Along Kings Creek	
Rail Road Ave	Probart - King	
Sheffield Pl	Sheffield (N) - Cambridge	Low tree branches. Otherwise Perfect.
Sheffield Pl	Sheffield (S) - Cambridge	Low tree branches. Otherwise Perfect.
Silversteen		

Street	Section	Notes
Silversteen		
Springhouse Trail		End does not go all the way to Upper Camp Town Road. Should be extended.
Straus Parkway	(Meadow - Stone House)	only on one side of street.
Straus Parkway	Asheville Hwy - Old Cottage Ln	Proposed SW does not exist. Required via Mkt St. PBD Permit
Straus Parkway	Pine Mtn - Meadow	Only on one side of street.
Straus Parkway	Private Drive - Asheville Hwy	Horrible intersection at Ash. Hwy. No safe way to cross.
Straus Parkway	Stone House - Private Drive	Horrible intersection at Ash. Hwy. No safe way to cross.
Throwbridge	Elm Bend - Manor (W))	
Throwbridge	Elm Bend - Sapphire (E)	
Throwbridge	Manor - Greenville Hwy (W)	
Throwbridge	Sapphire - Greenville Hwy (E)	
Timber Trace		
Tutor	Danbury - Terminus	
Unity		sidewalk is very narrow - 3feet
Unity		sidewalks separated by parking lots
Unity		sidewalks separated by parking lots
Unity		sidewalks separated by parking lots
Unity		
Unity		sidewalks separated by parking lots
Unity		sidewalks separated by parking lots
Unity		
Whitmire	(Palmer - Tinsley)	
Whitmire	(Railroad - Salem)	
Whitmire	(Tinsley - Railroad)	
Whitmire	College Station - Caldwell (N)	
Whitmire	Kingsmill / Burrel Mtn - Palmer	
Whitmire	Railroad - Salem (S)	
Whitmire	Salem - Caldwell (S)	
Whitmire	Salem - College Station	

Street	Section	Overall Condition
Allison Creek Bridge - City Softball Fields	Allison Creek Bridge - City Softball Fields	excellent
Asheville Hwy	(Fisher - Green Acre	good
Asheville Hwy	(K-Mart - Fisher	excellent
Asheville Hwy	Allison-Straus Parkway	excellent
Asheville Hwy	Asheville Hwy at Enmark	excellent
Asheville Hwy	Caldwell - K-Mart	good
Asheville Hwy	Caldwell / Broad / College - Food Lion	
Asheville Hwy	Deerlake - Osborne	poor
Asheville Hwy	Green Acre - Jackson / Chestnut	fair
Asheville Hwy	Green Acre - Old 64	excellent
Asheville Hwy	Jackson / Chestnut - Deer Lake (N)	poor
Asheville Hwy	Jackson / Chestnut - Deer Lake (S)	poor
Asheville Hwy	Osborne - Allison	good
Asheville Hwy	Straus Parkway - Hospital Drive	excellent
Asheville Hwy / Ecusta	At Lowes	
Blueberry Hill	Blueberry Hill	excellent
Brevard Elementary		
Brevard High School		
Broad N	Appletree - French Broad (E)	poor
Broad N	Appletree - French Broad (W)	poor
Broad N	Brevard College - Brevard College / Caldwell (E)	fair
Broad N	French Broad - Brevard College (E)	fair
Broad N	Kellys - Appletree (E)	fair
Broad N	Kellys - Appletree / Professional (W)	poor
Broad N	Main - Probart (E)	fair
Broad N	Main - Probart (W)	fair
Broad N	Probart - Kellys (E)	fair
Broad N	Probart - Kellys (W)	good
Broad S	Country Club - Miner (E)	good
Broad S	Jordan - Main (E)	excellent
Broad S	Jordan - Main (W)	excellent
Broad S	Miner - Varsity (E)	good
Broad S	Miner - Varsity (W)	good
Broad S	Morgan - Jordan (E)	good
Broad S	Morgan - Jordan (W)	good
Broad S	Oakdale - Morgan (E)	excellent
Broad S	Oakdale - Morgan (W)	excellent
Broad S	Rosman - Miner (W)	good
Broad S	Varsity - Oakdale (E)	excellent
Broad S	Varsity - Oakdale (W)	excellent
Caldwell	Colwell - Wa Bracken (W)	poor

Street	Section	Overall Condition
Caldwell	Food Lion - N Broad / College / Asheville Hwy (W)	excellent
Caldwell	French Broad - Methodist (E)	excellent
Caldwell	Jordan - Main (E)	good
Caldwell	Jordan - Main (W)	excellent
Caldwell	King / Kelly's Aly - Professional Aly (E)	excellent
Caldwell	Main - Probart (E)	good
Caldwell	Main - Probart (W)	excellent
Caldwell	Morgan - Jordan (E)	excellent
Caldwell	Morgan - Jordan (W)	good
Caldwell	Oakdale - Morgan (E)	fair
Caldwell	Oakdale - Morgan (W)	fair
Caldwell	Probart - Kelly's Aly / King (E)	excellent
Caldwell	Professional Aly - French Broad (E)	excellent
Caldwell	Silverstein - Oakdale (E)	fair
Caldwell	Silverstein - Oakdale (W)	fair
Caldwell	WA Bracken - Silverstein (W)	poor
Caldwell	Whitmire - Food Lion (W)	excellent
Cambridge Dr.	Cambridge (E) - Deer Lake	excellent
Cambridge Dr.	Cambridge (N) - Canterbury Trl	excellent
Cambridge Dr.	Cambridge (S) - Canterbury	excellent
Cambridge Dr.	Cambridge (W) - Deer Lake	excellent
Cambridge Dr.	Canterbury - Sheffield	excellent
Cambridge Dr.	Canterbury - Sheffield	excellent
Cambridge Dr.	Deer Lake - Sheffield	excellent
Cambridge Dr.	Deer Lake - Sheffield	excellent
Camp Sapphire	Meadow View - Camp Town	fair
Camp Town	Camp Sapphire - Meadow View	excellent
Camp Town	Camp Sapphire / Blueberry - Pine Mountain	excellent
Camp Town	Meadow View - Stone House	excellent
Camp Town	Stone House - END	good
Canterbury	Canterbury (E) - Cambridge	excellent
Canterbury	Canterbury (W) - Cambridge	excellent
Carver		fair
Carver		fair
Cashiers Valley		good
Cashiers Valley		fair
City Softball Fields - Ecusta Road	City Softball Fields - Ecusta Road	excellent
College Station	Caldwell - Food Lion	
Country Club	(Southview - Turnpike)	good
Country Club	(Turnpike - Monroe)	good
Country Club	Ashworth - Southview	good

Street	Section	Overall Condition
Country Club	End of Sidewalk - Bridge at H.S.	
Country Club	Gallimore - Brevard High	excellent
Country Club	Gallimore - Gillespie	good
Country Club	Gillespie - Ashworth	good
Country Club	Gillispie Circle - Hayes	fair
Country Club	Hayes - South Broad	good
Country Club	Monroe - Gillispie Circle	fair
Danbury	Hopewell - Tutor	
Danbury	Terminus - Hopewell	
Danbury	Tutor - Terminus	
Deer Lake	West terminal - Stone Dr	excellent
Deer Run	Deer Lake - Chipmunk	good
Deer Run	Deer Lake - Chipmunk	good
Deerlake	(Stone - Fox Cross)	good
Deerlake	Deer Run - Broad	good
Deerlake	Eldridge - Broad	excellent
Deerlake	Fox Cross - Deer Run	good
East Main St. Ext.	Wilson / Main / Greenville - Terminus	fair
Elm Bend	Greenville Hwy - Throwbridge / Magnolia	fair
Elm Bend	Throwbridge / Magnolia - Sapphire Manor	fair
England	(Main - Probart)	good
English Hills	Picklesimer Ave. - English Hills Project	excellent
Fisher	(between Montview intersections)	good
Fisher	(Montview - Broad)	good
Fisher	(Poplar - Montview)	good
Fisher	Brevard Middle School	
Franklin	Lakeview - French Broad (N)	fair
Franklin	Lakeview - Lakeview (S)	poor
Franklin	Main - Lakeview (N)	fair
Franklin	Main - Lakeview (S)	poor
French Broad	(Appletree - Franklin)	good
French Broad	(Broad - Johnson)	good
French Broad	(Caldwell - Broad)	poor
French Broad	(Franklin - Hilt)	good
French Broad	(Johnson - Appletree)	good
French Broad	(Railroad - Salem)	poor
French Broad	(Railroad - Salem)	poor
French Broad	(Salem - Caldwell)	poor
French Broad	(Salem - Caldwell)	poor
French Broad	Hilt - Woodlawn	good
French Broad	Woodlawn - Parkview	good

Street	Section	Overall Condition
Gaston	(Jordan - Main)	good
Gaston	(Jordan - Main)	good
Gaston	(Main - Probart)	good
Gaston	(Main - Probart)	good
Gaston	(Maple - Morgan)	good
Gaston	(Maple - Morgan)	good
Gaston	(Morgan - Jordan)	good
Gaston	(Morgan - Jordan)	good
Greenville Hwy	(Elm Bend - Hrowbridge)	good
Greenville Hwy	(Hrowbridge - sidewalk to Brevard Elementary)	fair
Greenville Hwy	E. Main - Elm Bend	poor
Greenville Hwy	sidewalk to Brevard Elementary	excellent
Hillview Ave	(Private Dr. - Picklesminer Ave)	fair
Hillview Ave		good
Hillview Ave		good
Hopewell	Knight - Danbury	
Hopewell	London - Knight	
Hopewell	Osborne - London	
Hospital Drive	Hospital Drive - Mid-Block	excellent
Hospital Drive - Allison Creek Bridge	Hospital Drive - Allison Creek Bridge	excellent
Johnson	(Appletree - Fr. Broad)	excellent
Johnson	(Jordan - Main)	good
Johnson	(Main - Probart)	excellent
Johnson	(Probart - Appletree)	good
Jordan	(Broad - Gaston) (N)	good
Jordan	(Broad - Gaston) (S)	good
Jordan	(Caldwell - Broad) (N)	good
Jordan	(Caldwell - Broad) (S)	good
Jordan	(England - Caldwell) (N)	good
Jordan	(England - Caldwell) (S)	good
Jordan	(Gaston - Johnson) (N)	
Jordan	(Oaklawn - S. England St.) (N)	good
Jordan	(Oaklawn - S. England St.) (S)	good
King	Floyd - Caldwell	poor
King	Railroad - Floyd (N)	poor
King	Railroad - Floyd (S)	
King	Railroad - Floyd (S)	
Knight Ct.	Hopewell - Terminus	good
London	Hopewell - Terminus	good
Main Street (E)	(Broad - Gaston) (N)	excellent
Main Street (E)	(Broad - Gaston) (S)	excellent

Street	Section	Overall Condition
Main Street (E)	(Franklin - Park) (N)	fair
Main Street (E)	(Franklin - Park) (S)	fair
Main Street (E)	(Gaston - Johnson) (N)	excellent
Main Street (E)	(Gaston - Johnson) (S)	excellent
Main Street (E)	(Johnson - Rice St.) (N)	good
Main Street (E)	(Johnson - Rice St.) (S)	good
Main Street (E)	(Rice - Franklin) (N)	good
Main Street (E)	Franklin - Woodside (S)	
Main Street (E)	Park - Greenville Hwy / E. Main	fair
Main Street (E)	Park - Greenville Hwy / E. Main	poor
Main Street (E)	Rice - St. Phillips (S)	good
Main Street (E)	St. Phillips - Franklin (S)	good
Main Street (W)	(Caldwell - Broad) (N)	
Main Street (W)	(Caldwell - Broad) (S)	excellent
Main Street (W)	(England - Caldwell) (N)	
Main Street (W)	(England - Caldwell) (S)	excellent
Main Street (W)	(Galloway - Oaklawn Ave)	
Main Street (W)	(Galloway - Oaklawn Ave) (N)	good
Main Street (W)	(Oaklawn - S. England St.)	
Main Street (W)	(Oaklawn - S. England St.) (N)	excellent
Maple	(Miner - Johnson (E)	fair
Maple	Batson - Willow / Grove (W)	fair
Maple	Grove - Miner (E)	good
Maple	Johnson - Gaston (E)	fair
Maple	Johnson - Gaston (W)	fair
Maple	Memory - Batson	fair
Maple	Miner - Johnson (W)	fair
Maple	Turnpike - Miner (W)	good
Maple	Willow / Grove (E) - Grove (midblock)	good
Maple	Willow / Grove (W) - Grove (midblock)	good
Meadow View	Camp Sapphire - Straus Parkway (E)	excellent
Meadow View	Camp Sapphire - Straus Parkway (W)	excellent
Meadow View	Camp town - Camp Sapphire (E)	excellent
Meadow View	Camp town - Camp Sapphire (W)	excellent
Mills	(Cashiers Valley - Private Dr.)	good
Mills	(Picklesminer Ave - Cashiers Valley)	poor
Mills	(Private Dr. - Galloway)	fair
Morgan	Broad - Gaston (N)	good
Morgan	Broad - Gaston (S)	excellent
Morgan	Caldwell - Broad (N)	fair
Morgan	Caldwell - Broad (S)	good

Street	Section	Overall Condition
Morgan	Carver / Hamlin / Oaklawn - Duckworth (S)	fair
Morgan	Duckworth - Caldwell (N)	good
Morgan	Duckworth - Caldwell (S)	fair
Neely Rd / Park View	(Greenville Hwy. - Private Dr.)	good
Neely Rd / Park View	(Park - Fr. Broad)	good
Neely Rd / Park View	Private Dr. - Park (NW)	poor
Neely Rd / Park View	Private Dr. - Park (SE)	poor
Oakdale Ln	(Caldwell - Broad)	poor
Oakdale Ln	Carver - Jenkins	fair
Oakdale Ln	Carver - Jenkins (N)	fair
Oakdale Ln	Duckworth Ave. - Orange	poor
Oakdale Ln	Hemphill Cr. - Duckworth Ave	poor
Oakdale Ln	Jenkins - Hemphill (S)	fair
Oakdale Ln	Orange - Caldwell	poor
Oaklawn Ave	(Main - Probart)	poor
Oaklawn Ave	(Morgan - Fortune)	excellent
Oaklawn Ave	Fortune - Main	good
Oaklawn Ave	Jordan - Morgan (W)	
Old Cottage Ln	Mid-Block - Cul-de-sac	excellent
Old Cottage Ln	Straus Parkway - Midblock	excellent
Park Ave	(Boxwood - Harold)	poor
Park Ave	(Harold - Parkview)	poor
Park Ave	(Main - Box Wood)	good
Park Ave	Boxwood - Harold (N)	poor
Park Ave	Main - Box Wood (N)	good
Pine Mountain	(Camp Town - Camp Straus)	excellent
Pisgah Forest Elementary		
Probart St.	(Caldwell - Broad) (N)	good
Probart St.	(Caldwell - Broad) (S)	good
Probart St.	(England - Caldwell) (N)	good
Probart St.	(England - Caldwell) (S)	good
Probart St.	(Railroad - Oaklawn) (N)	good
Probart St.	(Railroad - Oaklawn) (S)	good
Probart St.	Broad - Courthouse (S)	good
Probart St.	Oaklawn - England) (N)	good
Probart St.	Oaklawn - England) (S)	good
Rail Road / Asheville Hwy Connector	Along Kings Creek	
Rail Road Ave	Probart - King	poor
Sheffield Pl	Sheffield (N) - Cambridge	excellent
Sheffield Pl	Sheffield (S) - Cambridge	excellent
Silversteen		excellent

Street	Section	Overall Condition
Silversteen		excellent
Springhouse Trail		excellent
Straus Parkway	(Meadow - Stone House)	excellent
Straus Parkway	Asheville Hwy - Old Cottage Ln	excellent
Straus Parkway	Pine Mtn - Meadow	excellent
Straus Parkway	Private Drive - Asheville Hwy	excellent
Straus Parkway	Stone House - Private Drive	excellent
Throwbridge	Elm Bend - Manor (W))	good
Throwbridge	Elm Bend - Sapphire (E)	good
Throwbridge	Manor - Greenville Hwy (W)	good
Throwbridge	Sapphire - Greenville Hwy (E)	good
Timber Trace		excellent
Tutor	Danbury - Terminus	excellent
Unity		fair
Unity		good
Unity		good
Unity		good
Unity		
Unity		good
Unity		good
Unity		
Whitmire	(Palmer - Tinsley)	poor
Whitmire	(Railroad - Salem)	poor
Whitmire	(Tinsley - Railroad)	poor
Whitmire	College Station - Caldwell (N)	poor
Whitmire	Kingsmill / Burrel Mtn - Palmer	poor
Whitmire	Railroad - Salem (S)	poor
Whitmire	Salem - Caldwell (S)	poor
Whitmire	Salem - College Station	poor

**NON-GOVERNMENTAL ORGANIZATIONS CONTACT LIST
BREVARD, NORTH CAROLINA**

Organization	Position	Representative	Address	City, ZIP	Phone	Email
Boys and Girls Club	Director	Jeff Green	PO Box 1360	Brevard, NC 28712		
Girls Scout		Carol Sinfield	1299 New Hendersonville Hwy.	Brevard, NC 28712	(828) 966-4938	
		Gail Vance	111 Ashworth Avenue	Brevard, NC 28712	(828) 884-6474	
MOPS (Mothers of Preschoolers)		Karen Murr	97 Heather Lane	Brevard, NC 28712		
Chamber of Commerce	Director	Beth Carden	35 West Main Street	Brevard, NC 28712		
Heart of Brevard	Director	Marqaret Woodard	6 Times Arcade	Brevard, NC 28712		
Sierra Club - County Commissioners	Chair	Ray Miller	PO Box 28	Brevard, NC 28712	(828) 883-9430	MarieMiller@citcom.net
		Kelvin Phillips	PO Box 28	Brevard, NC 28712	966-9698	
		Jeff Duval	PO Box 28	Brevard, NC 28712	966-9698(H)	
		Jason R. Chappell	PO Box 28	Brevard, NC 28712	883-8153(W)	CJD2@citcom.net
		W. David Guice	PO Box 28	Brevard, NC 28712	862-6679	
Brevard Elementary School OPT	President	Jennifer Christian	c/o Brevard Elementary School 601 Greenville Hwy.	Brevard, NC 28712	883-4993	Guice@citcom.net
Pisgah Forest Elementary School ROPE		Lynn Harron	1160 Ecusta Road	Brevard, NC 28712		
Brevard Middle School PTO		Camille VanHorn	198 Fisher Road	Brevard, NC 28712		
Brevard Academy	Principal	Tom Mahan	1000 Probart Street Ext.	Brevard, NC 28712		
	Vice-Principal	Al Evans	1000 Probart Street Ext.	Brevard, NC 28712		
Rotary International - Brevard Club	President	John Candler	349 Andante Lane	Brevard, NC 28712	828-883-2920	
Brevard Music Center	Director of Marketing & Development	Gary Himes	349 Andante Lane	Brevard, NC 28712	828-862-2100	JCandler@brevardmusic.org
	Director of Operations & Finance	William Boggs	349 Andante Lane	Brevard, NC 28712		ghimes@brevardmusic.org
						billboggs@brevardmusic.org

**CITY OF BREVARD AND TRANSYLVANIA COUNTY CONTACT LIST
BREVARD, NORTH CAROLINA**

Board/Committee	Position	Representative	Address	City, ZIP	Phone	Email	
Transylvania County Planning Board	Chair	Clentis Williams Jon Wesley	PO Box 850 8089 Greenville Hwy.	Pisgah Forest, NC 28768 Brevard, NC 28712			
		Terry Crowe	PO Box 1143	Rosman, NC 28772			
		Jimmy Perkins Parker Platt	101 E. Probart St. 33 W. Main Street	Brevard, NC 28712 Brevard, NC 28712			
		John Dornier, III	13850 Greenville Hwy.	Cedar Mtn., NC 28718			
		Mason Sexton	840 Walnut Hollow Rd.	Brevard, NC 28712			
	Transylvania Planning & Economic Development Brevard Planning Board	Director	Mark Burrows	PO Box 1578	Brevard, NC 28712		
		Chair	Karla Atkinson	506 Stone Drive	Brevard, NC 28712		
	City of Brevard Recreation Advisory Committee	No Chair					
	City Council	Mayor	Jimmy Harris	Harris Hardware 123 W. Main St.	Brevard, NC 28712	828-883-2103 (W) 828-884-6501 (H)	c13172@citcom.net
			Dee Dee Perkins	101 East Probart St.	Brevard, NC 28712	828-862-4700 (W) 828-877-3804 (H)	dcbulwinkle@citcom.net
		Mac McKeller	39 East Main St.	Brevard, NC 28712	828-877-5502 (O)	mmckeller@citcom.net	
		Mac Morrow	563 East Main St.	Brevard, NC 28712	828-883-8547	morrow@keirmfg.com	
		Sara Champion	106 Pine Street	Brevard, NC 28712	828-862-4505 (H) 828-877-6606	sachampion@citcom.net	
		Rodney Locks	121 South Rice St.	Brevard, NC 28712	828-883-3985 (H)	rfl@citcom.net	
Brevard Board of Adjustment		Chair	John Erkkila	17 Sheffield Place	Brevard, NC 28712		
			Randy Baron	12 Park Avenue	Brevard, NC 28712	828-885-2041 (W) 828-862-6699 (W)	strauspark@citcom.net
UDO Committee			Mike Shotwell	107 Green Acre Lane	Brevard, NC 28712	828-884-2329 (H) 828-884-2091 (BMS)	mshotwel@transylvania.k12.nc.us
			Margaret Garren	624 Lost Mine Trail	Brevard, NC 28712	828-884-7020 (W)	margaretroy@citcom.net
		Angus Graham	1 Elks Club Road	Brevard, NC 28712	828-883-2732 (H)	acsurfs@yahoo.com	
		Mac McKeller	39 East Main St.	Brevard, NC 28712	828-884-2729 (H) 828-877-5502 (O)	mmckeller@citcom.net	
		Mac Morrow	563 East Main St.	Brevard, NC 28712	828-883-8547	morrow@keirmfg.com	
		Parker Pace	355 Big Bend Road	Brevard, NC 28712	828-883-3275 (H)	ppace@citcom.net	
		Jimmy Perkins	101 East Probart St.	Brevard, NC 28712	828-884-9111 (W) 828-877-3804 (H)	jed2perkins@citcom.net	
		Al Patt	33 W. Main Street	Brevard, NC 28712	828-884-2393 (O) 828-884-3628 (H)	al_aparc@citcom.net madidd_aparc@citcom.net	

**CITY OF BREVARD AND TRANSYLVANIA COUNTY CONTACT LIST
BREVARD, NORTH CAROLINA**

Board/Committee	Position	Representative	Address	City, ZIP	Phone	Email
		Pete Peters	5 Four Seasons Plaza	Brevard, NC 28712	828-884-9372 (O) 828-966-4583 (H)	pheters@citcom.net
		Mike Young	Falls Landing 23 East Main St.	Brevard, NC 28712	828-884-2835 (W) 828-506-3246 (C) 828-877-6171 (H)	my@citcom.net
		Bill Boggs	PO Box 1216	Brevard, NC 28712	828-862-211 (W) 828-884-2015 (H)	billboggs@citcom.net
		Roy Cowan	552 Sugar Loaf Road	Brevard, NC 28712	828-553-1089 (C)	roycowan@citcom.net
		Kimsey Jackson	PO Box 207	Brevard, NC 28712	828-884-7169 (H) 828-884-3791 (H)	
Downtown Master Plan Advisory Committee				Brevard, NC 28712		
Transylvania County Recreation Department Pedestrian Plan Project Steering Committee	Director	Morris Denton	137 Glen Cannon Drive	Pisgah Forest, NC 28768		
		Rick Pangle	1150 Ecusta Road	Brevard, NC 28712		
	Chair	Josh Freeman	151 West Main St.	Brevard, NC 28712	828-883-8580	

**MEDIA CONTACT LIST
BREVARD, NORTH CAROLINA**

Media	Contact Person	FAX	Phone	Email
Transylvania Times CATV	Derek McKissock	883-8158 885-2300	883-8156	ttimes@infoave.net
Hendersonville Times News WSQL Radio Station	John Rich Allen Reese	692-2319	884-5092, 877-5252	jonathan.rich@hendersonvilletimesnews.com aaandl@citcom.net
Pisgah Mountain News	Katheleen O. Davis - editor Phil Hensley II		699-3970 (cell)	k.davis@pisgahmountainnews.com phensle@pisgahmountainnews.com
Mountain Voice	Elaine Reynolds - reporter			

**LOCAL STORES AND OUTFITTERS CONTACT LIST
BREVARD, NORTH CAROLINA**

Board/Committee	Address	City, ZIP	Phone
White Squirrel Shoppe	2 W Main St.	Brevard, NC 28712	(828) 877-3530
Davidson River Outfitters Co	26 Pisgah Hwy	Pisgah Forest, NC 28768	
Celestian Mountain Music	16 W Main St	Brevard, NC 28712	(828) 884-3575
The Forest Place	100 S Broad St	Brevard, NC 28712	(828) 884-5713

**CITY OF BREVARD CHURCHES CONTACT LIST
BREVARD, NORTH CAROLINA**

Church	Address	City, ZIP
Brevard Church of Christ	PO Box 784	Brevard, NC 28712
Brevard Community Church	318 W French Broad St.	Brevard, NC 28712
Church of God GOPA The	2 Burrell Mountain Rd.	Brevard, NC 28712
Joy Outreach Fellowship	3200 New Hendersonville Hwy.	Brevard, NC 28712
Anchor Baptist Church & Ministries	3400 New Hendersonville Hwy.	Brevard, NC 28712
Bethel "A" Baptist Church	92 Oakdale St.	Brevard, NC 28712
Bethel Baptist Church	20 Mills Av.	Brevard, NC 28712
Brevard Community Church	1020 Aheville Hwy.	Brevard, NC 28712
Calvary Baptist Church	101 Osborne Rd.	Brevard, NC 28712
Carr's Hill Baptist Church	3075 Greenville Hwy	Brevard, NC 28712
First Baptist Church	122 Gaston St.	Brevard, NC 28712
Bethany Baptist Church & School	200 Bert Lane Road	Brevard, NC 28712
Macedonia Baptist Church	Silversteen Rd	Brevard, NC 28712
Sacret Heart Catholic Church	4 Brian Berg Ln	Brevard, NC 28712
Christian Science Society	235 W Main St	Brevard, NC 28712
St. Philips Episcopal Church	317 E Main St.	Brevard, NC 28712
Lutheran Church of the God Shepherd	808 N Broad St.	Brevard, NC 28712
Christ Covenant Church	250 Rosman Hwy.	Brevard, NC 28712
Brevard-Davidson River Presbyterian Church	300 E Main St.	Brevard, NC 28712
Cornerston Presbyterian Church	400 Elm Bend Rd.	Brevard, NC 28712
First United Methodist Church	500 N Broad St	Brevard, NC 28712
Selica United Methodist Church	529 Rosman Hwy.	Brevard, NC 28712
Mt. Moriah Calvert Baptist	1644 Calvert Rd.	Brevard, NC 28712
Dunn's Rock Baptist Church	109 Connestee Rd.	Brevard, NC 28712
New Hope Baptist	PO Box 1257	Brevard, NC 28712
Brevard First Baptist Church	122 Gaston St.	Brevard, NC 28712
Carson's Creek Baptist	4727 Connestee Trail	Brevard, NC 28712
Cherryfield Baptist Church	1290 Whitmitre Rd.	Brevard, NC 28712
Little Cove Baptist Church	PO Box 1194	Brevard, NC 28712
Morningside Baptist	106 Outland Ave.	Brevard, NC 28712
Cathey's Creek Baptist	722 Cathey's Creek Church Rd.	Brevard, NC 28712
Dunn's Creek Baptist Church	Rt. 1 Box 30-2	Brevard, NC 28712
Grace Baptist Church	120 McLean Rd.	Brevard, NC 28712
Affirm Word Church	1528 Calvert. Rd.	Brevard, NC 28712
Church of Nazarene	1200 Old Hendersonville Hwy.	Brevard, NC 28712
Full Gospel Baptist Church	PO Box 226	Brevard, NC 28712
Free Will Baptist Church	450 Whitmire St.	Brevard, NC 28712
Mid Way Baptist Church	886 Cathey's Creek Church Rd.	Brevard, NC 28712
Cornerstone Presbyterian	300 Water Oak Suite	Brevard, NC 28712
Grace Govenant Presbyterian	250 Rosman Hwy.	Brevard, NC 28712
St. Timothy United Methodist	PO Box 429	Brevard, NC 28712
Brevard Wesleyan Church	PO Box 632	Brevard, NC 28712
Sacred Heart Catholic Church	4 Fortune Cove	Brevard, NC 28712
Community Baptist	200 A Buena Vista Dr.	Brevard, NC 28712
Open Arms Tabernacle	25 McLean Rd.	Brevard, NC 28712

Number of Surveys mailed via City Newsletter 3,528
 Number of Surveys returned 167
 Response Rate 4.73%

Reason for >100%

Days per week

Taking a walk	169	101.20%
0	13	7.69%
1	5	2.96%
2	11	6.51%
3	30	17.75%
4	24	14.20%
5	36	21.30%
6	17	10.06%
7	33	19.53%

multiple people filling out same form

The majority of respondents walk 3-5 days per week.

Running/Jogging	168	100.60%
0	142	84.52%
1	4	2.38%
2	3	1.79%
3	8	4.76%
4	6	3.57%
5	2	1.19%
6	2	1.19%
7	1	0.60%

The majority of respondents don't run/jog, but of those who do, the majority does so 3 or 4 days per week.

Skating (in-line or other)	168	100.60%
0	163	97.02%
1	3	1.79%
4	1	0.60%
7	1	0.60%

The majority of respondents don't skate, but of those who do, the majority does so 1 day per week.

Using a motorized scooter outside home	168	100.60%
0	168	100.00%

None of the respondents use a motorized scooter.

Using a manual wheelchair outside home	168	100.60%
0	168	100.00%

None of the respondents use a manual wheelchair.

Using a motorized wheelchair outside home	168	100.60%
0	167	99.40%
7	1	0.60%

One respondent uses a motorized wheelchair.

Cycling	168	100.60%
0	125	74.40%
1	14	8.33%
2	11	6.55%
3	6	3.57%
4	4	2.38%
5	4	2.38%
7	4	2.38%

The majority of respondents don't cycle, but of those who do, the majority does so 1 or 2 days per week.

Hours per week

Taking a walk	170	101.80%	
0	20	11.76%	
<1	3	1.76%	
1-<2	11	6.47%	
2-<3	18	10.59%	
3-<4	27	15.88%	
4-<5	20	11.76%	
5-<6	17	10.00%	
6-<7	15	8.82%	
7-<8	13	7.65%	
8-<9	4	2.35%	
9-<10	2	1.18%	
10-<11	7	4.12%	
12-<13	4	2.35%	
14-<15	2	1.18%	
15-<16	4	2.35%	
18-<19	1	0.59%	
	25	1	0.59%
	48	1	0.59%

The majority of respondents walk 2-7 hours per week.

Running/Jogging	167	100.00%
0	140	83.83%
<1	1	0.60%
1-<2	7	4.19%
2-<3	8	4.79%
3-<4	4	2.40%
4-<5	2	1.20%
5-<6	2	1.20%
6-7	1	0.60%

The majority of respondents don't run/jog, but of those who do, the majority does so 1-4 hours per week.

12-<13	1	0.60%
16-<17	1	0.60%

Skating (in-line or other)		168	100.60%
	0	164	97.62%
<1		1	0.60%
1-<2		1	0.60%
2-<3		1	0.60%
4-<5		1	0.60%
Using a motorized scooter outside home		168	100.60%
	0	168	100.00%
Using a manual wheelchair outside home		168	100.60%
	0	168	100.00%
Using a motorized wheelchair outside home		168	100.60%
	0	167	99.40%
10-<11		1	0.60%
Cycling		167	100.00%
	0	125	74.85%
<1		2	1.20%
1-<2		14	8.38%
2-<3		10	5.99%
3-<4		1	0.60%
4-<5		3	1.80%
5-<6		1	0.60%
6-<7		2	1.20%
7-<8		3	1.80%
10-<11		3	1.80%
12-<13		2	1.20%
15-<16		1	0.60%
How often do you walk 10 minutes or more		149	89.22%
DK		9	6.04%
<1		25	16.78%
1-<2		7	4.70%
2-<3		17	11.41%
3-<4		15	10.07%
4-<5		7	4.70%
5-<6		12	8.05%
6-<7		5	3.36%
7-<8		9	6.04%
8-<9		7	4.70%
10-<11		12	8.05%
12-<13		2	1.34%
14-<15		7	4.70%
15-<16		6	4.03%
18-<19		1	0.67%
	20	3	2.01%
>20		5	3.36%
	30	1	0.67%
	36	1	0.67%
	50	1	0.67%
	120	1	0.67%
<i>Do you use any of the following</i>			
Cane		157	94.01%
Y		8	5.10%
N		148	94.27%
DK		1	0.64%
Seeing-eye dog		154	92.22%
N		154	100.00%
Wheelchair		154	92.22%
N		154	100.00%
<i>Why do you engage in ped activities</i>			
At work		149	89.22%
Y		39	26.17%
N		106	71.14%
DK		4	2.68%
On the way to/from work		149	89.22%
Y		33	22.15%
N		113	75.84%
DK		3	2.01%
On the way to/from public transportation		146	87.43%
Y		7	4.79%
N		138	94.52%
DK		1	0.68%

The majority of respondents don't skate, but those who do skate anywhere from less than an hour to 5 hours per week.

None of the respondents use a motorized scooter.

None of the respondents use a manual wheelchair.

One respondent uses a motorized wheelchair 10-11 hours per week.

The majority of respondents don't cycle, but of those who do, the majority does so 1-3 hours per week.

There was a wide range of answers to this question.

*Most respondents do not use a cane.
One respondent checked two boxes.

*None of the respondents use a seeing-eye dog.
None of the respondents said they used a wheelchair, though one said they used a motorized wheelchair above.*

*Most respondents do not walk at work.
Some respondents checked DK because they don't work.

*Most respondents do not walk to/from work.
Some respondents checked DK because they don't work.

Most respondents do not walk to/from public transportation, though some mentioned they would if it was available.

Walking to/from school	146	87.43%	
Y	11	7.53%	
N	133	91.10%	
DK	2	1.37%	
Running errands/shopping	164	98.20%	
Y	139	84.76%	
N	24	14.63%	
DK	1	0.61%	
For exercise	163	97.60%	
Y	152	93.25%	
N	10	6.13%	
DK	1	0.61%	
<i>Where do you engage in ped activities</i>			
At a park	147	88.02%	
Y	60	40.82%	
N	84	57.14%	
DK	3	2.04%	
On a walking trail or path	155	92.81%	
Y	107	69.03%	
N	46	29.68%	
DK	2	1.29%	
On school grounds or campus	150	89.82%	
Y	62	41.33%	
N	88	58.67%	
On sidewalks, streets, or roads in your neighborhood	167	100.00%	
Y	159	95.21%	
N	8	4.79%	
On sidewalks, streets, or roads NOT in your neighborhood	155	92.81%	
Y	98	63.23%	
N	56	36.13%	
DK	1	0.65%	
At your work place	149	89.22%	
Y	34	22.82%	
N	113	75.84%	
DK	2	1.34%	
<i>Do any of the following keep you from walking more?</i>			
Too busy	153	91.62%	
Y	52	33.99%	
N	98	64.05%	
DK	3	1.96%	
Poor health	154	92.22%	
Y	11	7.14%	
N	138	89.61%	
DK	5	3.25%	
No one to walk with	152	91.02%	
Y	12	7.89%	
N	139	91.45%	
DK	1	0.66%	
Dogs or other animals	153	91.62%	
Y	24	15.69%	
N	127	83.01%	
DK	2	1.31%	
No nearby paths or trails	156	93.41%	
Y	36	23.08%	
N	117	75.00%	
DK	3	1.92%	
No nearby parks	154	92.22%	
Y	28	18.18%	
N	124	80.52%	
DK	2	1.30%	
No nearby sidewalks	155	92.81%	
Y	54	34.84%	
N	98	63.23%	
DK	3	1.94%	
Unsafe street crossings	155	92.81%	
Y	55	35.48%	
N	97	62.58%	
DK	3	1.94%	
No nearby shops or other interesting places to go	149	89.22%	
Y	9	6.04%	
N	136	91.28%	
DK	4	2.68%	

Most respondents do not walk to/from school, but no college, and few other, students responded.

Most respondents walk when running errands/shopping.

Most respondents walk for exercise.

Most respondents do not walk at a park, but many do.

Most respondents walk on a trail or path.

Most respondents do not walk on school grounds or campuses, but many do. No college students responded.

Most respondents walk on sidewalks, streets, or roads in their neighborhood.

Most respondents walk on sidewalks, streets, or roads outside their neighborhood.

**One respondent checked two boxes.*

Most respondents do not walk at work.

**2 respondents checked DK because they do not work.*

**DK usually resulted from respondents checking multiple boxes.*

Most respondents do not feel they are too busy to walk.

Most respondents are in good health.

Most respondents do not feel they need someone to walk with.

Dogs/other animals do not keep most respondents from walking.

The lack of nearby paths/trails does not keep most respondents from walking.

The lack of nearby parks does not keep most respondents from walking.

The lack of nearby sidewalks does not keep most respondents from walking.

Unsafe street crossings do not keep most respondents from walking.

The lack of nearby shops/interesting places to go does not prevent most respondents from walking.

Not enough people walking around	151	90.42%	
Y	6	3.97%	
N	142	94.04%	<i>Most respondents walk regardless of the number of people walking around.</i>
DK	3	1.99%	
Fear of street crime	151	90.42%	
Y	6	3.97%	
N	144	95.36%	<i>Most respondents are not afraid of street crime in Brevard.</i>
DK	1	0.66%	
Too many cars	157	94.01%	
Y	67	42.68%	
N	89	56.69%	<i>Most respondents walk regardless of the number of cars, but many respondents believe there are too many cars.</i>
DK	1	0.64%	
Fast traffic	159	95.21%	
Y	82	51.57%	<i>Most respondents are deterred by fast traffic, but many are not.</i>
N	75	47.17%	
DK	2	1.26%	
Air pollution	154	92.22%	
Y	18	11.69%	
N	132	85.71%	<i>Most respondents do not believe air pollution is an issue in Brevard.</i>
DK	4	2.60%	
Lighting issues	153	91.62%	
Y	25	16.34%	
N	124	81.05%	<i>Most respondents walk regardless of lighting issues.</i>
DK	4	2.61%	
Unsafe or poorly maintained sidewalks	153	91.62%	
Y	44	28.76%	
N	103	67.32%	<i>Most respondents are not deterred by unsafe/poorly maintained sidewalks.</i>
DK	6	3.92%	
Access to sidewalks and greenways are an important factor to me in choosing a place to live.	163	97.60%	
A	140	85.89%	<i>Access to sidewalks and greenways is important to many of the respondents when choosing a place to live.</i>
D	17	10.43%	
DK	6	3.68%	
My neighborhood has safe and attractive areas for walking	165	98.80%	
A	121	73.33%	<i>Most respondents believe their neighborhoods have safe and attractive areas for walking.</i>
D	41	24.85%	
DK	3	1.82%	
I cannot walk in my neighborhood because there are no sidewalks, or existing sidewalks are not safe	157	94.01%	
A	37	23.57%	
D	114	72.61%	<i>Most respondents feel they can walk in their neighborhood because there are safe sidewalks.</i>
DK	6	3.82%	
I would like to live in a place where I can walk to work	143	85.63%	
A	86	60.14%	<i>Most respondents would like to live in a place where they can walk.</i>
D	29	20.28%	<i>*Many marked Disagree or DK because they do not work.</i>
DK	28	19.58%	
I would like to live in a place where I can walk to shop	157	94.01%	
A	123	78.34%	<i>Most respondents would like to live in a place where they can walk to shop.</i>
D	23	14.65%	
DK	11	7.01%	
Brevard has an adequate number of sidewalks	162	97.01%	<i>*Answers to this question tend to show how many respondents live in areas with sidewalks.</i>
A	50	30.86%	
D	94	58.02%	<i>Most respondents do not think there is an adequate number of sidewalks in Brevard.</i>
DK	18	11.11%	
The sidewalks in Brevard are in good condition	161	96.41%	
A	76	47.20%	<i>Most respondents think the sidewalks that exist in Brevard are in good condition, but many do not. *DK sometimes meant that respondents noted that conditions varied throughout the city.</i>
D	63	39.13%	
DK	22	13.66%	
<i>Factors in walking</i>			

Availability of sidewalks	168	100.60%	
I	155	92.26%	<i>Most respondents feel that availability of sidewalks is important.</i>
U	13	7.74%	
DK	7	4.29%	
Availability of benches	163	97.60%	
I	65	39.88%	<i>Most respondents feel that availability of benches is unimportant, but many feel it is or will be important in the future.</i>
U	91	55.83%	
DK	7	4.29%	
Availability of shady streets and sidewalks	164	98.20%	
I	126	76.83%	<i>Most respondents feel that availability of shady streets/sidewalks is important.</i>
U	35	21.34%	
DK	3	1.83%	
Destinations that are a reasonable distance away	165	98.80%	
I	135	81.82%	<i>Most respondents feel that it is important to have destinations that are a reasonable distance away.</i>
U	24	14.55%	
DK	6	3.64%	
Keeping a safe distance from traffic	165	98.80%	
I	152	92.12%	<i>Keeping a safe distance from traffic is important to most of the respondents.</i>
U	11	6.67%	
DK	2	1.21%	
Personal safety	163	97.60%	
I	113	69.33%	<i>Personal safety is important to most respondents.</i>
U	43	26.38%	
DK	7	4.29%	
Easy access to safe street crossings	166	99.40%	
I	147	88.55%	<i>Easy access to safe street crossings is important to most of the respondents.</i>
U	17	10.24%	
DK	2	1.20%	
Convenient access to paved trails	162	97.01%	
I	113	69.75%	<i>Convenient access to paved trails is important to most of the respondents.</i>
U	41	25.31%	
DK	8	4.94%	
Weather conditions	163	97.60%	
I	104	63.80%	<i>Weather conditions are an important factor in most respondents' decision to walk.</i>
U	53	32.52%	
DK	6	3.68%	
Attractive surroundings	163	97.60%	
I	134	82.21%	<i>Attractive surroundings are an important factor in most of the respondents' decision to walk.</i>
U	26	15.95%	
DK	3	1.84%	
Adequate street lighting	163	97.60%	
I	113	69.33%	<i>Adequate street lighting is an important factor in the decision of most respondents to walk.</i>
U	44	26.99%	
DK	6	3.68%	
Rude motorists	160	95.81%	
I	73	45.63%	<i>Rude motorists are nearly equally important and unimportant in the decision of respondents to walk.</i>
U	68	42.50%	
DK	19	11.88%	
Lack of access to parking	159	95.21%	
I	39	24.53%	<i>Lack of access to parking is unimportant to most respondents as a factor in their decision to walk.</i>
U	103	64.78%	
DK	17	10.69%	
Painted crosswalks	167	100.00%	
I	92	55.09%	<i>Most respondents said that painted crosswalks were an important factor in their decision to walk, while many said they were unimportant.</i>
U	68	40.72%	
DK	7	4.19%	
Signs or signals that make drivers stop	163	97.60%	
I	124	76.07%	<i>Signs/signals making drivers stop were an important factor in the decision of most respondents to walk.</i>
U	34	20.86%	
DK	5	3.07%	
Having other pedestrians nearby	159	95.21%	
I	49	30.82%	<i>Having other pedestrians nearby was unimportant to most respondents.</i>
U	100	62.89%	
DK	10	6.29%	
Objects blocking passage of sidewalks	161	96.41%	
I	59	36.65%	<i>Objects blocking passage of sidewalks were unimportant to most respondents, but many said that was an important factor in their decision of where to walk.</i>
U	87	54.04%	
DK	15	9.32%	

Push buttons at crosswalk signals	162	97.01%	
I	76	46.91%	
U	79	48.77%	
DK	7	4.32%	
Audio crossing signals	159	95.21%	
I	34	21.38%	
U	115	72.33%	
DK	10	6.29%	
Curb cuts at corners or curb ramps	161	96.41%	
I	74	45.96%	
U	79	49.07%	
DK	8	4.97%	
Amount of traffic	164	98.20%	
I	112	68.29%	
U	46	28.05%	
DK	6	3.66%	
Speed of traffic	163	97.60%	
I	141	86.50%	
U	18	11.04%	
DK	4	2.45%	
Traffic signs telling drivers to slow down or watch for pedestrians	164	98.20%	
I	120	73.17%	
U	38	23.17%	
DK	6	3.66%	
Handicap accessible sidewalks/greenways	162	97.01%	
I	78	48.15%	
U	74	45.68%	
DK	10	6.17%	
Rate importance of pedestrian improvements	154	92.22%	
	1	102	66.23%
	2	34	22.08%
	3	11	7.14%
	4	7	4.55%
<i>Demographics</i>			
How long have you lived in Brevard? _ years	167	100.00%	
<1	5	2.99%	
1-<2	10	5.99%	
2-<3	6	3.59%	
3-<4	15	8.98%	
4-<5	10	5.99%	
5-<6	6	3.59%	
6-<7	8	4.79%	
7-<8	6	3.59%	
8-<9	4	2.40%	
9-<10	4	2.40%	
	10	11	6.59%
	11	4	2.40%
	12	4	2.40%
	13	5	2.99%
	14	4	2.40%
	15	5	2.99%
	16	7	4.19%
	17	2	1.20%
	18	3	1.80%
	19	2	1.20%
	20	4	2.40%
>20	42	25.15%	
	22	1	0.60%
	23	2	1.20%
	24	4	2.40%
	25	6	3.59%
	27	2	1.20%
	28	3	1.80%
	29	1	0.60%
	30	2	1.20%
	31	1	0.60%
	32	1	0.60%
	34	1	0.60%

Push buttons at crosswalks were almost equally believed to be important and unimportant in respondents' decision to walk.

Audio crossing signals were unimportant to most respondents in their decision to walk.

Curb cuts/ramps at corners were almost equally believed to be important and unimportant in respondents' decision to walk.

Amount of traffic was an important factor to most respondents in their decision to walk.

Speed of traffic was an important factor to most respondents in their decision to walk.

Traffic signs telling drivers to slow down or watch for pedestrians was important to most respondents in their decision to walk.

Handicap accessible sidewalks/greenways were almost equally believed to be important and unimportant in respondents' decision to walk.

The majority of respondents rated the importance of pedestrian improvements very highly.

27.54% of respondents have lived in Brevard/Transylvania County for less than 5 years.

16.77% of respondents have lived in Brevard/Transylvania County for 5-10 years.

19.76% of respondents have lived in Brevard/Transylvania County for 10-15 years.

10.78% of respondents have lived in Brevard/Transylvania County for 16-20 years.

One-quarter of respondents have lived in Brevard/Transylvania County for over 20 years.

36	1	0.60%		
38	1	0.60%		
39	1	0.60%		
40	3	1.80%		
42	1	0.60%		
43	1	0.60%		
44	1	0.60%		
50	3	1.80%		
60	2	1.20%		
65	2	1.20%		
82	1	0.60%		
Do you expect to be living in Brevard 10 years from now?	166	99.40%		
Y	138	83.13%	<i>Most of the respondents expect to be living in Brevard 10 years from now.</i>	
N	10	6.02%		
DK	18	10.84%		
Do you work outside your home?	164	98.20%		
Y	71	43.29%	<i>Most respondents do not work outside of the home.</i>	
N	93	56.71%		
If 'yes', how many miles is work from home?	72	101.41%	multiple places of employment	
<1	12	16.67%	<i>Most respondents work within 3 miles of their home.</i>	
1-<2	17	23.61%		
2-<3	10	13.89%		
3-<4	4	5.56%		
4-<5	3	4.17%	<i>63.89% of respondents work less than 5 miles from home.</i>	
5-<6	4	5.56%		
6-<7	2	2.78%		
7-<8	1	1.39%		
9-<10	1	1.39%		
	10	4	5.56%	
	11	2	2.78%	
	14	1	1.39%	<i>25.00% of respondents work 5-<20 miles from their home.</i>
	20	4	5.56%	<i>15.28% of respondents work 20 miles or more from their home.</i>
>20		7	9.72%	
	23	1	1.39%	
	25	1	1.39%	
	26	2	2.78%	
	40	1	1.39%	
Do you drive?	162	97.01%		
Y	151	93.21%	<i>Most respondents drive.</i>	
N	10	6.17%		
DK	1	0.62%		
Do you need a car for getting to and from work?	142	85.03%		
Y	59	41.55%	<i>Most respondents say they do not need a car to get to/from work.</i>	
N	81	57.04%		
DK	2	1.41%		
Do you need a car for on-the-job travel?	142	85.03%		
Y	50	35.21%	<i>Most respondents say they do not need a car for on-the-job travel.</i>	
N	92	64.79%		
Gender	170	101.80%		
M	68	40.00%	<i>Most respondents are female.</i>	
F	102	60.00%		
Children under 18?	159	95.21%		
Y	33	20.75%	<i>Most respondents do not live with children under 18.</i>	
N	126	79.25%		
Do your children walk to school?	33	100.00%	multiple children in different schools	
Brevard Elementary School				
Y	3	9.09%		
N	4	12.12%		
Brevard Middle School				
N	4	12.12%		
Brevard High School				
Y	3	9.09%		
N	4	12.12%		
Brevard Academy				
N	2	6.06%		
Pisgah Forest Elementary				
Y	1	3.03%		
N	1	3.03%		

Other				
Y		2	6.06%	<i>Presbyterian, Presbyterian Preschool</i>
N		3	9.09%	
None given				
Y		2	6.06%	
N		5	15.15%	
	<i>Total</i>			
	Y	11	33.33%	
	N	23	69.70%	<i>Most students do not walk to school.</i>
Would they walk?		23	104.55%	
Brevard Elementary School				
Y		3	13.04%	
N		1	4.35%	
Brevard Middle School				
Y		3	13.04%	
N		1	4.35%	
Brevard High School				
N		4	17.39%	
Brevard Academy				
Y		1	4.35%	
N		1	4.35%	
Pisgah Forest Elementary				
Y		1	4.35%	
Other				
N		3	13.04%	<i>Rosman Elementary & Middle, TC Henderson Elementary</i>
None given				
Y		3	13.04%	
N		2	8.70%	
	<i>Total</i>			
	Y	11	47.83%	<i>Most students would not walk to school even if facilities were available, but many would.</i>
	N	12	52.17%	
Age		164	98.20%	
25-34		7	4.27%	
35-44		18	10.98%	
45-54		31	18.90%	<i>34.15% of respondents were under the age of 55.</i>
55-64		37	22.56%	<i>Most (65.85% of) respondents were over the age of 55.</i>
65-74		33	20.12%	
75-84		30	18.29%	
85+		8	4.88%	
Where do you work?		77	108.45%	<i>multiple places of employment</i>
Outside city		21	27.27%	
Downtown		26	33.77%	
North		11	14.29%	
South		5	6.49%	
East		5	6.49%	
West		9	11.69%	
Where do you live?		166	99.40%	
Not in city		12	7.23%	
In city		154	92.77%	<i>Most respondents live within the city of Brevard.</i>

Comments regarding pedestrian facilities in Brevard

1. Thank you for your attention to this issue. We bike & walk daily. Anxiously awaiting completion of bike path. Meanwhile – I bike daily on the sidewalk along Hwy 64. This sidewalk is very old – uneven – and I feel unsafe in the areas where it butts up against the lane of traffic with no grass along the shoulder. Also at some stretches along the hwy – the sidewalk forces bikers almost out into the oncoming traffic – most notably right before the intersection at 64 and Chestnut St. There is a ditch right there and the sidewalk dumps you into the traffic lane. Very unsafe when riding a bike. Also please note that I recently requested that the sidewalks be edged (overgrown grass that was taking up many inches of the sidewalk) I e-mailed Don Parks and within 3 days the sidewalk along 64 was edged! Great service! I would love edging to be a regular maintenance task. Thanks!
2. Please continue with the bike/walk path. Would be GREAT if downtown was connected to Pisgah NF. It's a wonderful addition – GREAT for ALL ages.
3. I'm very much in support of continuing the bike trail. I use it quite often for running. I go into Pisgah often because of its beauty & proximity.
4. As someone who works in tourism the idea of a walkable town can only increase tourism and spending downtown.
5. I am delighted that the city is doing a needs assessment regarding pedestrian facilities – the main barrier for me (walking to town, to college path) is lack of sidewalks on Park Avenue – plus very narrow shoulders, speeding cars, etc. – a recipe for pedestrian injuries.
6. We moved here because we could walk to town!
7. I love walking in Brevard & since I am a committed walker, I often don't notice the absence of sidewalks, but I am sure it would encourage others if we made more... Yes, yes, do anything you can to get people out of their cars and off their couches & start discovering the Joy of Movement! I live in a perfect area for walks, others are less fortunate.
8. I love that a path is being made down 280. I love to bike and run/walk in the forest, but hate having to bike on the busy road with no shoulder to get there. ...The new path down the Ecusta Road is a good start [to improving conditions for walking/biking]. Please extend it over that busy intersection, past Pizza Hut, into the Forest!
9. I welcome efforts to make healthy activities like walking and biking safer and easier.
10. I appreciate Brevard College's welcoming of local residents to their campus, "cornfield", and track for walking.
11. For all the wonderful reasons I live in Brevard it surprises me that there has not been an intentional greenway system in place to create a pedestrian friendly environment. I run & cycle regularly & support your efforts to create a safe environment to promote healthy living.
12. My husband and I are seasonal residents of Brevard, but we hope to live there all year round in the next year or so. Walkability is one of the main reasons we chose to move to Brevard, so we already think it's great. However, any improvements would always be welcome. Cheers to your efforts!
13. We like walking on the bike path in good weather. Also, sometimes walk on the path at Brevard College. Prefer unpaved walking area for legs and feet. Usually feel safe anywhere walking but prefer walking partner.-----
14. Some dogs run loose [keeping me from doing more walking].

15. A leash law would be most helpful in our neighborhood – I have been followed multiple times by neighborhood dogs as well as losing much personal property from PET damage of other people in the neighborhood.
16. Animals run loose in the neighborhood [and keep me from doing more walking].
17. Dogs that people let out and off of a leash keep me from doing more walking.
18. While I do not need handicap access to sidewalks, I applaud their existence for my friends who do need them. A great need to enforce prohibition of blocking walkways to such traffic – e.g. Inn of Brevard “Cocktail Hour” advertising sign, garbage pick-up throwing cans/lids onto sidewalk after emptying, etc.
19. Enforce no U-turns.
20. Stop all “right turn on red” – drivers do not take enough caution prior to turning – cut off pedestrians & speeding up to get into traffic.
21. More public bathrooms, more sculptures, no double parking (clogs up traffic, which increases pedestrian danger), a color coded street sign that signifies the amount of pedestrian use.
22. If traffic would honor posted speed limits, it would help pedestrians at certain street crossings. Brevard has a lot of old fogies like me and horn-blowing is rude as we are doing the best we can.
23. Not even the police cars stop for pedestrians on crosswalks.
24. Safe crossing insured at [Blue Ridge Community College], CVS and elsewhere on N. and S. Broad Street.
25. The crosswalk laws need to be enforced – when a person is in the crosswalk the traffic in all lanes must stop until the person has crossed the street. I have almost been hit a few times crossing in the crosswalk.-----
26. Put a sidewalk in [along] Deerlake [Road] to Broad [Street] so folks don’t have to walk in the street.
27. Please – Please we need a sidewalk on Deerlake Road. It is very dangerous to walk in the street. Also at the corner of Highway 64 and Deer Lake a turn signal is needed. The Enmark station also adds turning traffic there.
28. We need a sidewalk extension along Deerlake Road. A new sidewalk from Deerlake Road to Strauss Park. Upgrade sidewalks from Deerlake Road to Downtown.
29. No pedestrian crossing light at intersection of Deerlake Road & Route 64. Plus no sidewalks on west side of Route 64.
30. Badly need a sidewalk from Enmark Gas Station and Sagebrush Steakhouse on Deerlake Road to connect to Deerlake Village Sidewalk system that starts at Entrance sign on Deerlake Road. Very bad curve in road with construction/older people driving.
31. The sidewalk on Deerlake Road needs to be completed between Cambridge to Cambridge Drive. An extension [of the sidewalk] to 64 would be great. Without the sidewalk in place it is a dangerous area to walk along the edge of the road. That area has a hill & curve, and traffic/autos do not adhere to the posted speed limit.
32. Would love to be able to walk to & from Deerlake Village to town, but highway is not a pleasant thoroughfare to use – proximity of parts of sidewalk to traffic, air pollution – non-existent sidewalks on one side of 64 – all are deterrents to such a walk. Consequently drive to Brevard College & walk from there.-----
33. Gallimore Road is between 2 schools, Brevard HS and Brevard Elem. There are a lot of HS kids walking on Gallimore. There are 2 large apartment complexes on Gallimore as well as

- 2 doctor's offices and the Boys & Girls Youth Center. I think a sidewalk is desperately needed for safe pedestrian walking.
34. We need a sidewalk on Gallimore Road – Being close to the high school and elementary school – now we have the Rise & Shine Program. There are a lot of walkers and joggers on that road.
 35. Putting sidewalks on Gallimore Road should be the city's next priority. It has 2 apartment complexes, a large school at both ends, the Boys' & Girls' Club, a doctor's and a dentist's office, and a high level of car traffic. Gallimore needs a sidewalk more than French Broad Street, which already has one, in my opinion.
 36. I feel the greatest need for a sidewalk in city is along Gallimore from 276 to Country Club.
 37. Elm Bend Drive is very narrow and dangerous because cars come speeding in from Wilson Rd. There are no sidewalks until you get to Sapphire Manor. Often there are potholes in the road. We need speed bumps along Elm Bend Road and sidewalks. Another very dangerous road is from the high school (BHS) to beyond the Health and Fitness Club. Road is very narrow and people speed around the turns & cross the center line. Also no sidewalks.-----
 38. Elm Bend Road needs sidewalks and better control of speeding traffic.
 39. Our section of Elm Bend has been promised sidewalks for the last several years. Many children walk to BES and it is not safe. With Morgan Heights & McCary Acres connecting to Elm Bend it would be very helpful to have sidewalks.-----
 40. The corner of Gaston and Maple has only 1 stop sign and it seems to be in the wrong place. Many accidents have nearly happened there and the situation will be worse when the new library opens. Many people have spoken about the “close calls” they have had.
 41. Maple Street sidewalks need some attention and I would like to see them extended all the way down the street. They are cracked, uneven, etc. – looks like it has been awhile since any maintenance has been performed. The neighborhood is safe, but the sidewalks are in disrepair.
 42. Crosswalk and pedestrian signs at crossing on Maple.
 43. Crosswalk and 3-way stop at Maple and Gaston at new library!-----
 44. I tried to walk from Lutheran Church to Hardware Store on Main. Crossing at Ingles/Kmart entrance had a light but so many turning cars required great pedestrian vigilance. I couldn't cross Broad Street at Food Lion shopping center (no light – light is at place where there is no sidewalk on Caldwell Street). – My primary experience was that turning cars do not expect, watch for, or have patience for pedestrians.
 45. A sidewalk that needs improvement is Broad Street from the Ingles to Osborne Road. The Sidewalk is broken up with driveways, narrow, too close to road, and vanishes occasionally. Having a sidewalk jump from one side of the street to the other means one has to cross street/traffic. Gallimore Street definitely NEEDS a sidewalk. I think painted white crosswalks at most intersections would be a big improvement for pedestrian safety.
 46. North Broad Street from Ingles to Osborne Road – It should be 5-feet wide with at least a 2-foot utility strip. It should have crosswalks and handicap ramps. The sidewalk is broken and uneven. Also when the sidewalk switches sides of the street that means one has to cross from one side to the other through traffic. Every sidewalk should have a utility strip.
 47. The sidewalks coming toward town on 276S to Main from light need repairs and cleaning.
 48. Main Street close it – become pedestrian mall
 49. Brevard Main St. is a beautiful area for walking. Several side streets' sidewalks are in poor condition – i.e. Whitmire, French Broad, and King St. – these are streets I frequently walk on. I love the hike/bike trail behind the hospital and also the track at the college.

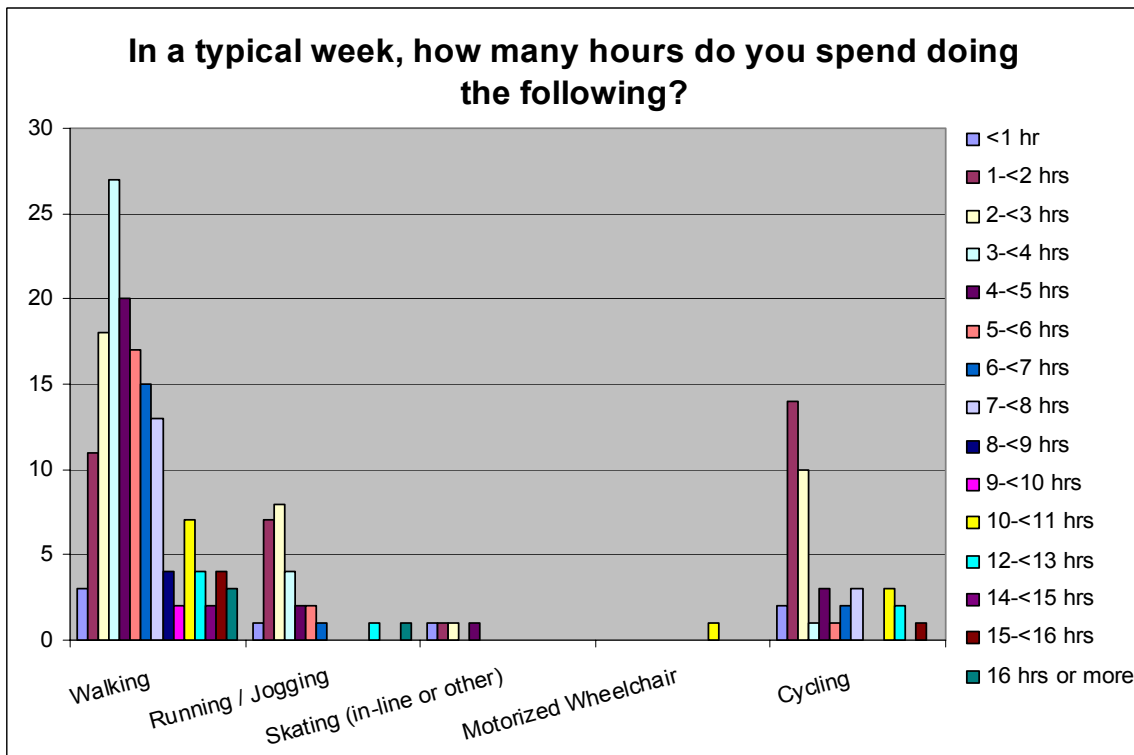
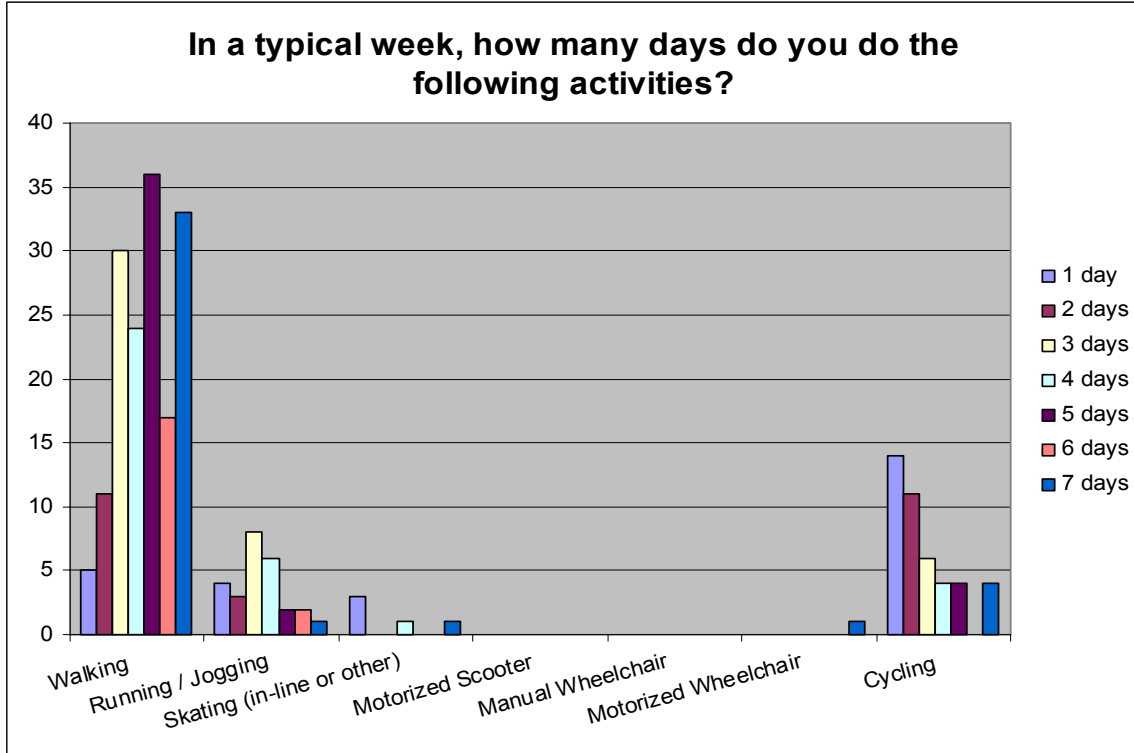
50. There is no safe way to walk along 64-W from Country Club Road west toward Rosman.
51. [Sidewalks should be added to] Highway 64 West from red light @ intersection of Country Club and 64 West to City Limit Sign on both sides of Highway 64 West.
52. Sidewalks along Hwy 64 broken in places / too close to traffic.
53. The sidewalk to Brevard Elementary along 276 from Franklin St. to school is so close to fast traffic in some areas. Frighteningly close (about 1'). The sidewalk down Broad Street toward the bike path has so many places for cars to pull in its hard to get to the bike path from Downtown. Slower side street traffic would improve foot travel safety on sidewalks that already exist.
54. Sidewalk repairs; sidewalks on all residential streets. Educate drivers who block pedestrian crosswalk while waiting for light to change. Improve pedestrian crossings, especially at busy 4-way intersections, i.e. downtown. Downtown, program traffic lights to stop vehicle traffic from all directions – “walk” signal in all directions for pedestrian traffic. No right turn for cars at these intersections. Strict enforcement of speed limits in town; install speed bumps E. Main between traffic lights @ Johnson & Parkview, also Broad Street between Main & French Broad. Enforce “No U-Turn” ordinance; enforce pet leash ordinance. [Suggested] ordinance – no use of cell phones while driving in town.
55. Main Street to the Hospital (at least) should have sidewalks from downtown. The current sidewalk is too close to road – unsafe.
56. [Highway] 64 across from AutoZone, near NCDL Office, trees need to be trimmed so that limbs are not blocking sidewalk.
57. I would like to see sidewalks on Broad Street where there are none.
58. Sidewalk along North Broad gets drowned by traffic after any rain – poor drainage.
59. The pedestrian facilities in my particular area are generally good, especially for a retired person who does not have to worry about getting to work. The main problem for me is crossing the highway (64/276) getting to the bike path at the entrance [of Straus Park] without the traffic signal. I also ride a bike and, except for the new bike path, the conditions for bike riding in and around Brevard are quite hazardous.
60. Sidewalk – East Main Street from 276 to turn at end.
61. Many sidewalks are greatly in need of replacement or repair. Better control of traffic speed on Miner Street.
62. The short gap in the sidewalk on Asheville Hwy. at old RR crossing near Chestnut Sq. should be filled. A safe signalized crosswalk and Broad and Caldwell is urgently needed. The discontinuity in the sidewalk along Caldwell between French Broad and Whitmire should be addressed. A sidewalk condition survey should be made.
63. To me, the single greatest improvement in Pedestrian Friendly conditions would be the provision of pedestrian cycles (or demand control) at a number of crossings of Broad and Main Streets, e.g. N. Broad and McLean, French Broad, Main, and Rosman Highway. Possibility of putting the latter nearer the new library.-----
64. Brevard is a dangerous place for pedestrians. I would hope Neely Road and Park would be considered for a sidewalk. I gladly pay my share. Sidewalks should connect the whole town. It’s crazy to spend a quart of gas to buy a quart of milk.
65. Park Avenue needs slower traffic and sidewalks.
66. I am so glad to have an opportunity for input! Living on Parkview Drive, I am very aware of the excessive speed and number of vehicles that come by. Watching the Brevard Elementary children walk by on the sidewalk (which is very close to the road), I fear that

- one will get hit by a car. I feel it is important to: (1) reduce the speed and/or number of vehicles, (2) build up the curb higher than the sidewalk level, and (3) extend the Parkview sidewalk to the Brevard College track (bridge dividing Park Ave. & Neely Rd.). This would prevent people from walking on the road and tie the neighborhood to the college.
67. I would like to be able to walk or ride a bike from Park Avenue to Brevard Middle School, the fitness trail in front of Blue Ridge Community College, and the Rec. Dept., but there is no way to get there without having to go on the road.
 68. Park Avenue sidewalk needs to be extended from French Broad to at least Old Hendersonville Road.
 69. Slow down traffic on Park Avenue & Parkview Drive. These streets have become the downtown bypass unfortunately. They are residential streets & NOT designed for the speed, volume, & type of vehicle traffic. They also lead to Elementary school so have high volume of kids traveling. Sidewalks are not in great shape, no crossing assistance except crossing guard during school, & the sidewalk is way too close to road (at some points no median between road & walkway).-----
 70. Probart Street needs repairs, safety upgrades, sidewalks, paved, etc.
 71. I work at Brevard College and walked to work until the Brevard Academy opened. The speed & volume of traffic caused me to have several close calls because there are no sidewalks up Probart. I have often wondered why the city did not extend sidewalks to the music center. We don't go to shows because we cannot get out of our driveway. BUT we would go to performances if we could walk safely. Folks could have dinner in town & walk to BMC.-----
 72. South Caldwell Street from 602 South Caldwell Street (Attorneys White & Dalton) to the intersection of Highway 64 & South Caldwell Street.
 73. Sidewalk repairs along South Caldwell Street, crosswalk/sign at Caldwell and Silverstein, sidewalk continuation all the way to Rosman Highway on Caldwell.
 74. Caldwell needs a sidewalk from the College to Downtown.-----
 75. Sidewalk from Brevard Elementary School (Greenville Hwy) up past Silvermont is too narrow & in disrepair... students walking home & also many "walking field trips" taken along that route.
 76. When my children were of school age I drove them to Brevard Elementary because there were NO sidewalks and today there are still NO sidewalks.
 77. Traffic is moving so fast as it takes the 276 [Greenville Highway]/Main Street curve by Silvermont that cars often leave their lanes, hit poles and come up close to the sidewalk. To cross over at Silvermont on the crosswalk is risky because of speeding traffic that does not observe crosswalk sign – or pedestrians. If not crossing over the highway at that point a pedestrian is faced with a telephone pole & uneven grating & concrete at Park Avenue & 276 [Greenville Highway].
 78. There are no sidewalks near Brevard Elem., so I drive to church and walk from there sometimes when the weather is nice. Would like to be able to do more often (walk from home). -----
 79. Sidewalks near Silvermont and towards elementary school.
 80. The sidewalks in front of Silvermont need regular cleaning.-----
 81. There is a need for safety upgrades on the sidewalks on Whitmire Street. They are old, uneven and have some places that could be dangerous to the elderly such as tripping. Some are cracked up with patches of grass growing up between. This section is in need of

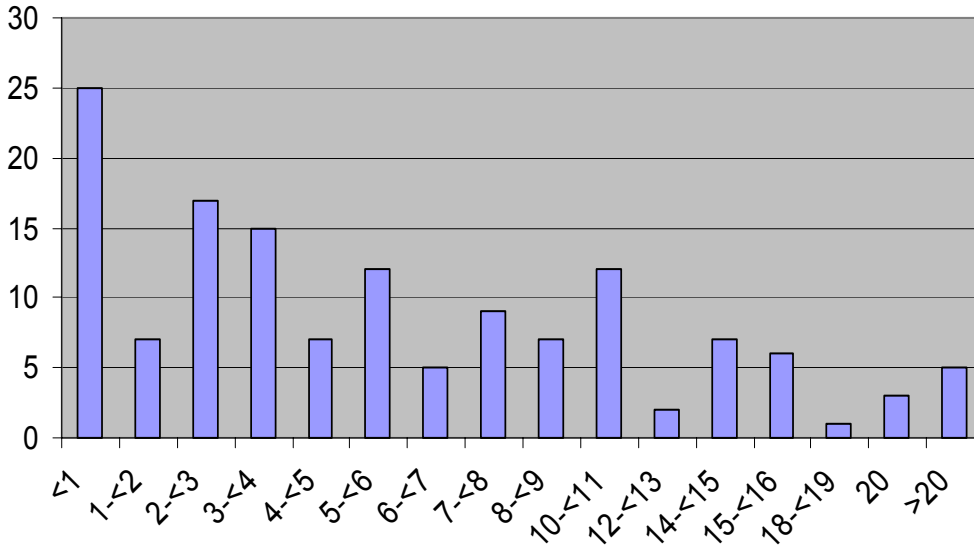
- improvement. I think of how wonderful it would be to make a historical replica of the Old Depot that once energized Brevard.
82. It would be nice to have a sidewalk on Railroad Avenue between Whitmire and Tinsley Road.-----
 83. Oaklawn is a cut-thru speedway [pedestrian safety compromised].
 84. Morris Road has NO improvements ever.
 85. Sidewalks are in need of repair in several places. Behind the jail is a good example. My husband fell near Cardinal Drive-In. I know others who have fallen. The sidewalk coming from Brevard High is too close to traffic and traffic goes too fast there. Sometimes trees hang over the sidewalk.
 86. A walking route from College Walk to the Ingles/Kmart shopping area.
 87. There are some “breaks” (no sidewalk) in the area between Osborne Road and Ingles. If these were added then downtown would be connected to the new bike/walk trail.
 88. Batson Road needs sidewalks. Traffic around blind curve too fast, dangerous.
 89. Batson Road and Miner Street could use sidewalks.
 90. There ... are some areas along Franklin Street on the north side of the street where limbs and branches are low over the sidewalk. Thank you for providing the opportunity to those of us that walk to let you know our concerns and opinion of pedestrian facilities.
 91. I would appreciate it if the pavement could be repaired so it is level on French Broad Street and other well-used streets.
 92. There are no sidewalks past Brevard High School.
 93. My child would not walk to school even if sidewalks or greenways were available until High School.
 94. My daughter and I love the bike path and walk it almost weekly with our dog. We would love to see a sidewalk extended to the Racquet Club as we live next door and a lot of children live in this area.
 95. Children walk to school, but ONLY with an adult, NOT alone due to traffic.
 96. Child walks to school (Presbyterian) with parent assistance.
 97. If we had public transportation, I would walk to/from it.
 98. I would like to see more public transportation in Brevard to reduce reliance on individual car use. Decreasing traffic should figure prominently in any pedestrian plan because it improves air quality.
 99. Continue the bikepath/greenway (offroad) to Downtown and the National Forest. Also to Brevard College at Kings Creek.
 100. Some of the city walks are impeded by the grass growing on both sides. Edging them would make the sidewalks wider and more usable. I enjoy the pedestrian facilities very much and appreciate the attention you give them! A high priority for why we moved here in Dec. 2004.
 101. Extension of pedestrian/bike sidewalk from Brevard towards Pisgah Forest (past Texaco Station)
 102. Not enough litter containers along side streets.
 103. Some sidewalks are not kept clear of over-hanging, extended growth of bushes, shrubs, weeds, etc.
 104. Need buttons to press for pedestrians to cross.
 105. Enough sidewalks, but many need to be redone. (repair, widening)

106. I am very concerned about the “Yield to Pedestrians” sign across Kelly’s Rest. It is never honored, and if it were there would be some serious accidents at that point. I suggest remove the sign and replace it with “Cross with Care”.
107. Occasional benches to rest would help on mini rest stops.
108. We have several blind folks in town, so audio crossing signals are important.
109. Parts of town have an adequate number of sidewalks, and parts of town have sidewalks that are in good condition. Others do not.
110. I would like to cycle, but it’s unsafe on Brevard Streets.
111. Traffic too fast, no safe crossings, too busy.
112. It is important that push buttons at crosswalks are working.
113. The sidewalk in front of our home is in terrible condition! I think we should have it fixed before someone gets hurt. [Did not indicate where in the city respondent resides]
114. Need more marked pedestrian crosswalks.
115. Less noise from trucks and less black smoke help for cleaner air. There is no reason for all the exhaust noise from trucks.
116. Some sidewalks in Brevard are in good condition, others are not!
117. Many sidewalks too close to road.
118. Would like to cycle, but it is too dangerous. We don’t even allow our child to ride in town.
119. Fast traffic is a problem, especially on streets NOT designed for the volume of traffic using them.
120. SLOW DOWN TRAFFIC
121. Pressure-wash all river rock walls in city (use Clorox).
122. Repair sidewalk in front of Hobnob, Presbyterian Church.
123. My neighborhood has safe and attractive areas for walking, but no sidewalks.
124. I would say that one of our largest concerns [as pedestrians] is the speed of traffic throughout Brevard, not just on Maple Street.
125. Convert alley between Essence of Thyme & Gallery on Main to one-way south (or vacate): somebody is going to get killed.
126. Replace benches on E. Main.
127. Adequate lighting is very important.
128. Some lighting needs to be added to the new bike/walk trail. Parts of it are lighted by existing street lights and “night lights” at the apartments but there are some areas that these lights don’t reach.
129. Although my children are grown I do have 1 granddaughter and would like to see the community expand its pedestrian & bikeways to allow access to activity centers for youth and make it safe for them to get there by foot or bike. [Respondent resides southeast of downtown]
130. There are fewer and fewer nearby shops/interesting places to go.
131. Shady streets and sidewalks are very important.
132. Keep planting shade trees.
133. Cant’s have too many trees, flowers, vegetation.
134. Traffic signs telling drivers to slow down or watch for pedestrians are ignored.
135. Traffic signs telling drivers to slow down or watch for pedestrians, especially children, are very important.
136. Repair broken sidewalks
137. Making right turns on redlight – the cars seem to have the right-of-way [over pedestrians].

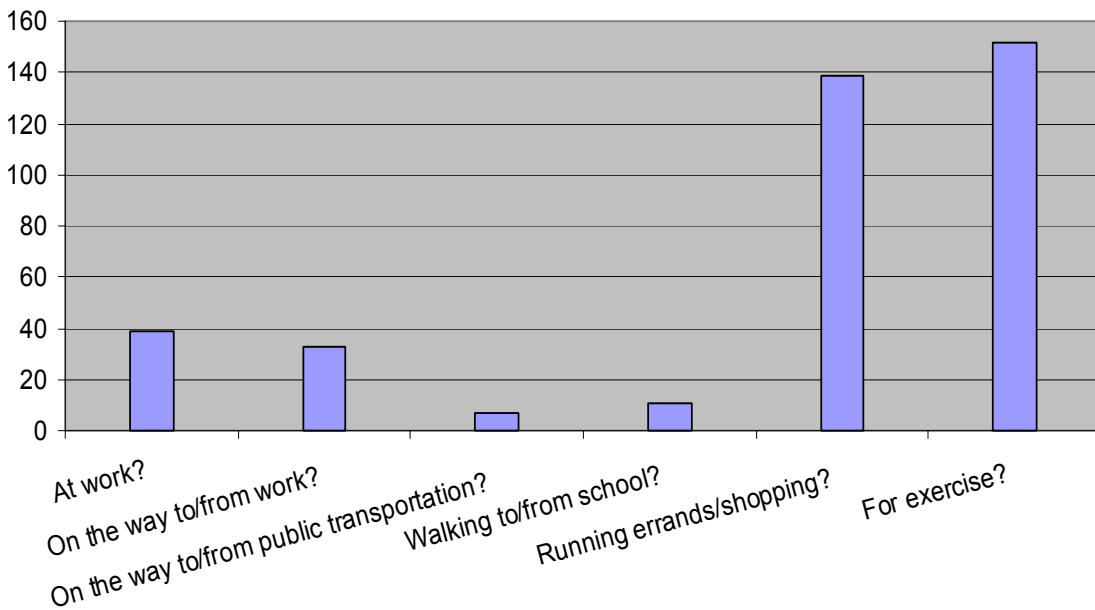
138. Brevard does not have enough sidewalks in the various neighborhoods for safe walking. Could also have more street lights in the neighborhoods.
139. Convenient access to trails is important – they don't have to be paved.
140. Availability of benches is unimportant to me now, but that may be different as I get older.
141. More sidewalks
142. Convenience store in town without going up & down hills
143. People opening doors into oncoming [pedestrian] traffic
144. Most sidewalks need upgrading. Need path to & from city.
145. There are no sidewalks in my neighborhood, but I walk in the street.
146. Brevard has an adequate – not ideal – number of sidewalks.
147. I am... very appreciative of sidewalks and the intersections that have the signals for walkers to cross. I feel completely safe walking in our town regardless of the time of day or night. However, ... there is a stretch of sidewalk behind the jail on Gaston Street with many cracked sections, one so much lower than the rest that during a rainstorm it is filled with water. The roots of a tree have lifted another section to a point that I feel is unsafe. My main pleasure in walking is aesthetics and I must say that the folks who planted the beautiful flower garden by the sheriff's department have given me much pleasure. In sharp contrast, the Board of Elections' grounds leave much to be desired. ... The grass grows out onto the sidewalk, weeds grow in the gutters and planters, and there is always debris of some sort on the sidewalk in front of the building (even before the steps were removed). ...When the city or county groundsmen mow they just leave the grass on the sidewalk for the rain or wind to disperse. Wet grass or leaves on a sidewalk can become dangerous for walkers. Another issue I would like to address is the recent placement of trash receptacles on Main Street. The people eating outside at Falls Landing get to look at one while enjoying their meal and the one that used to be in front of Essence of Thyme that was used constantly is gone and now farther down the block [to a location where it is rarely used]. All in all I feel the city is doing a great job when it comes to pedestrian facilities. Those wonderful clean restrooms in the new city hall are the answer to many a pedestrian prayer. Thank you for that addition!
148. Many existing sidewalks have low overhanging trees or shrubs blocking the sidewalks making it difficult to walk or forcing you onto the roadway.
149. Walking paths are great – but there is no easy access to them from our neighborhood – we have to drive to either BRCC or the activity field.
150. I feel that pedestrian facilities are fine, no improvement. It is unsafe to ride a bike anywhere, because there are no shoulders. Bike path was a big waste of money, as are Straus Park improvements.

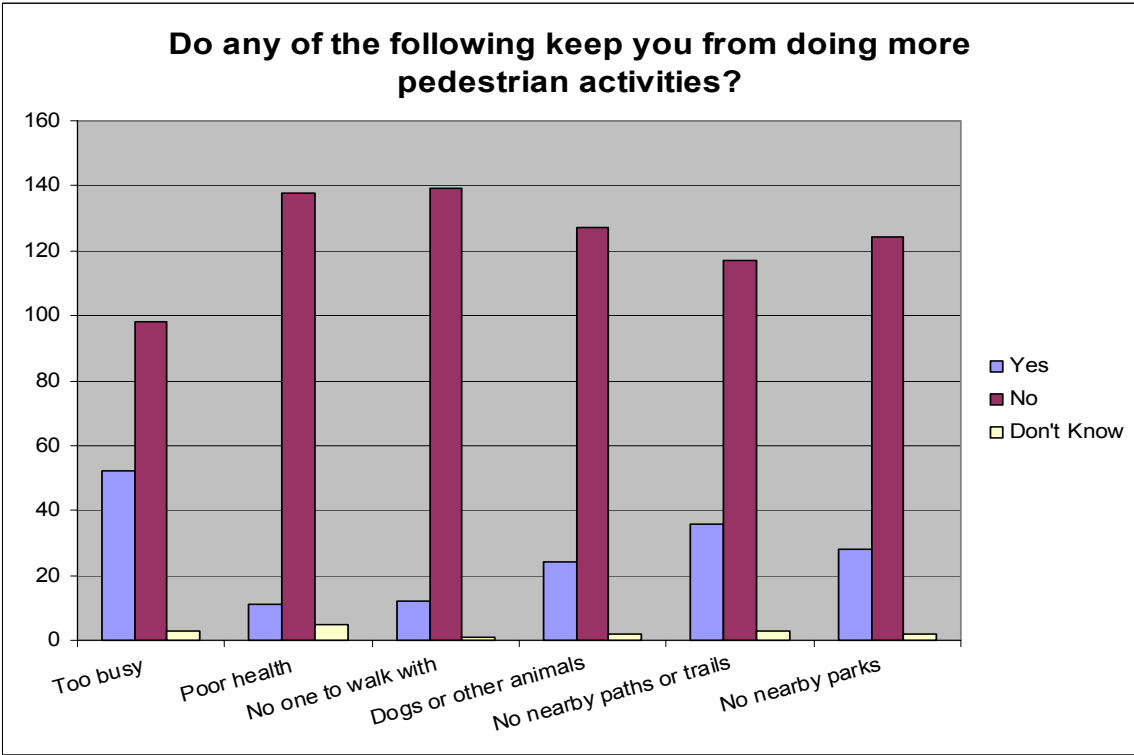
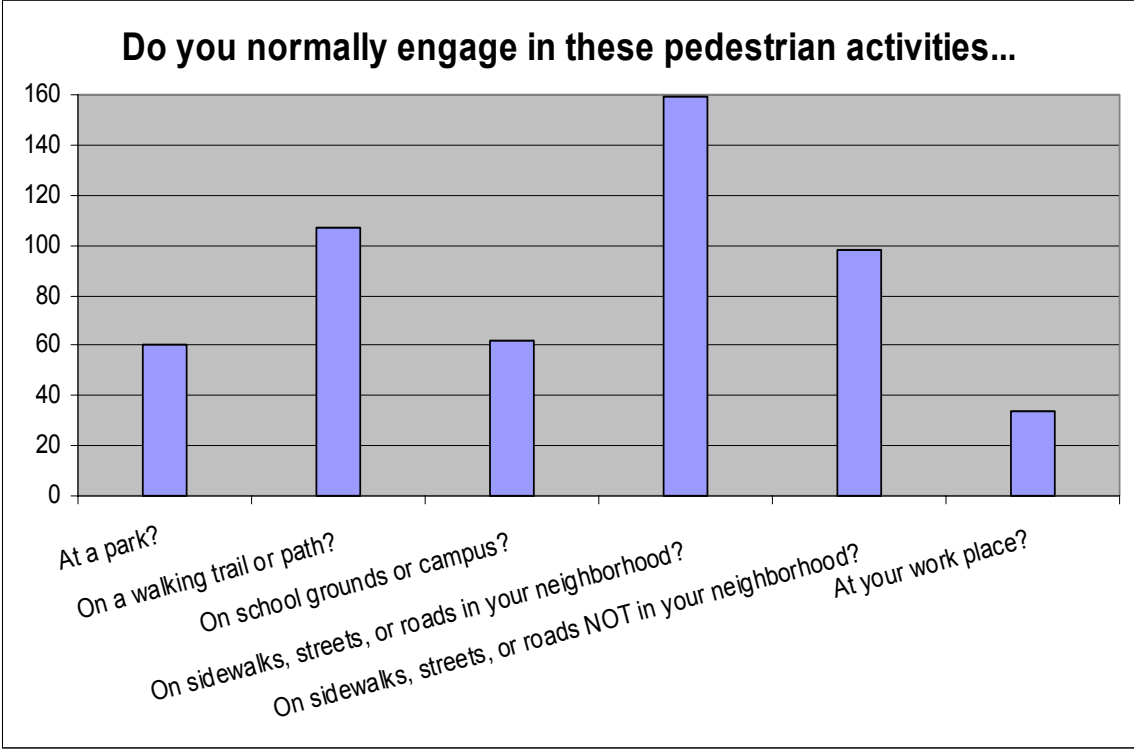


How often do you walk 10 minutes or more in getting to or from a destination in a typical week?

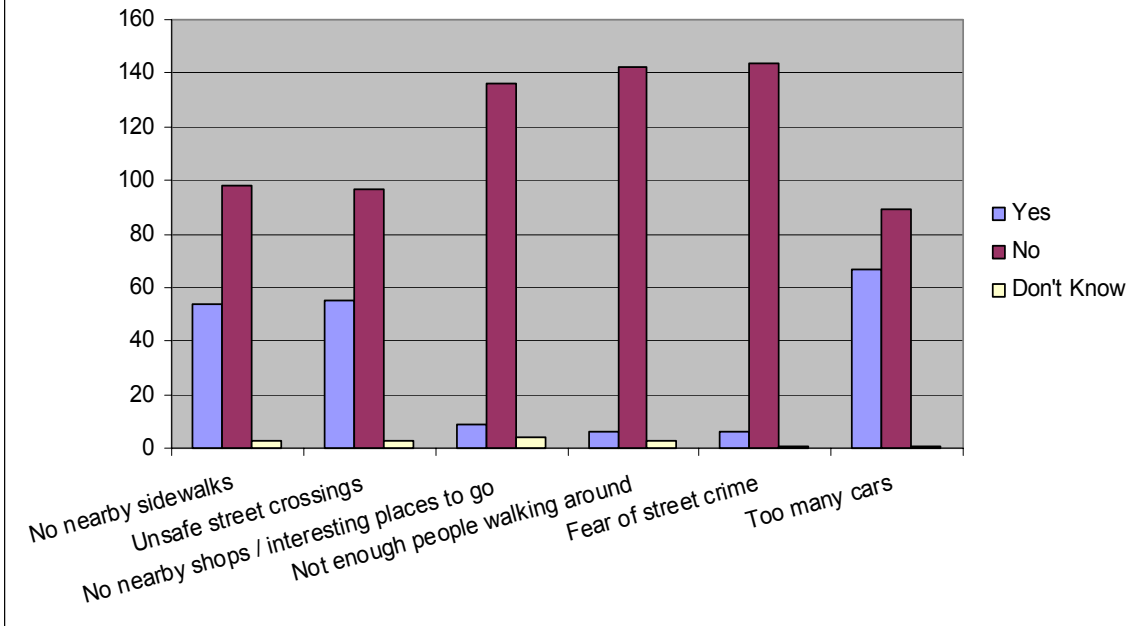


Do you engage in pedestrian activities...

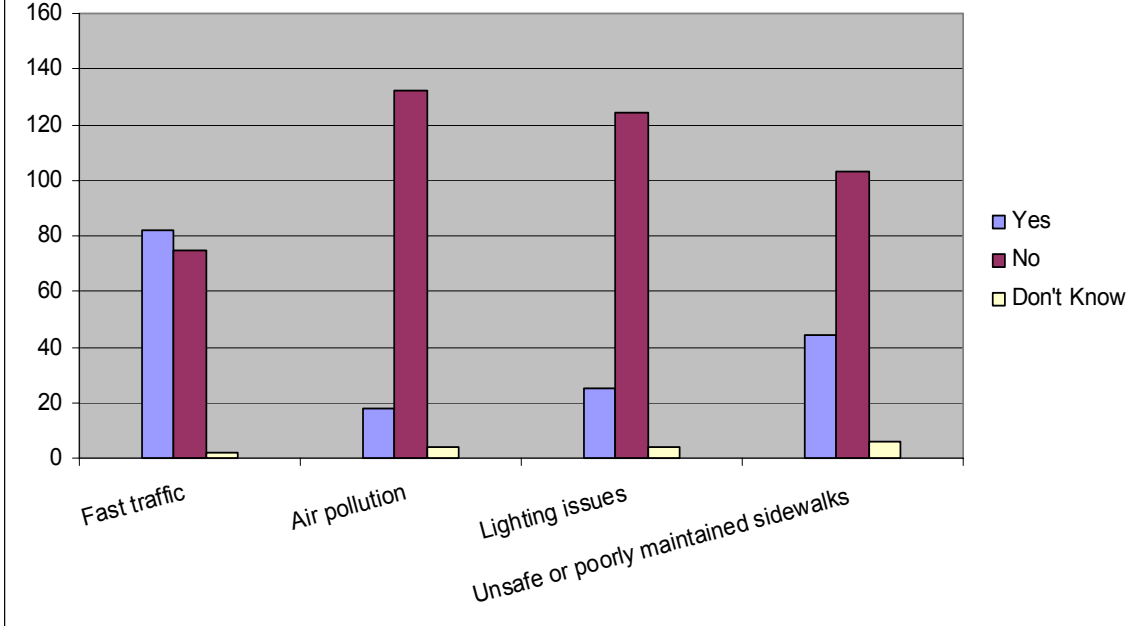




Do any of the following keep you from doing more pedestrian activities? (cont.)



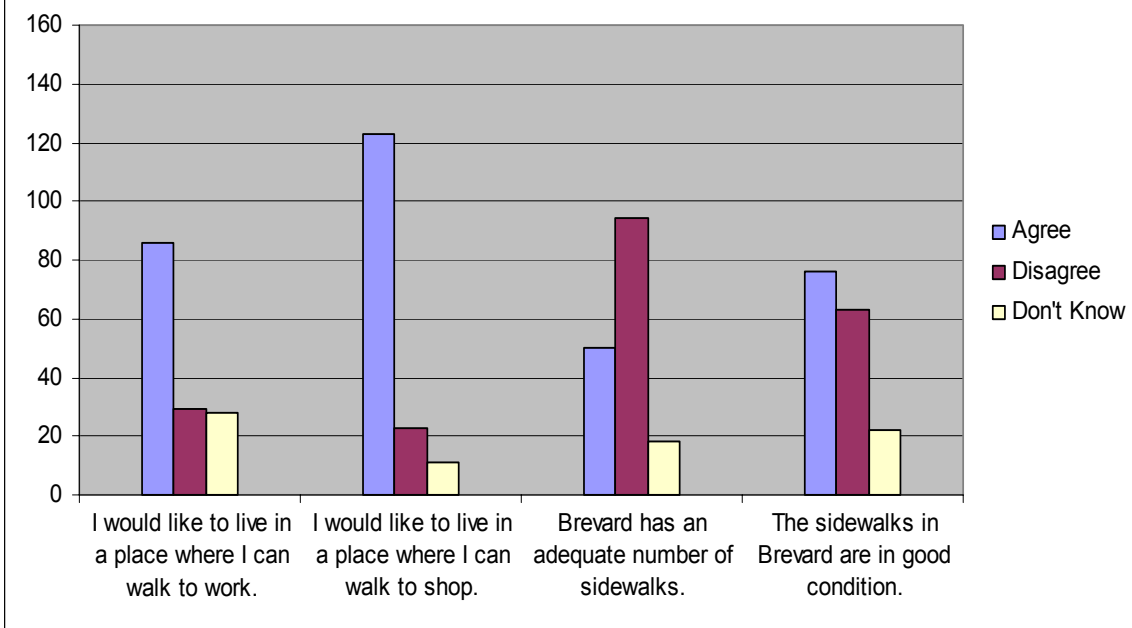
Do any of the following keep you from doing more pedestrian activities? (cont.)



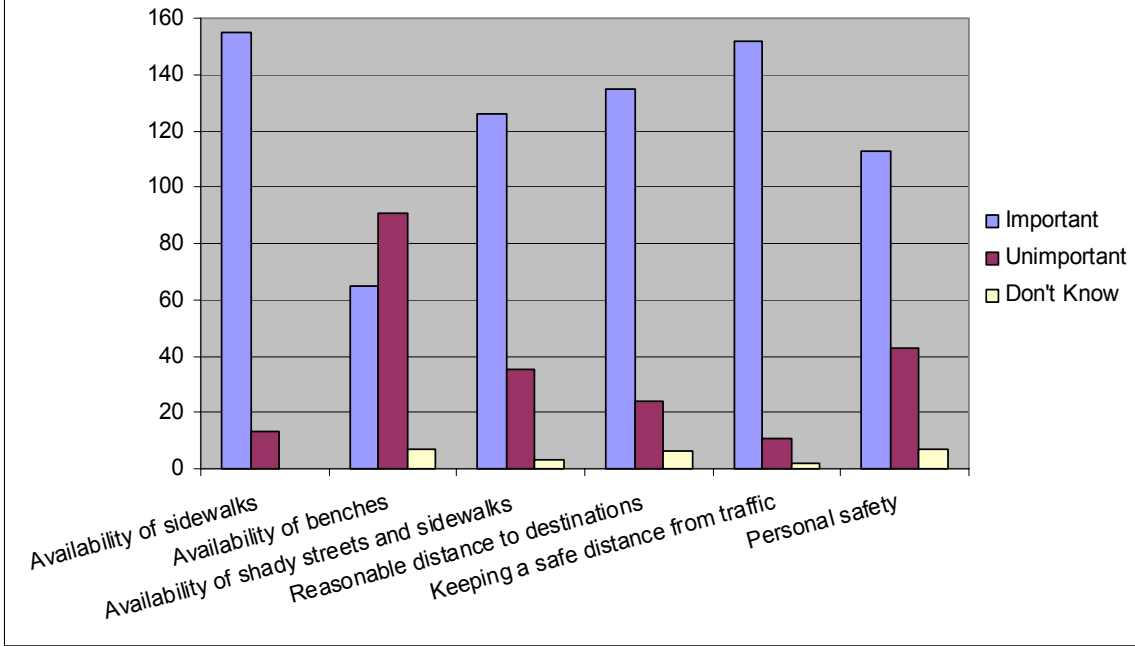
Do you agree or disagree with the following statements as they relate to Brevard?



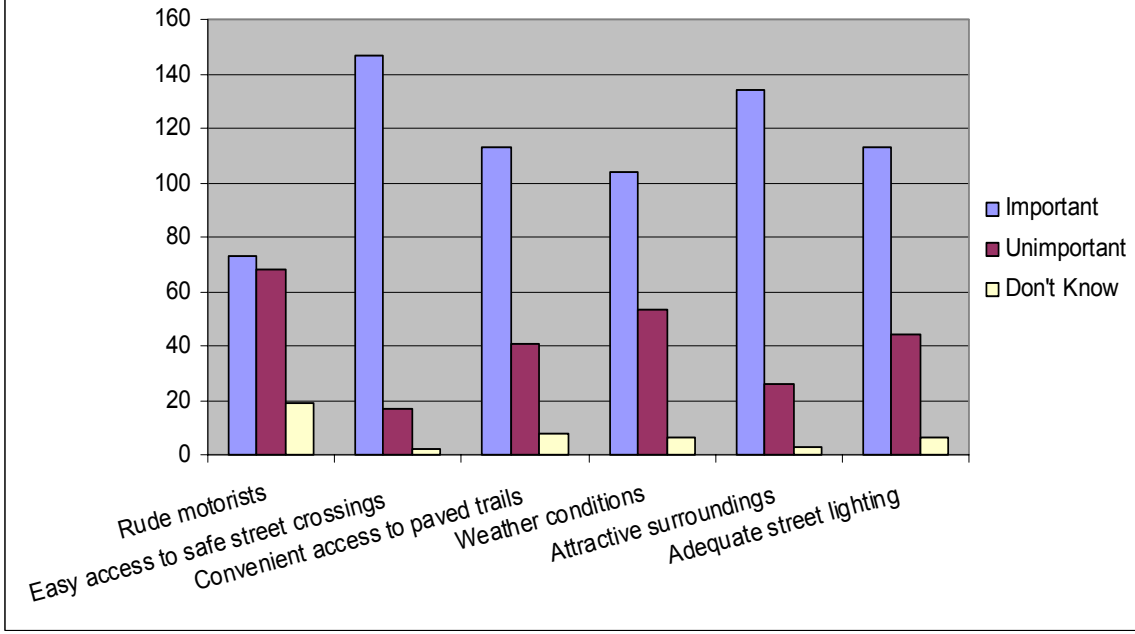
Do you agree or disagree with the following statements as they relate to Brevard? (cont.)

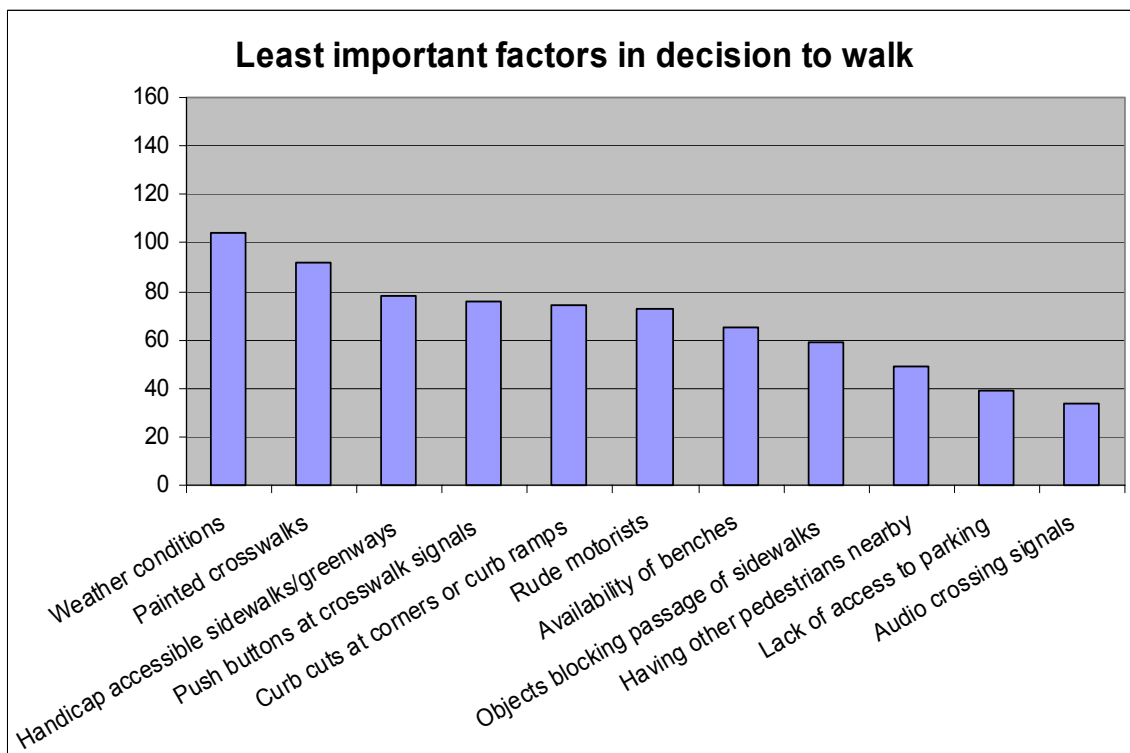
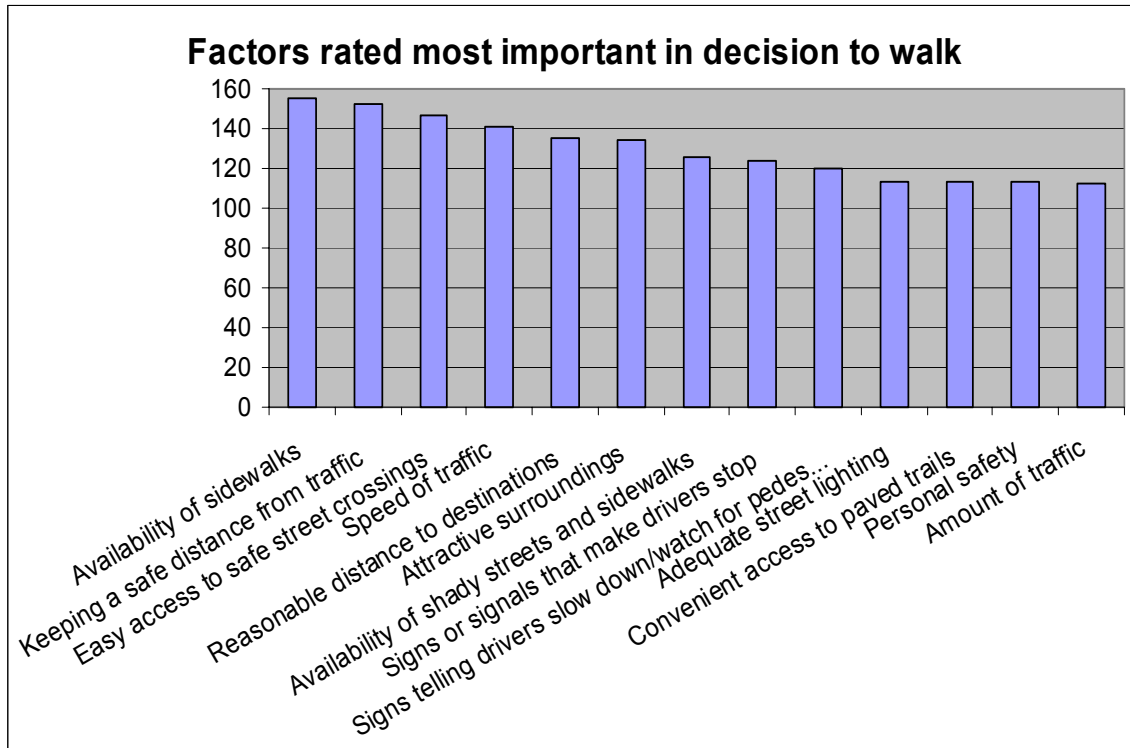


How important are the following factors in your decision to walk?

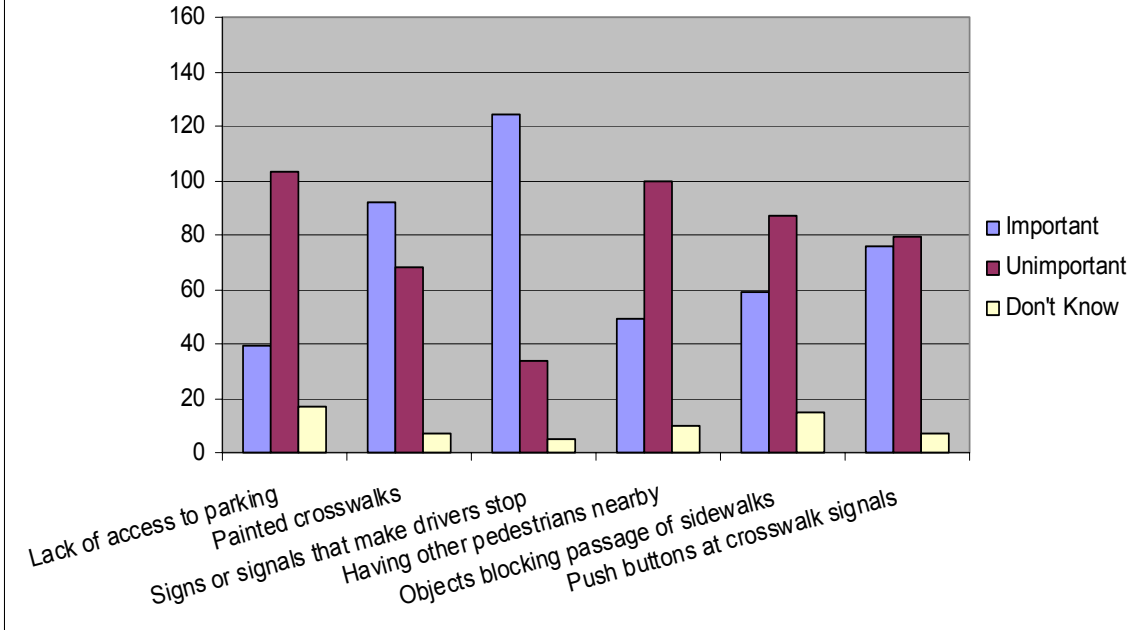


How important are the following factors in your decision to walk? (cont.)

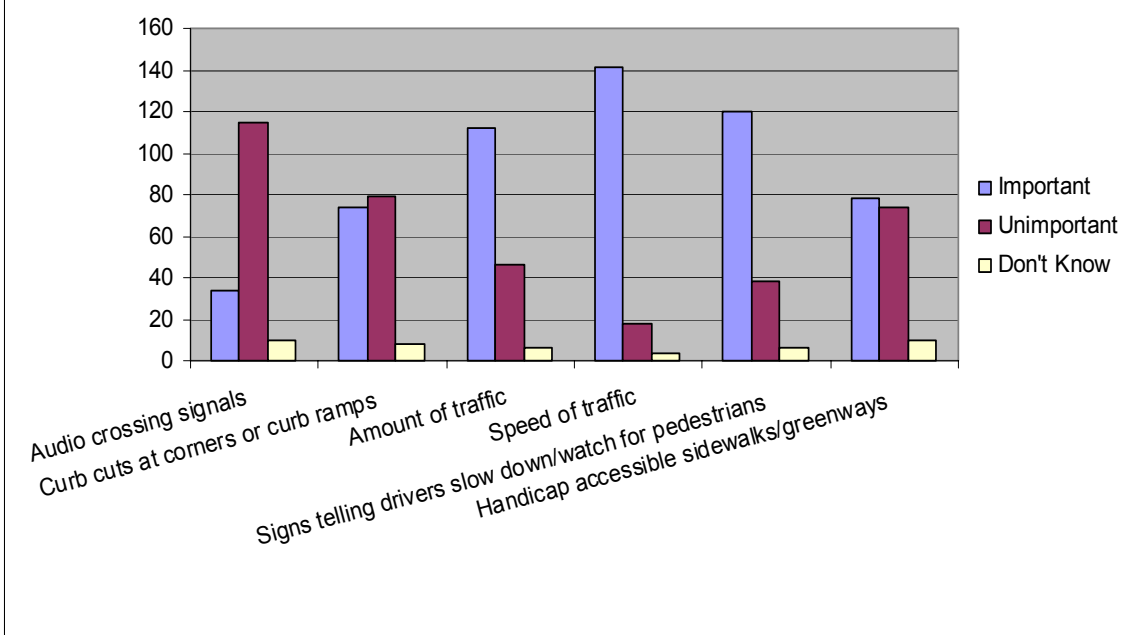




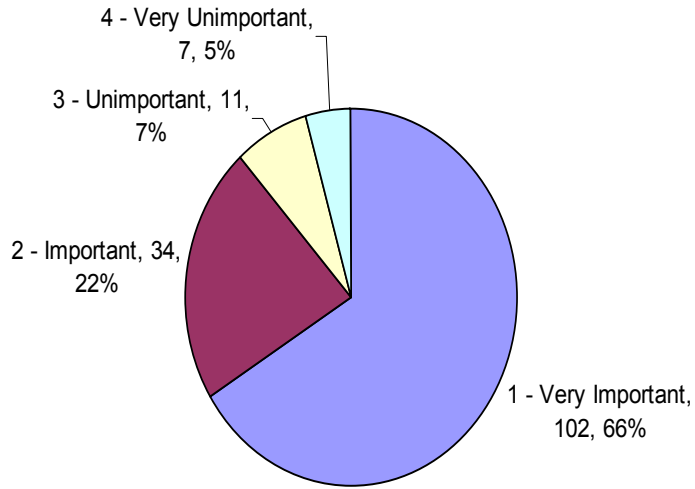
How important are the following factors in your decision to walk? (cont.)



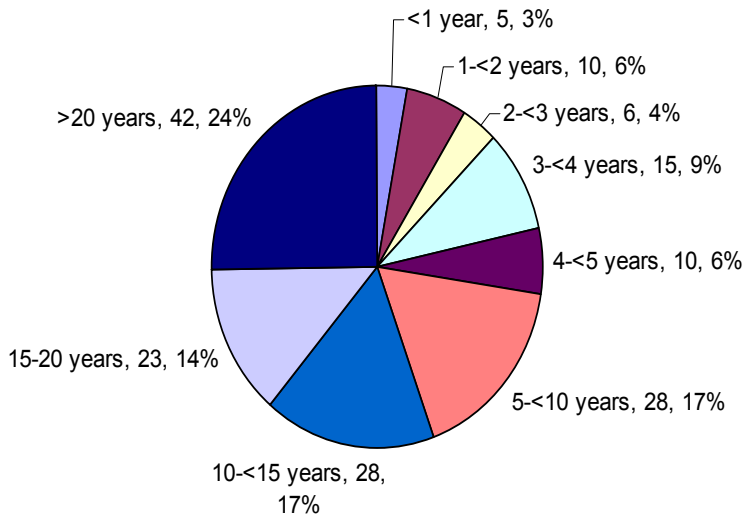
How important are the following factors in your decision to walk? (cont.)



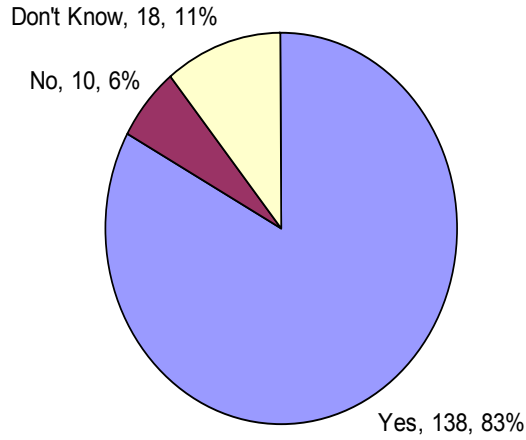
How important are pedestrian improvements to you on a scale of 1-4?



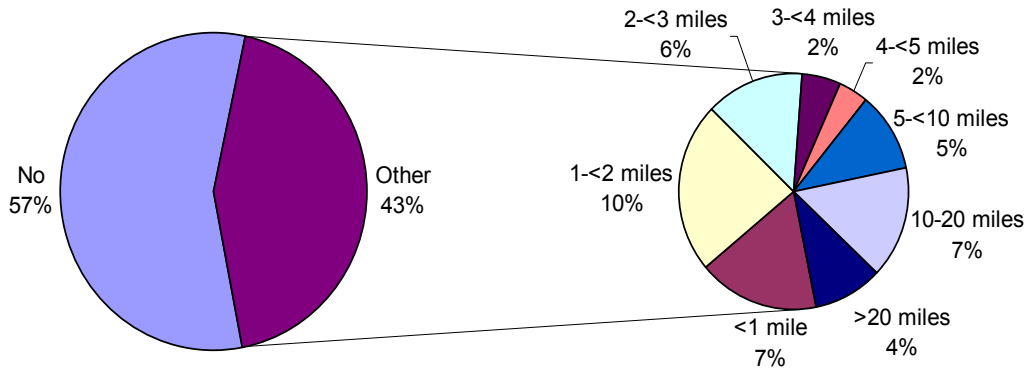
How long have you lived in Brevard?

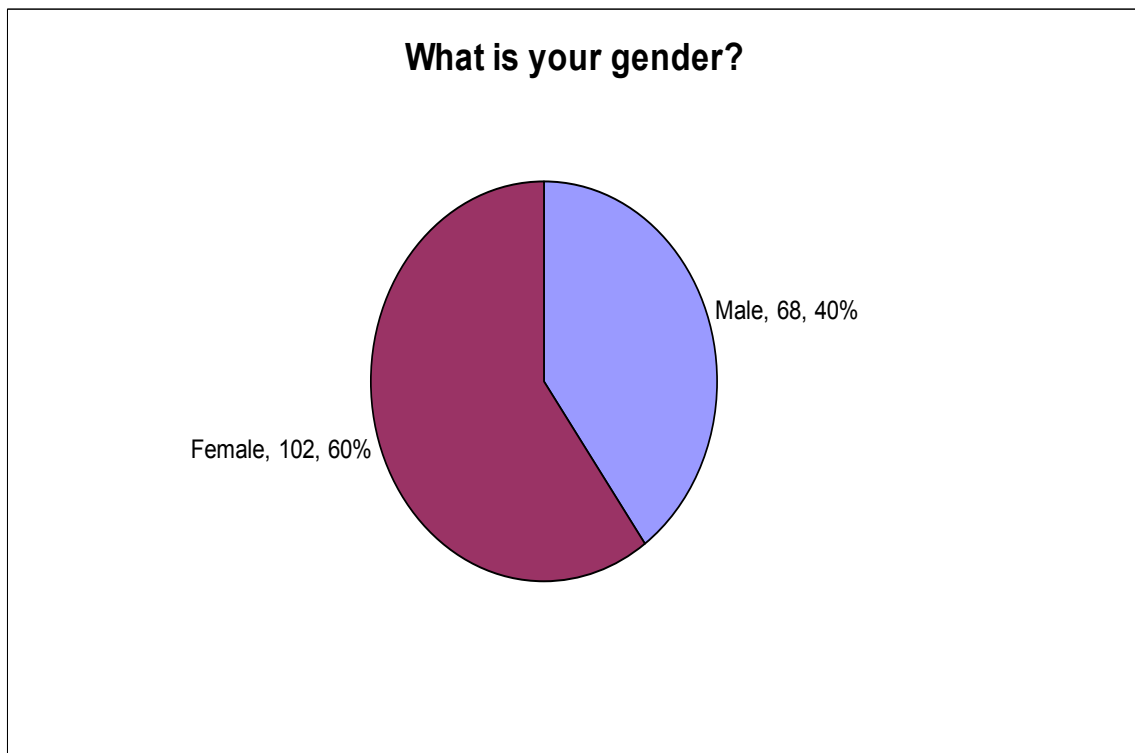
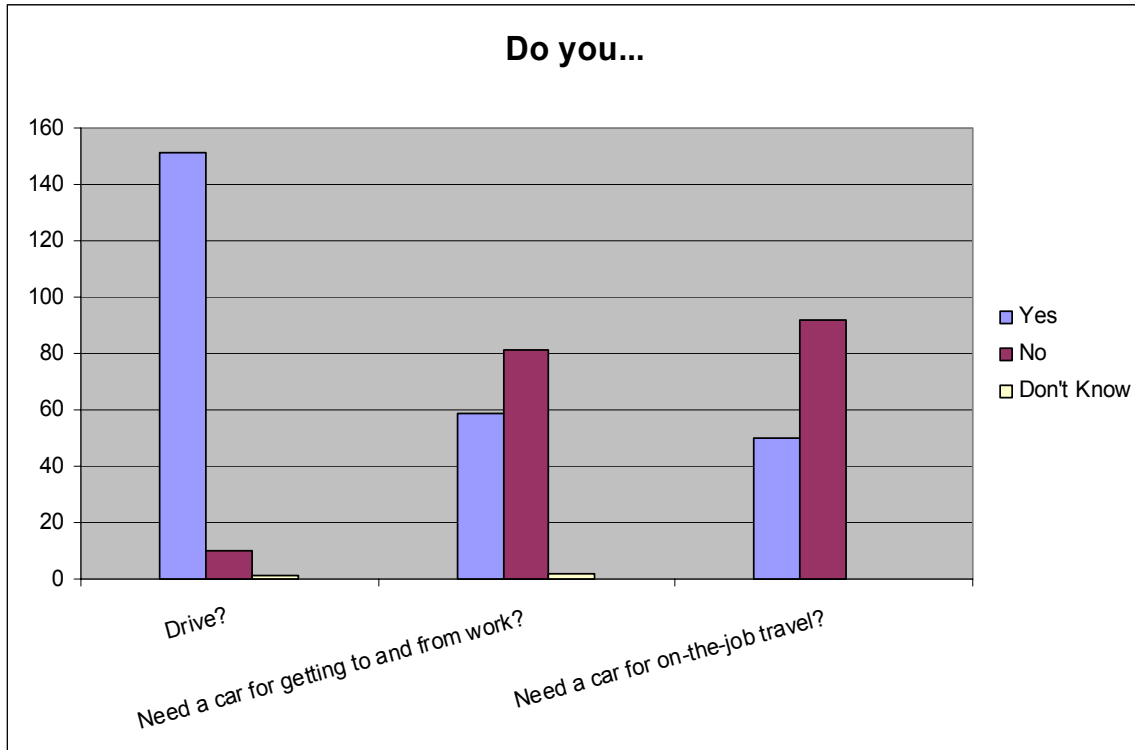


Do you expect to be living in Brevard 10 years from now?

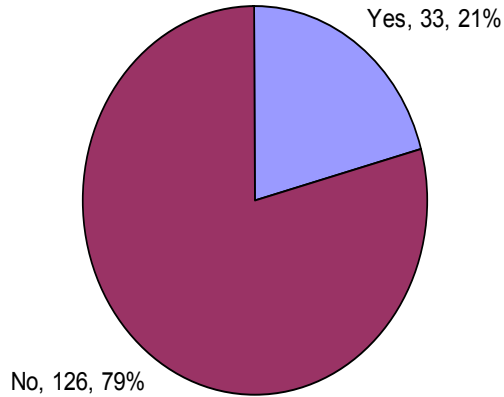


Do you work outside your home? If so, how many miles is your work from home?

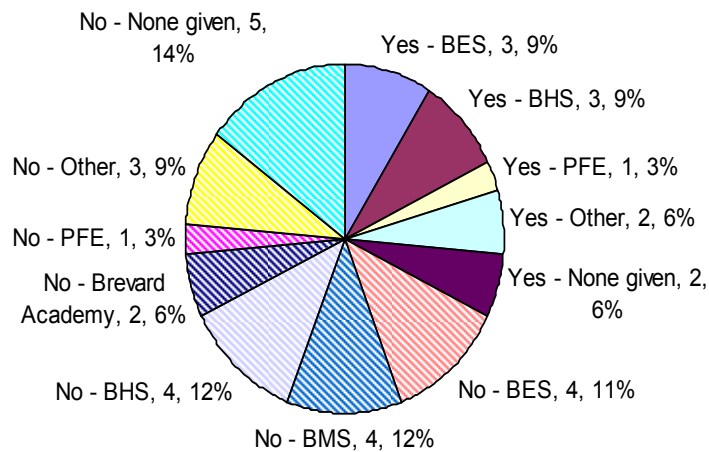




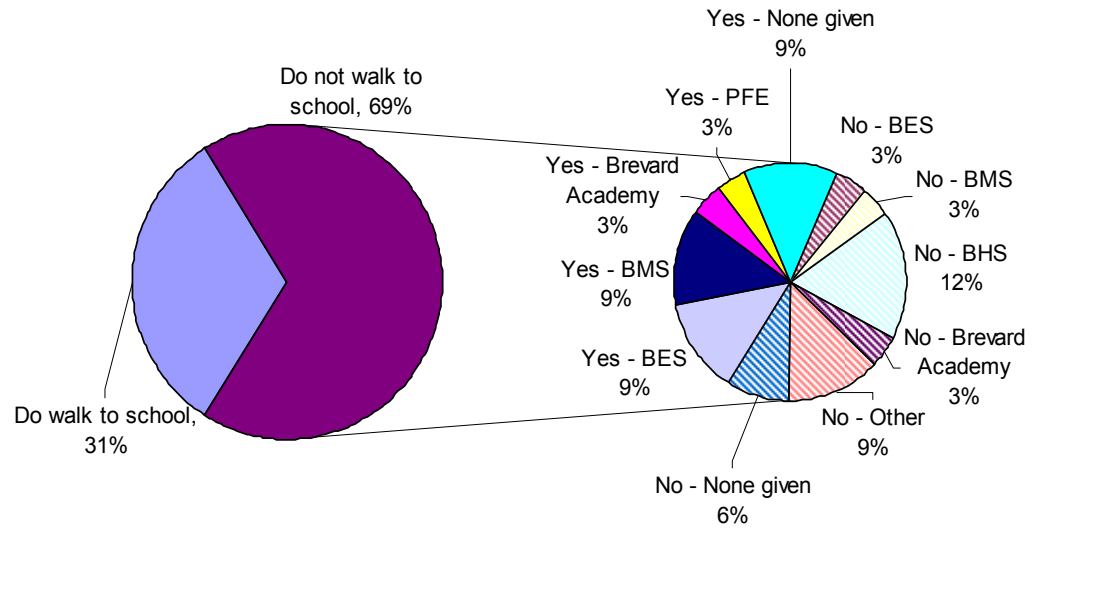
Do you have children under 18 years old living at home with you?



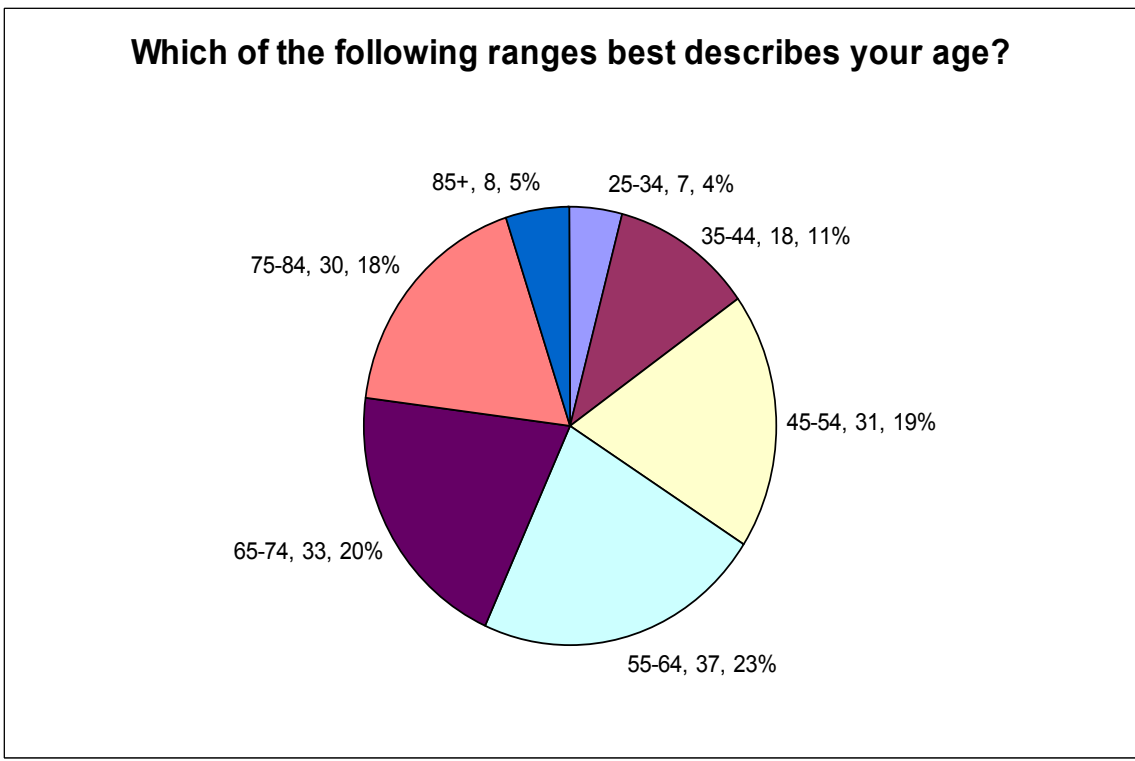
Do your children walk to school?



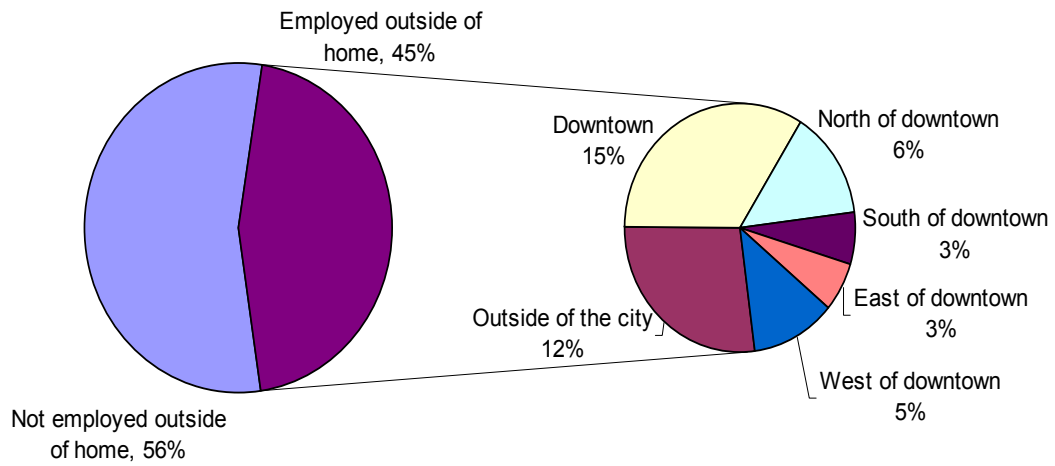
If your children don't walk to school, would they if facilities were available?



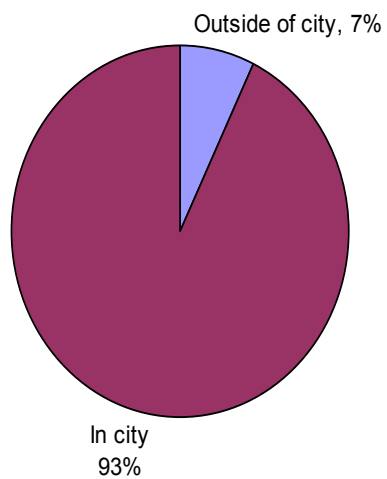
Which of the following ranges best describes your age?



If you are employed, where do you work?



Generally speaking, where in the city do you live?



Generally speaking, where in the city do you live?

I do not live in the city	12		
*None Given	1	Highlands Drive (PF)	1
Brown Road (PF)	1	Meadow Lane	1
Country Club Road	3	Morris Road	1
Deerwoode Lane	1	Ross Road	1
Forest Hills Road	1	South Hill Drive	1
In City	154		
*None Given	2	Jordan Road	3
Allison Road	1	Killian Lane	1
Ashworth Avenue	1	Laurel Lane	2
Autumn Glen Court	1	Maple Street	10
Azalea (Ave?)	1	Meadow View Road	1
Batson Road	2	Miner Street	2
Blue Bird Lane	2	Morningside Drive	1
Boxwood Lane	1	North Oak Street	1
Broadview Circle	3	Oaklawn Avenue	1
Bush Drive	1	Oakwood Drive	1
Cambridge Drive	5	Owen Street	1
Camp Sapphire Road (SP)	4	Park Avenue	8
Camptown Road (SP)	2	Parkview Drive	3
Canterbury Trace (DV)	2	Pine Street	1
College Row	5	Placid Drive	1
College Walk	1	Probart Street	1
Curlee Street	1	Quail Run Circle	1
Deerlake Road (DV)	3	Quinn Drive	1
Deer Run (DV)	1	Resada Drive	4
Dogwood Drive	1	Rice Street	1
Eagle Point Drive	1	Ridgewood Place	1
Eagle View Drive	1	Salem Street	1
East	1	South Caldwell Street	1
East French Broad Street	3	South Rice Street	3
East Main Street	2	Southview Drive	2
Eastview Drive	2	Stone Creek Trail (SP)	1
Elm Bend Road	4	Stone Drive	4
Far Hills Terrace	1	Stratford Avenue	1
Fox Cross Drive (DV)	4	Straus Park	1
Franklin Street	3	Temple Church Road	1
Gallimore Road	1	Trailside Road (SP)	1
Gaston Street	2	Tree Haven Drive	1
Grandview Avenue	1	Turnpike Road	2
Green Acre Avenue	1	Warren Lane	2
Greenville Highway	2	West Jordan Street	1
Grove Circle	2	West Main Street	1
Grove Street	2	Whitmire Street	4
Hamilton Drive	1	Wilson Drive	2
Hillview Street	1	Wintergreen Drive	2
Hilt Street Extension	1	Woodside Drive	1

Spring, 2005 City of Brevard Newsletter containing Pedestrian Survey.

City of Brevard Newsletter

Spring—2005



A publication for the Citizens of Brevard

Mack McKeller appointed to City Council

During the regular meeting of City Council, Monday, May 2nd, Council appointed Mr. Mack McKeller to fill the vacant seat on City Council. Mr. McKeller will serve the citizens of Brevard in this capacity until the November 2005 election. At that time, the citizens will elect a candidate to fill the remaining two years of the term. Mr. McKeller was chosen from among five citizens who submitted letters of interest for the vacancy.

Mr. McKeller has lived and practiced law in Brevard for the past ten years. He has also been serving on the City's Unified Development Ordinance Advisory Committee. In nominating Mr. McKeller for this position, Council Member Morrow stated that "Mack is a principled man, a devoted husband, father of four daughters and a respected attorney". Mr. McKeller responded that he is excited about what the City is doing and that the decisions being made are looking forward. The mayor, council, staff and employees welcome Mr. McKeller to this position and look forward to working with him. Mr. McKeller was sworn in and seated on May 16, 2005.

Message from Mayor Jimmy Harris: When I was first elected Mayor in November, 1999, I made good on a campaign promise by writing the North Carolina Department of Transportation and asking to have our traffic signals in Brevard synchronized. They (NCDOT) informed me that our current system had a mixture of antiquated signal units that could not network together to deliver the desired result. Basically, each traffic signal at each intersection would have to be manually set with timers that would not keep accurate time, especially when spring arrived along with "thunder boomers" that would disrupt the timers. I asked what the alternative was. "New signals with a new system" was their reply.

That was in 1999, and today, 2005, we are starting to see the results from that first inquiry. The new synchronized traffic signals called the "Closed Loop System" will deliver the "smartest" traffic management system offered. Many of you have seen the new mast arm poles at some intersections that will hold the new signal lights. What you do not see are the many sensors buried below the pavement. Those sensors will collect and report instant data to "live" online modems that constantly deliver information to a DOT computer bank in Sylva, NC that will make real-time decisions for each traffic light at each intersection as to traffic flow pattern needs. Some traffic lights may stay green longer or go to red quicker, depending on the traffic experience at the previous and successive intersections. This will reduce the unnecessary "stop and go" congestion and frustration that we currently have. This will also improve drive times and realize some fuel savings too. This is a much anticipated improvement to our traffic situation; however, it is not the permanent solution to all our traffic woes. There is much ongoing debate as to what is!

Do expect inconveniences and some delays from time to time as this new traffic system is being installed from Osborne Road to Country Club Road. We must realize that the end result will be worth it. And, what is the cost of all this? About \$1.3 million dollars which was provided by grant dollars from the NCDOT. The completion of this work is scheduled for this fall.

I hope to see you at one of our upcoming city council meetings — held the first and third Monday of each month at 7 p.m. Meetings are in City Council Chambers, 151 West Main Street.

Board/Commission Vacancies - 2005

During the next few months, City Council will be making appointments to various City Boards and Commissions. These include the Brevard ABC Board, Brevard Planning and Zoning Board, Brevard Personnel Board, Brevard Housing Authority and the Brevard/Transylvania Human Relations Council. If you are interested in serving on any of these Boards, contact the City Clerk at 884-4080 for more information.

BUDGET INFORMATION

The FY 2005-06 City Budget was presented to Council on May 16th. A public hearing has been scheduled to receive comment from the public. The hearing will be held at 7:00 p.m. or as soon thereafter as possible on June 6th. The public is invited to attend.

It's in the Bag

The Sanitation Division collects small yard waste fast! Grass clippings, leaves, pine cones, etc. may be bagged and placed at the curb for pickup. Call 884-2171 and bags will be removed within three (3) business days.

ELECTRONICS & HOUSEHOLD HAZARDOUS WASTE

The Transylvania County Landfill requires the following Electronics & Hazardous Waste Items to be disposed of separately. In order to provide citizens access to disposal, Transylvania County is holding an "**Electronics & HHW Recycling Event**" each first Wednesday of the month from **April through October**.

The hours are from 9:00 AM—12:00 PM at the Transylvania County Landfill on Woodruff Road.

You may call 884-5011 to schedule an appointment for disposal at other times if you can't make it on these days.

Items collected include:

Electronics (\$10 for computer monitors & TVs, \$5 for small items)

Computer Monitors and TV Sets	Computer Components
Telephones & Cell Phones	Copy & Fax Machines
Stereo Equipment	VCRs & Camcorders
Connectors, Cables & Wiring	

HHW (No Charge)

Used Motor Oil and Antifreeze, Pesticides, Weed Killers, Bug Sprays, Rodent Baits
Batteries, Cleaners, Degreasers, Solvents, Pool Chemicals

If you are unsure of an item for disposal, please contact the Transylvania County Landfill at 884-6830.

Frequently Asked Questions about Tree Removal

Do I have to have a permit to cut trees in the City of Brevard?

Yes, if you intend to cut a tree that meets the following criteria:

- The tree has a trunk six inches or more in diameter at one foot above the ground; or
- The tree is of a horticultural variety, or is highly ornamental (such as a dogwood, redbud, crab apple, sourwood, flowering cherry, holly or any like or similar such plant) and has a trunk diameter of three inches or more at one foot above the ground.

Failure to acquire a permit is a misdemeanor criminal offense.

Are any trees exempt?

Yes. You do not need a permit to remove a tree if any of the following apply:

- The tree is at a single-family or duplex residence.
- The tree is on property to be developed for single-family or duplex residential uses, except those developments that are permitted under Planned Housing Development provisions.
- You are pruning trees as normal maintenance provided such pruning does not result in the mutilation, death or destruction of the tree.

Also, the following trees are exempt from permit requirements:

- Chinese Elm, Silver Maple, Lombardy Poplar, Paulonia, Mimosa, and Tree of Heaven (Ailanthus).

What factors would lead to permit approval?

The City will generally approve tree removal permits in the following circumstances:

- The tree is diseased, in danger of falling, it is too close to existing or proposed structures, or it interferes with utility services.
- It is necessary to remove trees in order to construct proposed improvements to allow economic enjoyment of the property.

The City will take into consideration topography, the effect of tree removal on erosion, soil retention, the number of trees nearby, aesthetic factors, etc.

Where do I go to apply for a permit?

Permits cost \$5.00, and can be obtained at the City of Brevard Planning Department, 151 West Main Street, Brevard NC 28712. Telephone: (828) 883-8580. Fax: (828) 883-2853.

Dear Newsletter Recipient:

"Walkability" is emerging as a top concern of cities and towns across the nation. This renewed interest in making our communities more pedestrian-friendly is being driven by a number of factors: the public health community is very concerned with the rise in obesity, diabetes, and other related illnesses. Expanding fuel prices are causing Americans to reduce automobile trips. Housing developers and home buyers are increasingly interested in creating and living in communities where neighbors are connected to neighbors by pathways and recreation space, and where the built environment is designed around people rather than the automobile.

The City of Brevard is strongly committed to the development of a safe, functional, city-wide pedestrian system. In order to realize that goal the City is developing its first Comprehensive Pedestrian Plan. The purpose of the pedestrian plan will be to establish goals and a "roadmap" for the continued development of Brevard's pedestrian infrastructure – sidewalks, bike / walking paths, greenways, trails, etc. This plan will be used to guide City regulations, policies, and budgetary planning as those things relate to the development and maintenance of sidewalks and other pedestrian infrastructure.

One very important step in the development of the pedestrian plan is the gathering of public input. This newsletter contains a survey that will help the City identify deficiencies in the current sidewalk system and establish priorities for the expansion of that system. Please help us by taking time to fill out and return your survey. You may return your survey in one of the following ways:

- ✓ Mail to: City of Brevard Planning Director, 151 West Main Street, Brevard NC, 28712.
- ✓ Hand deliver to the Utility Payment Window or the City Planning Department at 151 West Main Street, Brevard.

Also, we encourage you to share this survey with other city residents who may not receive our newsletter. You may download and print an additional copy of this survey by clicking on the "Pedestrian Survey" link on the City's web page at <http://www.cityofbrevard.com/>.

One survey respondent will be randomly selected to win a \$100 cash prize.

To be eligible you must return your survey. Please print your name, address and telephone number in the spaces provided for within the survey. **Eligible surveys must be returned by June 15, 2005.**

Finally, please watch this newsletter and the local media for the dates and times of upcoming public meetings to discuss pedestrian issues, to be held at City Hall. In the meantime, if you have any questions or comments about this survey, the pedestrian plan, or the City's sidewalk system, please call the City of Brevard Planning Department at (828) 883-8580, send email to brevplandir@citcom.net, or visit us in City Hall at 151 West Main Street.

Thank you in advance for your time and thoughtful survey responses.

Respectfully,

Joshua S. Freeman, Planning Director



CITY OF BREVARD PEDESTRIAN NEEDS ASSESSMENT SURVEY

The City of Brevard is preparing a Master Pedestrian Plan for Brevard and the surrounding areas. Your response to this Needs Assessment Survey will help ensure that the finished plan reflects the needs and desires of the citizens of Brevard. Please return your completed survey to: **Joshua S. Freeman**, City of Brevard Planning Director, Planning Department, 151 West Main Street, Brevard, North Carolina 28712

In a typical week, how many days do you / how many hours do you spend doing the following activities?

	Days per week	Hours per week
Taking a walk		
Running / Jogging		
Skating (in-line or other)		
Using a motorized scooter outside of your home		
Using a manual wheelchair outside of your home		
Using a motorized wheelchair outside of your home		
Cycling		

How often do you walk 10 minutes or more in getting to or from a destination such as work or shopping, in a typical or average week? Please count each walk you make, both to and from a destination. Your answer must be a number. If it is a range, report the average. _____ **times**

If you walk less than once a week, check here []. (Check here if "don't know" [])

For the next section, please mark **Yes**, **No**, or **Don't Know**.

	Y	N	DK
Do you use any of the following to help you get around?			
Cane			
Seeing-eye dog			
Wheelchair			
Do you engage in pedestrian activities?			
At work?			
On the way to / from work?			
On the way to / from public transportation?			
Walking to / from school?			
Running errands / shopping?			
For exercise?			
Do you normally engage in these pedestrian activities...			
At a park?			
On a walking trail or path?			
On school grounds or campus?			
On sidewalks, streets, or roads in your neighborhood?			
On sidewalks, streets, or roads NOT in your neighborhood?			
At your work place?			

For the next section, please mark **Yes**, **No**, or **Don't Know**.

	Y	N	DK
Do any of the following KEEP YOU from doing more walking or other pedestrian activities?			
Too busy			
Poor health			
No one to walk with			
Dogs or other animals			
No nearby paths or trails			
No nearby parks			
No nearby sidewalks			
Unsafe street crossings			
No nearby shops or other interesting places to go			
Not enough people walking around			
Fear of street crime			
Too many cars			
Fast traffic			
Air pollution			
Lighting issues			
Unsafe or poorly maintained sidewalks			

The following statements are about walking in and around Brevard. Please indicate whether you **Agree** or **Disagree** with the following statements. You may also answer " **Don't Know**."

	A	D	DK
Access to sidewalks and greenways are an important factor to me in choosing a place to live.			
My neighborhood has safe and attractive areas for walking.			
I cannot walk in my neighborhood because there are no sidewalks, or existing sidewalks are not safe.			
I would like to live in a place where I can walk to work.			
I would like to live in a place where I can walk to shop.			
Brevard has an adequate number of sidewalks.			
The sidewalks in Brevard are in good condition.			

How important are the following factors in your decision to walk? Please mark **I** Important or **U** Unimportant. You may also answer “ **DK**.”

	I	U	DK
Availability of sidewalks			
Availability of benches			
Availability of shady streets and sidewalks			
Destinations that are a reasonable distance away			
Keeping a safe distance from traffic			
Personal safety, including fear of attacks from other people or animals			
Easy access to safe street crossings			
Convenient access to paved trails			
Weather conditions			
Attractive surroundings			
Adequate street lighting			
Rude motorists who deter me from walking			
Lack of access to parking			
Painted crosswalks			
Signs or signals that make drivers stop			
Having other pedestrians nearby			
Objects (for example, telephone poles) blocking passage of sidewalks			
Push buttons at crosswalk signals			
Audio crossing signals			
Curb cuts at corners or curb ramps			
Amount of traffic			
Speed of traffic			
Traffic signs telling drivers to slow down or watch for pedestrians			
Handicap accessible sidewalks / greenways			

Considering everything you’ve noted in this survey, how important are pedestrian improvements to you on a scale of 1-4 (1 = important, 4 = unimportant. Circle one)?

1 2 3 4

The rest of the questions on this survey ask for optional, demographic information. Your answers to the following questions will help us better analyze the results of this survey. **You may skip any question you choose not to answer.**

For the next section, please mark **Yes, No,** or **Don’t Know.**

	Y	N	DK
How long have you lived in Brevard? _____ years			
Do you expect to be living in Brevard 10 years from now?			
Do you work outside of your home?			
If “yes,” how many miles is your work from home? _____ miles			
Do you drive?			
Do you need a car for getting to and from your work?			
Do you need a car for on-the-job travel?			

- What is your gender? Male Female
- Do you have children under 18 years old living at home with you? Yes No
 If Yes, do your children walk to school? Yes No
 (Optional: School name _____)
- If No, would they walk to school if sidewalks or greenways were available
 Yes No
- Which of the following ranges best describe your age?
 18-24 35-44 55-64 75-84
 25-34 45-54 65-74 85+
- If you are employed, where do you work?
 Outside of the city North of downtown East of downtown
 Downtown South of downtown West of downtown
- Generally speaking, where in the city do you live? _____
 I do not live in the city Street name
 Brevard College Campus

The following area is for your comments regarding pedestrian facilities in Brevard. If you know of areas that need improvement (repairs, safety upgrades, new installation, etc.) please include that here.

Name _____ Telephone Number _____

Address _____

City _____ State _____ Zip _____

Thank you for your time and thoughtful survey responses. If you have questions or concerns, contact Joshua S. Freeman at the City of Brevard Planning Department.

Place
Stamp
Here



Joshua S. Freeman
 City of Brevard Planning Director
 Planning Department
 151 West Main Street
 Brevard, NC 28712

HOW TO BE HEARD BY CITY COUNCIL

The Brevard City Council is always interested in what you have to say and cordially invite you to attend a council meeting, held regularly the first and third Monday evening monthly. The meetings are held in City Council Chambers, located at 151 West Main Street and begin at 7 pm.

If the subject matter you want to address is a **Public Hearing** topic (listed on the agenda under Public Hearings), please sign your name and address on the Public Hearings Sign-Up Sheet provided by the City Clerk.

If you wish to address the Council on a topic that is **NOT** listed on the agenda, you may speak during the **Public Participation** section of the agenda. To be recognized you must sign up with the City Clerk prior to the meeting.

All other items on the agenda generally are not open for public discussion and interaction with Council during the meeting. The time for public input in these cases has either passed or is of the type that does not require public comment. In these cases, citizens must trust their elected officials to act in their behalf.

When you are recognized, please approach the podium and state your name and address for the record. You may then proceed with your comments, limiting your time at the podium to 3 minutes or less. If your subject matter demands more than 3 minutes of time, then you may want to consider submitting a written request to have your concern placed on a future agenda.

As a rule, Council will not take any action on matters for which they have not had the opportunity to fully investigate and gather complete information. Council may direct that the subject appear as an agenda item at a future date, or Council may direct that the matter be referred to staff.

Citizens may contact their elected officials at any time outside the Council Chambers. A list of phone numbers and email addresses is available from the City Clerk.



Another section of the pedestrian/bikeway has been completed. The newly paved

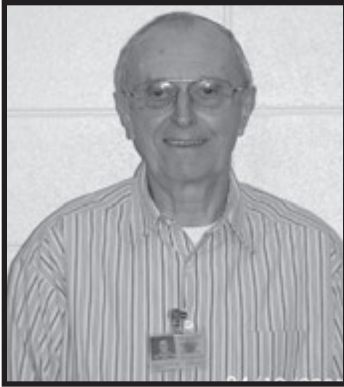
section begins off Ecusta Road at the Brevard Sports Complex. This section adds approximately 1/3 mile to the existing pathway. The next section to be completed will be to cross over Ecusta Road at the Sports Complex and go along Ecusta Road to the Davidson River Bridge

NEED MULCH ? The Public Works Department's Sanitation Division collects brush and yard debris. Brush is collected curbside and taken to the Public Works Facility where it is ground into mulch. This mulch is available to the public at no cost.

Bring your truck or trailer to the facility which is located at 405 Cashiers Valley Road between the hours of 7:30 am and 4:00 pm, Monday through Friday, and we will be happy to load for you.

Smaller amounts are available too! Bring your can, bag or box and take the mulch you need. This mulch is great for lawn or garden use and helps recycle the City's yard waste and save landfill space.

Brevard Police
Department
Volunteer
Ed Imhoff



Who is here on Monday morning to start the week off with a positive attitude and in an upbeat mood ?? — Volunteer Ed Imhoff.

Ed started volunteering with the Brevard Police Department in October 1995, and was the first volunteer through the Land of Sky Regional Retired and Senior Volunteer Program of Asheville. Ed decided to volunteer because his wish was to help the community and the local law enforcement agency. His past experience in law enforcement as a patrolman and as a detective for Nassau County Police Department in Long Island, NY has aided the Brevard Police Department personnel with his knowledge and his “stories” of past law enforcement.

Ed assists Ina on Mondays with paperwork; but, his outgoing personality, humor and concern for law enforcement makes him a very valuable asset to the department. As Ina says, “we couldn’t do it without him as he starts the week off the right way — POSITIVE!

If you would like to join this fine group, please contact David Stackpole at 877-3558, or Ina Whitmire at the Police Department at 883-2212

Franklin Pool is set to open Memorial Day Weekend.

Please check out website for hours of operation
and details in May—www.cityofbrevard.com

There will be no sanitation collection services for City of Brevard customers on Memorial Day, Monday, May 30th. Monday’s make up day will be on Wednesday, June 1st. The City will also be closed on Monday, July 4th so there will be no garbage or cardboard collection on that date. Monday’s makeup day will be Wednesday, July 6, 2005. If further information is needed, please call 884-2171 or email at brevpwks@citcom.net.

If you would like to purchase a brick at the Franklin Park Playground, call the City Clerk’s office at 884-4080 for information and an order form.

The cost per engraved brick is \$50.00

Peddlers & Door to Door Salesmen

If someone comes to your door or approaches you in a parking lot or on the streets and sidewalks, use caution before you purchase their merchandise. If it sounds too good to be true, it usually is!! Everyone selling merchandise in the City is required to have a City license . Ask to see their license! Many times companies send people in to town after hours or on the week-ends and sell their products without having valid licenses. The city requires certain information be on file about the company and the sales people. Always use caution when being approached, ask to see identification and note the type vehicle they are driving, get a tag number if possible . Finally, if they will not take “no” for an answer, call the Police Department and ask for assistance. A little caution could save lots of hassle!!

Become A Part Of History

by sponsoring a bench, street lamp or refuse container



"Downtown Brevard represents the 'fabric of our community' and it is defined by how it functions and how it looks.

An ordinary place becomes extraordinary when elements are integrated and the visual character is enhanced."

- Brevard Downtown Master Plan Executive Summary



Become A Sponsor

You're invited to help celebrate the continuing revitalization of Brevard's historic downtown by sponsoring a bench, street lamp or refuse container.

This initiative is being coordinated by the Heart of Brevard under the direction of the Downtown Master Plan Advisory Committee.

Each downtown street element will have a plaque dedicated to the sponsoring person, company or organization.

Benches, street lamps and refuse containers can be "sponsored by, in memorial to, or in honor of" the person of your choice.

Become a part of historic downtown Brevard and help create a better Brevard today... and for years to come.

The Look Of The Past. . . But Built For The Future

President John F. Kennedy once said, "Change is the law of life and those who look only to the past are certain to miss the future". The City of Brevard's Downtown Master Plan looks to the future but has its foundation firmly set in our rich historical past.



Downtown Brevard is part of the City that belongs to everyone. It is where private business, the City, the County, and a community work, live, and play. It is the community gathering place.

While the City of Brevard and the Heart of Brevard are leaders in this revitalization effort, the implementation of the Downtown Master Plan also depends on the efforts of community groups, organizations and individuals.

We invite you to leave your name, your mark, on downtown. Become a part of this important revitalization process for our community.



Sponsorship brochures are available at:

The Heart of Brevard Office
Brevard City Hall
Brevard/Transylvania
Chamber of Commerce.

For additional information, call
The Heart of Brevard at
(828) 883-4679.

City Contact Information

City Manager	883-3880
City Clerk	884-4080
City Attorney	884-4113
Police Department	883-2212
Finance/Water Department	884-4123
Human Resources	884-4124
Planning Department	883-8580
Public Works Department	884-2171
Building & Grounds	862-4119
Utilities (Water & Sewer Plants)	883-8461
Fire Department (Volunteer)	883-3333



Interested in serving on a City Board or
Commission — for information call
884-4080

Brevard City Council

Jimmy Harris, Mayor
Rodney Locks, Mayor Pro-Tem
Mac Morrow
Mack McKeller
Dee Dee Perkins
Sara Champion

City Council Meetings

The Council meets on the first and third
Monday evenings, monthly at 7 p.m.
Meetings are held in Council Chambers
located at 151 West Main Street

City Offices will be closed on

Monday, May 30, 2005—Memorial Day

Monday, July 4, 2005—Independence Day

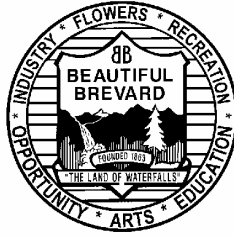
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www.cityofbrevard.com

Appendix D4-Public Input Session Background Documents:

1. Strategy Document
2. Poster Advertisement
3. Agenda



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Pedestrian Plan

Strategy for the 2nd Public Involvement

Wednesday October 5th, 2005

Goals – what information are we going to give and what do we want to discuss? :

- Present results of the questionnaire survey, (McGill A. – Rhonda H. Honeycutt)
- Present some parts of the draft pedestrian plan – as it corresponds to the results of survey : summary, objectives and top priorities, maintenance issues, maps including legends - 2 maps of existing and proposed conditions, (text: McGill A. – Rhonda H. Honeycutt, Dale Pennell; maps: City of Brevard – Joshua S. Freeman)
- Discuss areas that need improvement in groups – review the maps and take comments

Target Groups:

- City inhabitants - Primarily students (High school, College) /youth and their parents
- Local NGO's (see a list of organizations)
- Private sector – local outfitters - outdoor shops, Brevard Athletic Club, Fitness Factory...
- Local authorities – county and city committees, boards and city council (see the list)

Time Schedule, Program, Material and Personal Ensure:

- Public discussion/meeting

Date: Tuesday 6th December 2005, 6pm - 8pm

Place: City Chamber

Program:

6:00 - 6:05 pm	Opening – Mayor: Jimmy Harris (5 minutes)
6:05 - 6:45 pm	Presentations: 10 min Results of the Survey; 20 min Summary, Objectives and Top priorities, Maintenance issues; 10 min maps ... (40 min. – but we <i>should have an effort to make it shorter – up to 30 min.</i>)
6:45 – 6:50 pm	Break, furniture rearranging (5 minutes)
6:50 - 7:30 pm	Work in groups – reviewing the maps and taking comments (40 min. - answering the committee questions, scheduling priorities, making list of pros and cons ...)
7:30 - 7:50 pm	Presentation of the group's conclusions (20 min. or more)
7:50 - 8:00 pm	Conclusion (10 min.)

Moderator: McGill A. - Dale Pennell

Presentations: McGill A. - Rhonda H. Honeycutt, Dale Pennell; City of Brevard - Joshua S. Freeman

Facilitators: Members of the Steering Committee → ***meeting of the committee: November 10, 2-4pm in City Chamber***

Administrator, Distributor: Radka Chalupkova

City of Brevard will provide a setup of the chamber, office materials (papers, markers, pencils, flipchart, flipchart papers, computer, projector . . .), food and beverages. . .

Work in groups: groups will be facilitated by some of the committee members, tasks: scheduling priorities, listing specific neighborhoods between which connections are wanted/unwanted and why, ask the group questions about the plan (up to five, formulated by the committee → set up a committee meeting, give proposed facilitators basic information about the facilitating process) - the group will discuss those questions and after that can make a list of pros and cons of the plan with their comments.

Sorting process for the work groups: random mixing by color of name tags after the opening and presentations (Students should be in their own group(s) – want to hear what they have to say in their own words, not through adult/parent. Divide husbands and their wives.)

Advertising: City of Brevard – Radka Chalupkova, Joshua S. Freeman

- **Start at the beginning of November**
- **Display draft plan in:** Transylvania County Library, Brevard College Library, High School Library, City Hall, Brevard Athletic Club, on web pages (City's, County's, Chamber's of Commerce) → draft plan must be done by the end of October
- **Invitation:**
 - ✓ Newspapers – big paper article in Transylvania Times that highlights various components of the draft plan → press release, paid graphic advertisement
 - ✓ City Newsletter – similar to in newspaper
 - ✓ Local radios, TV → press release, maps, pictures
 - ✓ College radio / newspapers (if there are any)
 - ✓ Internet - City's, County's, Chamber's of Commerce web pages
 - ✓ Posters, Cards – grocery stores, outfitters, athletic clubs, all type of schools – students can bring it home, send to contacts from the questioner survey → create the poster
 - ✓ High school class teachers / Scouts – possibility of special credits for attending the public meeting (Civics, FBLA, Physical Education . . .)
 - ✓ Personal invitations – to the NGO's heads and their members, private business owners, local authorities members

Comments:

- Processing : McGill A. - Rhonda H. Honeycutt
- Discussing: ***meeting of the steering committee on December 13, 2pm-4pm in City Chamber***
- Publication: revised draft plan displayed in the same places as before the public meeting, article in the newspapers – highlight changes.

List of NGO's that might be interested and might public an invitation in their newsletters:

- Scouts
- MOPS
- Chamber of Commerce
- Heart of Brevard
- Transylvania County Development Organization
- SIERRA Club
- Brevard College Student Government
- ? Neighborhood Associations?
- PTO (Parent/Teacher Organizations)
 - Brevard Elementary School
 - Pisgah Forest Elementary School
 - Brevard High School
 - Brevard Middle School
 - Brevard Academy
- ? Some organization for handicapped people ?
- Rotary International

List of local Authorities that might be interested:

- Transylvania County Planning Board
- City of Brevard Planning Board
- Transylvania County Recreation Advisory Committee
- City of Brevard Recreation Advisory Committee
- City Council
- City of Brevard Zoning Board of Adjustment
- UDO Committee
- Down Town Master Plan Advisory Committee
- PP Steering Commiteee

Private sector

- local outfitters (NALI) - outdoor shops,
- Brevard Athletic Club, Fitness Factory...



DO YOU ENJOY WALKING,
JOGGING OR CYCLING?

ARE YOU A DOG WALKER
OR BIRDWATCHER?

HELP US CREATE

WALKABLE BREVARD

ATTEND THE CITY OF BREVARD PEDESTRIAN PLAN INPUT SESSION

IT IS AN OPPORTUNITY FOR YOU AS A MEMBER OF THE COMMUNITY TO WORK COLLECTIVELY ON PEDESTRIAN TRANSPORTATION ISSUES.

GOALS FOR THIS SECOND INPUT MEETING:

- OVERVIEW OF THE RESULTS OF THE QUESTIONNAIRE SURVEY
- PRESENTATION OF THE DRAFT PEDESTRIAN PLAN
- DISCUSS NEEDED IMPROVEMENTS TO BREVARD'S SIDEWALKS
- REVIEW MAPS AND TAKE COMMENTS.



WHEN?

TUESDAY, DECEMBER 6TH,
FROM 6-8 PM

WHERE?

CITY COUNCIL CHAMBERS
151 WEST MAIN STREET, BREVARD

FOR MORE INFORMATION: VISIT WWW.CITYOFBREVARD.COM,
CALL: 828-883-8580, E-MAIL: BREVPLANDIR@CITCOM.NET

Public input session

Date: Tuesday 6th December 2005, 6pm - 8pm

Place: City Chamber

Moderator: Josh

Presentations: McGill A. - Rhonda H. Honeycutt, Dale Pennell; City of Brevard - Joshua S. Freeman

Facilitators: Josh, Rhonda, Radka, Dale, Desiree ...

City of Brevard will provide a setup of the chamber, office materials (papers, markers, pencils, flipchart, flipchart papers, computer, projector ...), food and beverages (few gallons of water, cups, ice, coffee, variety of cookies)...

Goals – what information are we going to give and what do we want to discuss? :

- **Present results of the questionnaire survey**, (McGill A. – Rhonda H. Honeycutt)
- **Present some parts of the draft pedestrian plan – as it corresponds to the results of survey** : summary, objectives and top priorities, maintenance issues, maps including legends - 2 maps of existing and proposed conditions, (text: McGill A. – Rhonda H. Honeycutt, Dale Pennell; maps: City of Brevard – Joshua S. Freeman)
- **Discuss areas that need improvements in groups and take comments**

By door (Desiree and Radka) – everyone gets nametag (blue, green, red), 5 points and Josh's priority list

6:00 - 6:05 pm	Opening – Mayor: Jimmy Harris (5 minutes)
-----------------------	--

PUT YOUR NAME ON THE NAME TAG - *do not explain the sorting process at this point.*

6:05 - 6:45 pm	Presentations: 10 min (6:05 – 6:15) Results of the Survey (Rhonda) 20 min (6:15 – 6:35) Summary, Objectives and Top priorities, Maintenance issues; 10 min (6:35 – 6:45) maps (Josh – be able to see on which side of the road SW and MUP is planned)
-----------------------	---

(40 min. – but we should have an effort to make it shorter – up to 30 min.)

6:45 - 7:00 pm	Questions – <i>be sure to make the public reserve any comments for the work in groups</i>
-----------------------	--

7:00 – 7:10 pm (10 min) Break, reviewing the maps - lobby (set up a display)

furniture rearranging – one room already ready (Josh, Dale, Rhonda)

Tables: 1 Josh
 2 Dale, Desiree
 3 Radka, Rhonda
 4 ☠

Color sheet that indicates a color of the group, maps, markers, flipchart papers, pencils, notepads, tape and “point” for each table

Group sorting, name tags – make people to return to their table (by color – blue, red, green)


7:10 – 7:15 pm (5min) – make people sit by the tables by colors and explain the work in groups

7:15 - 7:45 pm Work in groups

- Sidewalk ideas and prioritization (neighborhood connections, improvement of conditions, prioritization of existing list, where facilities need to go, ...)
- Multi use trails ideas (connection, rail trails, where facilities need to go, and prioritization of existing list ...)
- “What I don’t want, I disagree with?” – list
- We are not going to discuss crossroads at this point – but we will have “parking lot” list for this

1. step – make idea lists for SW and MUP – work with Joshes priority list to do not double the ideas – 20 min (7:15 – 7:35)

Group’s ideas for SW	Joshes priority list for SW– facilitator needs to know	Group’s ideas for MUP	Joshes priority list for MUP – facilitator needs to know
-------------------------	---	-----------------------------	---

What they don’t like 	Crossroads “p”
--	-------------------

2. step – prioritization – use the labels (color points) to make Group’s “top ten list”, evaluate only group’s ideas for SW and MUP, do not use more than one point for one idea, group should pick up somebody to present the list - 10 min (7:35 – 7:45)

Group’s **TOP TEN** priority list
– includes SW and MUP

3. presentations of the results of the groups – 15 min (7:45 – 8:00)

Group’s top ten

Group’s top ten

Group’s top ten

Josh is going to make a list for the prioritization during the presentation to eliminate doubles. Radka assists to hang the papers.

Expl:

Neely	SW	MUP
W Main	SW	

4. prioritization of the group’s top ten lists (working whit the list Josh made during the presentations) 15 min (8:00 – 8:15)

Public input session
TOP, TOP,
TOP

8:15 - 8:25 pm Conclusion (10 min.)

Appendix D5-Public Input Session Results Summary



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Brevard Comprehensive Pedestrian Plan Results of the 2nd Public Input Session

Date: Tuesday 6th December 2005, 6pm - 8pm

Place: City Council Chamber

Attendance: 75 people

Summary:

Approximately seventy-five people attended the public input session to discuss the City's Comprehensive Pedestrian Plan, which will identify and prioritize new Multi Use Paths (MUP), sidewalks (SW) and recreation and fitness trails (REC) in Brevard. Participants worked in three groups to develop a priority list of new facilities. The groups first worked on their own by ranking their priorities of the MUP, SW and REC segments. Then they presented their "top ten" list to the other groups. Together the groups then created the overall "**master top ten priorities**" of the public input session.

Participants also suggested some **new ideas** which were not included in the original priority list. Some ideas, such as No. 10 Priority – MUP from Davidson River all the way to the Fish Hatchery in the Pisgah National Forest, fall beyond the scope of the Comprehensive Plan and are unlikely to be listed as priorities in the Plan. However, such ideas will be noted in the plan as potential future opportunities. Virtually all other priorities including other new ideas identified by the public are recommended to be added into the Plan.

Overall Master Priority List of the Public Input Session

MUP/SW/Rec.	PR	Street	Street Segment	Points
MUP	1	Gallimore Road	US 276 to Country Club	23
Rec.	2	Bracken Mountain	through	19
SW	3	Probart St.	Railroad Ave to Music Center	17
MUP	4	Neely Rd / Park View Drive		17
MUP	5	Main Street (West) to Music Center & Rosenwald Community		14
MUP	6	Elm Bend		13
MUP	7	Asheville Hwy to Davidson River Campground	Along Davidson River	12
SW	8	US 276 (South)	Main St. (East) to Brevard Elementary both sides / opposite MUP	8
SW	9	Elm Bend	Saphire Manor - East View	8
MUP	10	<i>Davidson River - Hatchery</i>		8
MUP	11	Rosman Hwy to Country Club Connector		7
MUP	12	Asheville Hwy	Hospital Drive to Wal-Mart	6
MUP	13	Old Hendersonville Hwy	Neely Rd / Park View to Ecusta Road	4
Rec./MUP	14	Kings Creek		4
SW	15	Hospital Dr / Temple Church Rd	Hospital to Morris Rd	3
SW	16	Asheville Hwy	Food Lion to Pisgah Forest	2
SW	17	Elm Bend		2
SW	18	Johnson		1

PR = priority

Points = points added by people from all groups

Results of the public input session were partially reviewed by the City of Brevard Pedestrian Plan Committee members on December 13, 2005. The priority list will be completed at the next committee meeting which will be held in the second half of February, 2006. It's likely that the committee will make some changes to the priority list in light of the results of the public input session and the committee's own deliberations.

No additional public input sessions are planned. Results from the public input session, their overview by the committee and final draft will be presented to the City Council, who will take public comments prior to the adoption of the Plan.

Public Overall = priority by points received in all three groups before a creation of the “master top ten priority list”

* New Ideas of the public

PROPOSED MULTI-USE PATHS			
Street	Street Segment	Priority	Points
Allison Road to McLean Road		5	10
Asheville Hwy	Hospital Drive to Wal-Mart	7	8
Asheville Hwy to Davidson River Campground	Along Davidson River	3	22,17
Brevard College	Internal connecting Neely Rd to Asheville Hwy	8	7
Cashiers Valley to Rosman Hwy		10	5
Ecusta Rd	Connect Asheville Hwy to Old Hendersonville Hwy	7	8
Gallimore / High School / Rosman Hwy	Along Norton Creek	7	8
Gallimore Road	US 276 to Country Club	2	28
Greenville Hwy	Elm Bend to Bert Lane Rd	7	8
Main Street (West) to Music Center & Rosenwald Community		4	19
Main Street (West) to Rosman Hwy & Rosenwald Community	Along Kings Creek	13	2
McLean Road to Probart Street		9	6
Neely Rd / Park View Drive	US 276 to Old Hendersonville Hwy	1	34
Old Hendersonville Hwy	Neely Rd / Park View to Ecusta Road	9	6
Pisgah National to Davidson River Campground		----	
Railroad Ave to Asheville Hwy along Kings Creek	Along Kings Creek	7	8
Rosman Hwy	Carolina Avenue to Winn Dixie	12	3
Rosman Hwy to Country Club Connector		4	19
US 64	276/64/280 Intersection east towards Hendersonville	10	5

*Chestnut Street		13	2
*Davidson River to Hatchery		6	9
*Elm Bend	US 276 to Wilson Road	9	6
*Greenville Hwy	E. Main to Wilson	13	2
*Hospital Road to Hospital		14	1
*Lambs Creek Road		13	2
*Wilson Road	Knob Creek to Old Hendersonville Hwy	11	4

Mentioned but no votes: Osborne Rd. from Asheville Hwy. to Old Hendersonville Hwy., Brevard to Hendersonville, & Brevard to Cashiers

PROPOSED RECREATION PATHS			
Street	Street Segment	Priority	Points
Along Kings Creek to National Forest	Along Kings Creek	1	17
Bracken Mountain Property		2	15
Through Deer Lake to National Forest		3	6

PROPOSED SIDEWALKS			
Street	Street Segment	Priority	Points
Allison Rd	Connection to Multi-Use Path	11	0
Asheville Hwy	Food Lion to Pisgah Forest	6	5
Asheville Hwy / Ecusta Rd Connector		9	2
Azalea Ave		11	0
Brevard High School	Within facility connect Gallimore to Country Club	7	4
Broad Street (North)	Main Street to Food Lion both sides	8	3
Caldwell	Food Lion to Rosman Hwy both sides	5	8
Caldwell to Broad to Varsity at Cardinal's		11	0
Cambridge Drive to Multi-Use Path connection behind McDonalds		7	4
Camp Town Road		11	0
Carolina Avenue		11	0
Cashiers Valley Road		11	0
Chestnut Street		10	1
Deer Lake Road		9	2
Ecusta Rd	Asheville Hwy to Ecusta Road opposite MUP	6	5
Elm Bend		2	14

French Broad		6	5
Grove St. / Silvermont Park	Maple St. to Greenville Hwy through Silvermont	6	5
Hayes St. / Laurel Lane / Turnpike / Aspen Place	Country Club to Turnpike through Shephard Square	11	0
Hospital Dr / Multi-Use Path Connector	Hospital Drive to City Ball Fields along Koala boundary	6	5
Hospital Dr / Temple Church Rd	Hospital to Morris Rd	5	6
Johnson	Maple - Jordan	3	12
Keith Court to Mulit-Use Path	Connect Keith Ct to proposed MUP	11	0
Kings Creek Rd	From Tinsley Rd to Jurisdictional Boundary	10	1
Kings Mill Rd	Probart St. to Whitmire St.	11	0
Main St Ext	Main St Ext to Wilson Drive	8	3
Street	Street Segment	Priority	Points
Main Street (West) to Probart through WCCA		10	1
Maple St Ext	Maple St. to Gallimore Rd	6	5
McLean Rd	Railroad Ave to Asheville Hwy	11	0
Miner	Miner	9	2
Morris Rd	Asheville Hwy to Ecusta Rd	9	2
NC 280	276/64/280 Intersection to Wal-Mart	5	6
Oakdale Rd	Old Hendersonville Hwy to Osborne	10	1
Oakdale Rd / Ash. Hwy. Connector	Oakdale to Asheville Hwy through Chestnut Square	9	2
Old Hendersonville Hwy	Neely Rd to Asheville Hwy	5	6
Osborne Rd		10	1
Owen		11	0
Probart St.	Railroad Ave to Music Center	1	27
Rosman Hwy	Broad (South) to Winn Dixie both sides / opposite MUP	11	0
Silversteen	Cashiers Valley Rd to Caldwell	8	3
Southview Dr	MUP along Norton Creek to Country Club	11	0
Spring House Trail		11	0
Straus Parkway		10	1
Tinsley Rd		11	0
Turnpike		5	6
US 276 (North)	276/64/280 Intersection to National Forest	3	12
US 276 (South)	Main St. (East) to Brevard Elementary both sides / opposite MUP	4	10

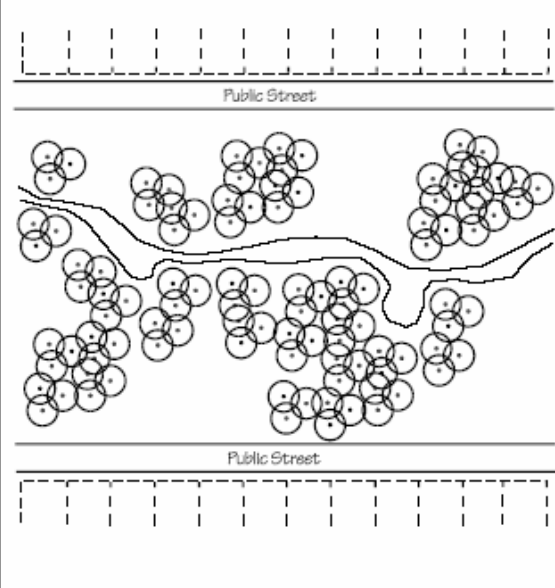
US 64	276/64/280 Intersection east towards Hendersonville	6	5
Varsity / Gaston Streets	Broad (South) to Maple	11	0
Verdery Ave	Carolina Avenue to MUP along Norton Creek	11	0
<i>*Franklin Park</i>	<i>Hilt, Lakeview, Harold</i>	6	5
<i>*Probart St.</i>	<i>Broad to Caldwell</i>	11	0
<i>*Wilson Dr.</i>		8	3
<i>*Wilson Rd.</i>	<i>Glen Cannon to Knob Creek</i>	8	3

CITY OF BREVARD PLANNING DEPARTMENT SUMMARY OF EXISTING SIDEWALK REGULATIONS

From the Unified Development Ordinance

CHAPTER 7. OPEN SPACE

Section 7.6 – Open Space Types Type II – Public Open Spaces

 <p>The diagram illustrates a parkway or greenway. It features a central, irregularly shaped area containing a winding path, a creek, and clusters of trees represented by circles with dots. This central area is bounded by two horizontal lines, each labeled "Public Street". Above and below these street lines are dashed lines representing the boundaries of the parkway or greenway.</p>	<p>9. PARKWAYS / GREENWAYS</p> <p>Parkways and greenways are large, irregular open spaces designed to incorporate natural settings such as creeks and significant stands of trees within neighborhoods. Parkway are to be partially bounded by streets or pedestrian R-O-Ws within developed areas. Greenways are exempt from this requirement.</p> <p>Parkways and greenways differ from parks and squares in that their detailing is natural (i.e. informally planted) except along rights-of-way, and may contain irregular topography. Parkway and greenways may be used for certain active recreational uses and should provide at a minimum, recreation trails for walking, jogging, or bicycling.</p> <p>Interior areas should remain natural and any additional plantings should be informal in design.</p>
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CHAPTER 9. CIRCULATION AND CONNECTIVITY

Section 9.2 – Connectivity Index

F. Pedestrian and Bicycle Facilities: Where residential developments have cul-de-sacs or dead-end streets, such streets shall be connected to the closest local or collector street or to cul-de-sacs in adjoining subdivisions via a sidewalk or multi-use path, except where deemed impractical by the Administrator.

G. Required Connectivity Index: 1.5 This is required for new neighborhoods, though variations may be granted for severe topographic conditions. In neighborhoods with a low Connectivity Index due to severe topographic conditions, pedestrian and/or bicycle path connections, a minimum of 10 feet wide, shall be installed to supplement the street system by linking dead-end streets.

CHAPTER 10. PARKING STANDARDS

Section 10.3 – Off Street Parking Requirements

F. Variations in Parking Requirements

1. Satellite Parking

b. The off-site parking spaces shall be located within three hundred (300) feet walking distance of a public entrance to the structure or lot containing the use for which such spaces are required. A safe, direct, attractive, lighted and convenient pedestrian route shall exist or be provided between the off-site parking and the use being served;

Section 10.5 – General Off-Street Parking Area Design Standards

F. Pedestrian Amenities:

1. Off-street parking areas shall be designed to allow pedestrians to safely move from their vehicles to the building. Barrier free access shall be provided across parking lots and between adjacent pedestrian generators such as commercial or public buildings.

2. All structures (other than single-family and duplex structures) shall be connected to the street and, if available, an adjacent public sidewalk, by a sidewalk. Furthermore, all structures within a group development, planned development, or other project containing multiple principal structures shall be connected by a sidewalk.. Sidewalks shall also connect other phases of the same development, and adjacent developments, as required by the Administrator

3. In addition to any required sidewalk, barrier free access shall also include providing ramps at curbs, clear walkways between pre-cast wheel stops, and clearly delineated walkways on the parking lot surface across travel lanes.

4. For lots of 36 spaces or greater, no parking space shall be more than 65 feet from a sidewalk that provide access to the entrance of the building or to other sidewalks connecting to the building. Sidewalk corridors a minimum of 5 ft in width shall be provided within the parking area or along the perimeter to provide safe building access for pedestrians.

CHAPTER 11. LIGHTING

Section 11.2 – Outdoor Lighting Standards

B. Outdoor lighting shall be designed, located, and mounted at heights no greater than eighteen (18) feet above grade for pedestrian lights, or thirty-five (35) feet above grade for street or parking area lights.

C. Light sources may be located within the perimeter landscaped area and along pedestrian walkways providing provision 12.2 (E) is met.

G. Lighting fixtures scaled for pedestrian activities shall provide for uniform lighting distribution to produce minimal shadows.

Section 11.3 – Design Procedures

USE	Illumination (in Foot-Candles)	
	Maximum (Glare)	Minimum (Safety)
UTILITY LIGHTING		
Minimum Visibility	-	.5
Driving	1	.5
Pedestrian Ways	1	.5
General landscaped areas	2	1
AREA LIGHTING		
Gas Station & Convenience Store Canopies	10	1
Parking Lots	4	1
BUILDING EXTERIORS		
Entrances (frequent use)	6	2
Entrances (infrequent use)	1	.5
Vital Locations or Structures	6	2
Building Surroundings	2	1

Section 11.5 – Street Lighting

Street lighting shall be placed on all streets by the developer to ensure for the safe use of streets by both cars and pedestrians. All street lighting shall be placed in accordance with the following minimum design standards:

- A. Minimum average street light spacing:
 1. Single Family Homes and Duplex lots (greater than 100 feet wide): 500 feet
 2. Single Family Homes and Duplex lots (less than 100 feet wide): 300 feet
 3. Multi-Family Uses: 150 feet
 4. All other uses: 300 feet
- B. Preference in placement shall be given to street intersections and street curves.
- C. Pedestrian lighting should be prioritized over automobile lighting. Lighting should be placed in a manner to limit the casting of shadows on sidewalks.
- D. All lighting shall utilize full-cut-off fixture. Where buildings are close to the street (less than fifteen (15) feet from the right-of-way) full cut-off fixtures are required to limit glare and light spillage on upper levels.
- E. Alleys are excluded from the spacing and placing requirements of this Chapter but are encouraged to be illuminated using private security lights, wall packs, or similar low level decorative lighting.
- F. The requirement that street lights be provided may be waived by the approving authority in mountainous areas where street lights would be visible from the valley floor.

CHAPTER 12. SIGNS

Section 12.9 – Signs allowed with permits.

J. Decorative flags, banners and other moving devices

9. No banner, flag, pennant, or moving device shall be secured to the wall in such a manner that the bottom portion is at least seven feet from the sidewalk or does not interfere in any manner with pedestrian traffic.,

L. Festivals and special events sponsored by government, educational, religious, charitable, civic, fraternal, or political organizations and institutions:

Signs, banners, balloons, decorative flags and windsocks (hereafter, “special event signage”) may be displayed during festivals and special events by permit during the day(s) of the event, subject to the following requirements:

7. No special event signage shall create a traffic hazard or obstruction to motorists or pedestrians.

11. Special event signage may not be illuminated, moving, or otherwise constitute a vehicular or pedestrian safety hazard.

CHAPTER 13. INFRASTRUCTURE IMPROVEMENT REQUIREMENTS

Section 13.2 – Required Improvements for all Development Plans

Public Improvement Types	Brevard City Limits	Extra-Territorial Jurisdiction
Potable Water	City Water Required	Private Water Required; City Water Optional
Fire Suppression System	Hydrants Required	Hydrants required if City Water Provided; Dry Hydrants may be required
Sanitary Sewer	City Sewer Required	Adequate Septic Required; City Sewer Optional
Public Streets	City Streets Required	NCDOT Streets Required
Street Rights-of-Way	Dedication to City Required	Dedication to NCDOT Required
Utility & Access Easements	Dedication to City Required	Dedication to City Required
Sidewalks / Multi-Use Paths / Other Pedestrian Facilities	Required	Optional
Pedestrian Easements or Right-of-Way	Dedication to City Required	Required Along Streets; Dedication to NCDOT Required
Curb and Gutter	Required	As required by NCDOT
Street lights	Required	Optional
Underground Wiring	Required	Optional
Dedicated Open Space	Required as per Chapter 8 of this Ordinance	Required as per Chapter 8 of this Ordinance
Street Trees & Landscaping	Required	Required

Section 13.3 – General Provisions

C. Conformity to existing maps or plans: The master plan and any plat of a subdivision and the master plan and development plan for any other development shall conform to any applicable adopted plans of the City of Brevard, including but not limited to the *Comprehensive Transportation Plan*, the *Comprehensive Pedestrian Plan*, the adopted Land Use Plan or Comprehensive Plan, the *Downtown Master Plan*, any adopted recreation plan, corridor plan, master plan or small area plan, and any adopted transportation plans or policies of Transylvania County that are not in conflict with adopted plans or policies of the City.

Section 13.5 – Street Design

C. Pedestrian & Bicycle Infrastructure shall be required as set forth within this Chapter and conformance with adopted plans and policies of the City or Transylvania County.

K. Blocks:

2. Blocks shall not be less than 200 feet nor more than 660 feet (1/8 mile), as measured from edge of right-of-way, unless site and topography or other special circumstances are present as determined by the TRC. Where deemed necessary by the TRC, a pedestrian crosswalk of at least ten feet in width may be required.

L. Landscaping:

1. Streets shall be landscaped with street trees. Commercial streets shall have trees which compliment the face of the buildings and which shade the sidewalk. Residential streets shall provide for an appropriate canopy, which shades both the street and sidewalk, and serves as a visual buffer between the street and the home.

N. Pedestrian & Bicycle Infrastructure:

Sidewalks or multi-use paths shall be constructed along both sides of all streets except alleys and commercial service streets. The following requirements shall apply:

1. Streets shall be bordered by sidewalks or multi-use paths on both sides. Exceptions to this requirement may be granted by the TRC for developments in steep slope areas and where warranted by environmental or topographic conditions, or where this requirement would serve no useful purpose.
2. Without exception, sidewalks shall be required along all new public and private streets within new subdivisions or developments, and within new phases of existing subdivisions and any other form of development.
3. Without exception, sidewalks shall be required along existing streets within or abutting new subdivisions and any other form of development (except subdivisions in GR districts establishing four or fewer parcels); or along existing streets within or abutting any form of existing development undergoing substantial improvement (except single-family and duplex residential structures in GR districts). Such infrastructure shall be required along the same side of the street upon which the development fronts, except that when a redevelopment project is located on both sides of the same street the TRC may require that sidewalks be installed on both sides of the street. The Administrator or the TRC may require the dedication of additional street right-of-way or a pedestrian easement when sufficient right-of-way does not exist to comply with this requirement.
4. When site characteristics and/or traffic patterns are such that the construction of sidewalks in accordance with this section would be a hardship and would not result in useful pedestrian walkways, the Administrator, upon recommendation from the TRC may allow the applicant to pay the designated cost of constructing such sidewalks into the City sidewalk fund in lieu of requiring construction of the sidewalks. The Administrator may accept a guarantee for the construction of sidewalks on behalf of the City in situations where no other public infrastructure is proposed in accordance Chapter 16 of this Ordinance.

5. Multi-Use Paths and other infrastructure:

(1) Multi-use paths, and other pedestrian and bicycle infrastructure shall be provided instead of or in addition to sidewalks wherever called for on an adopted plan or policy of the City. Such plans or policies include but are not limited to: *City of Brevard Comprehensive Transportation Plan, City of Brevard Comprehensive Pedestrian Plan, City of Brevard Street Schedule, City of Brevard Land Use Plan, City of Brevard Recreation Plan*, other master plans and small area plans, and other plans and policies.

(2) When a multi-use path is required in an area not adjacent to a public or private street, then such facility shall be credited towards the satisfaction of the open space requirements as set forth in Chapter 7 of this Ordinance.

(3) All required multi-use paths shall be dedicated to the City of Brevard by means of right-of-way or pedestrian easement.

6. On-street bicycle lanes shall be when called for upon an adopted plan or policy of the City.

7. Pedestrian and Bicycle improvements shall be as follows:

Zoning District	Facility Width	
	Neighborhood Street	All Other Street Types*
GR	5 feet	5 feet
RMX, CMX, DMX, GI, IC	5 feet	8 feet
Multi-Use Path (where required)	10 feet	10 feet
On-Street Bike Lane (where required)	5 feet	5 feet

*Sidewalks are not required along Alleys and Commercial Service Streets except when required as a condition of a Planned Development Overlay District, Conditional Zoning District, TND, or Special Use Permit.

8. All sidewalks shall comply with the minimum requirements for handicapped accessibility in compliance with the Americans with Disabilities Act. During the construction of any sidewalk, whether new or replacement, handicapped ramps shall be placed in the sidewalk where it intersects with streets and other pedestrian and vehicular travel ways.

9. All sidewalks shall be made of 4,000 PSI concrete with a minimum depth of 4 inches, except that street and driveway crossings shall be a minimum of 6 inches in depth. Bike lanes and multi-use paths shall be made of asphalt, designed according to the North Carolina Bicycle Planning and Design Guidelines published by the NCDOT and shall include all appropriate signage and pavement markings. Alternative materials and designs may be approved by the TRC in consultation with the Public Works Director.

Section 13.6 Typical Street Design

A. Street designs shall permit the comfortable use of the street by cars, bicyclists, and pedestrians. Pavement widths, design speeds, and the number of vehicle lanes should be minimized without compromising safety. The specific design of any given street must consider the building types which front on the street and the relationship of the street to the City's street network.

B. The precise design of any public or private street within any development shall be determined by the TRC. Street classifications shall be assigned by the TRC in consultation with the Public Works Director and the City Engineer, and constructed in accordance with the typical cross sections shown below, subject to any required modifications to account for actual conditions on the site, utility conditions, transportation plans and policies, and other safety and design considerations.

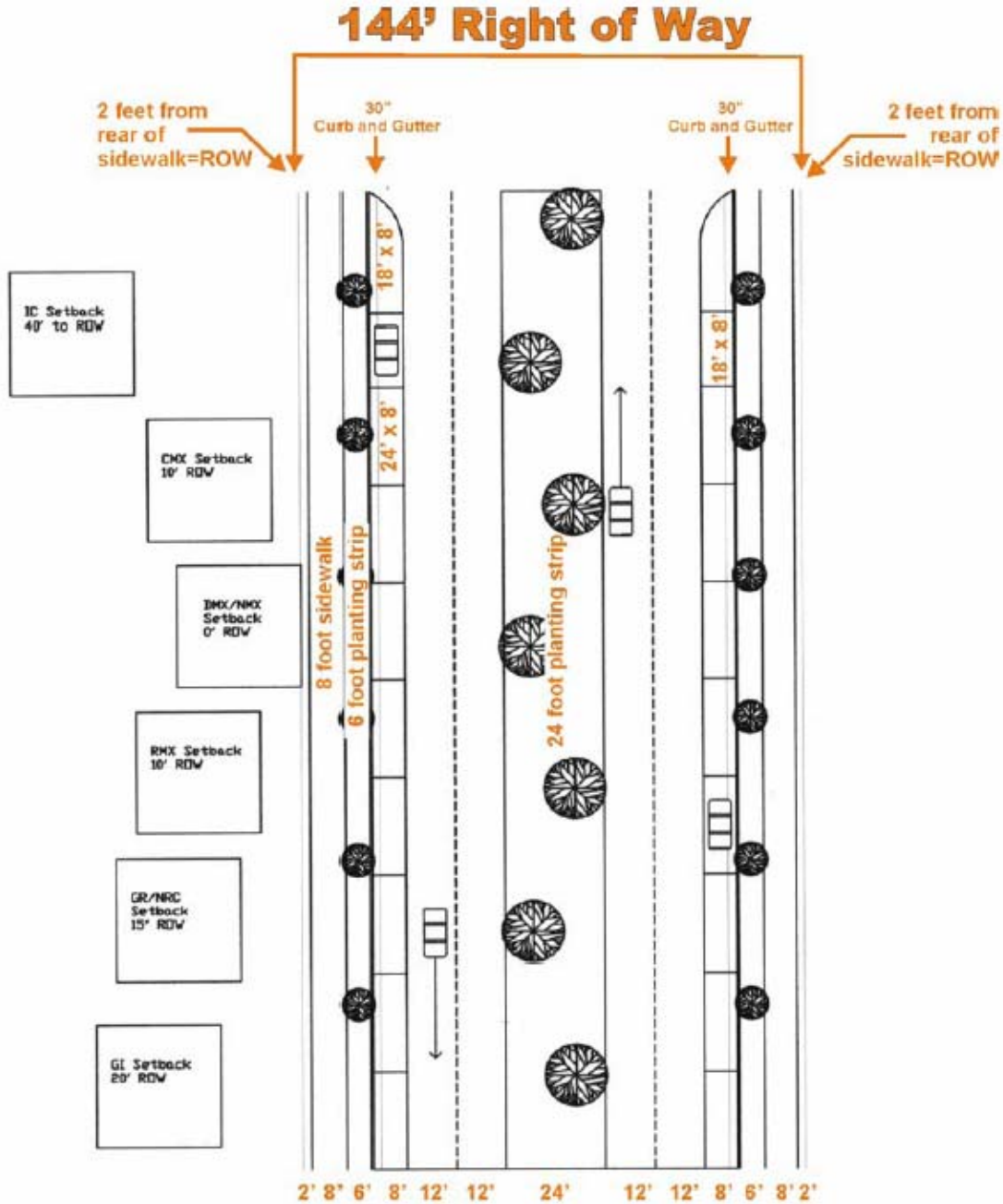
C. Minor variations and exceptions to street cross-sections may be permitted with approval by the TRC. Such exceptions include variations to the pavement width, tree planting areas, street grade, and centerline radii in accordance with principles above.

D. Improvements to streets under the ownership and maintenance of NCDOT shall be subject to the requirements of the NCDOT, in addition to any requirements of the City of Brevard pertaining to sidewalks, bike lanes, and multi-use paths.

E. The street designs shown below are general and intended for planning purposes only as they assume on-street parking and a 5 foot wide sidewalk in all situations. Final right-of-way widths shall be determined by the TRC based upon actual parking requirements, the sidewalk requirements set forth above, other plans and policies of the City, and site considerations.

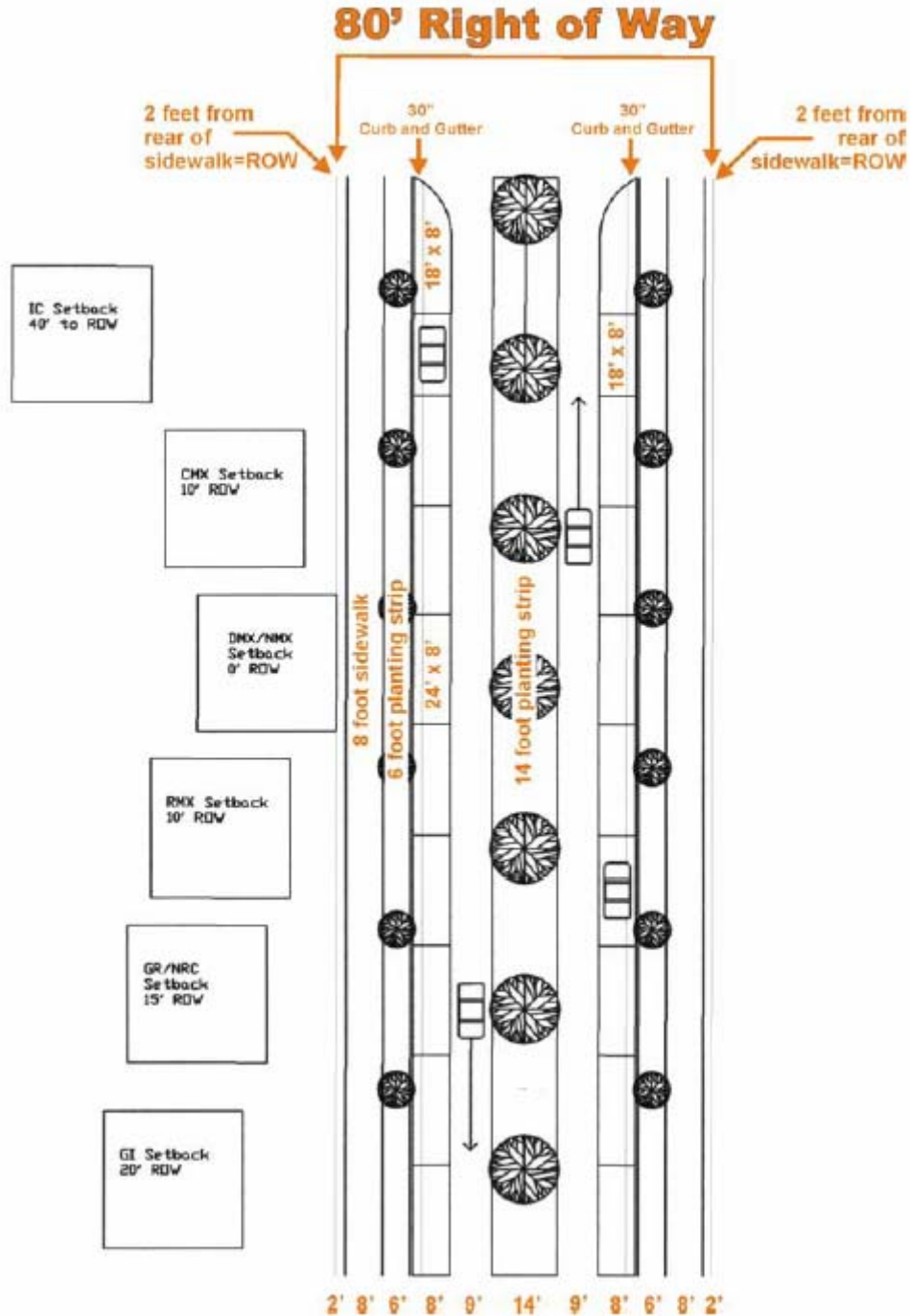
F. Major Boulevard

The boulevard serves as a long-distance, medium-speed vehicular corridor which traverses an urbanized area. It is usually lined by parallel parking, wide sidewalks, or side medians planted with trees. Center medians may be continuously planted or have trees in individual planting areas. Buildings uniformly line the edges.

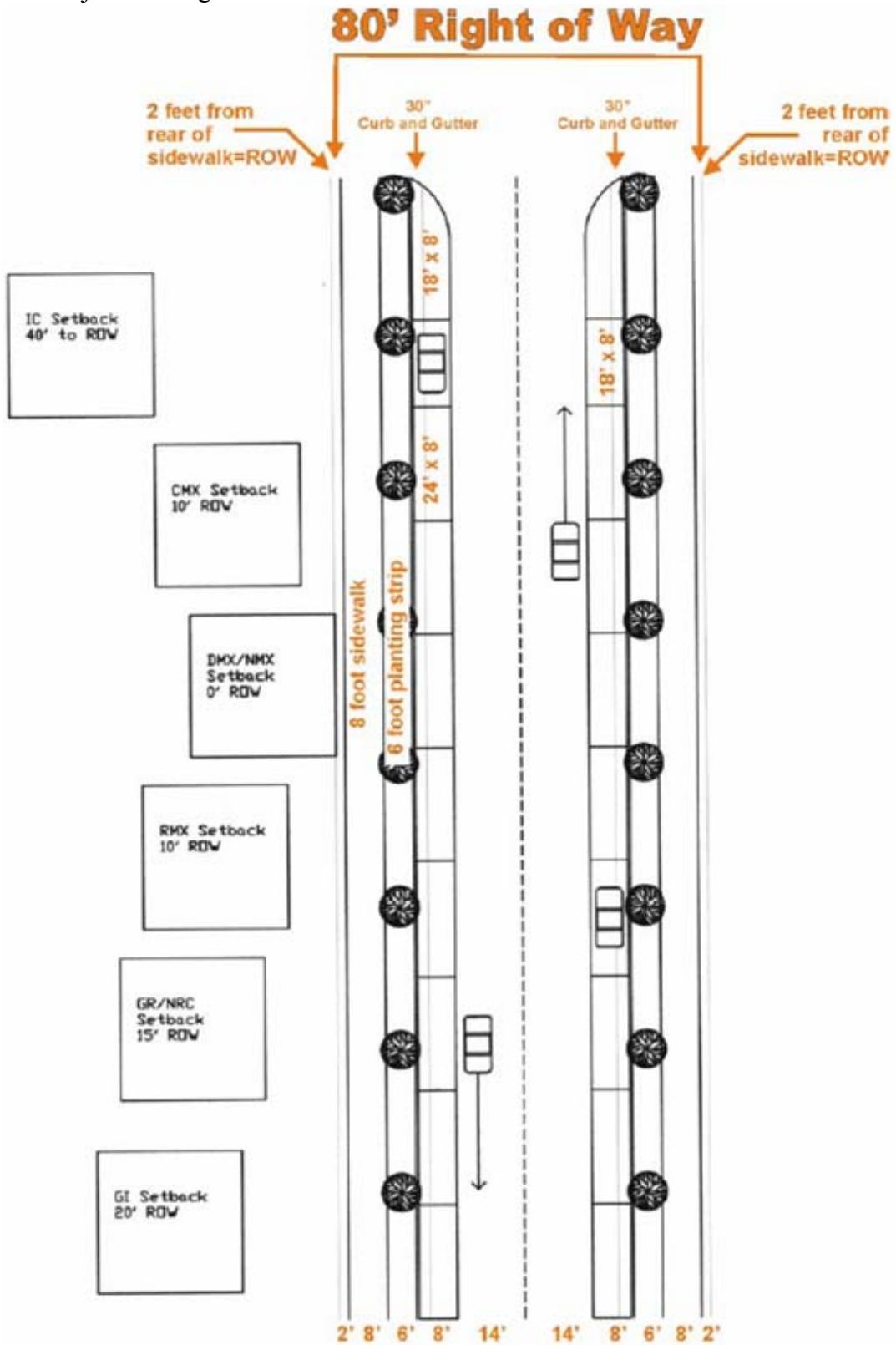


G. Local Boulevard

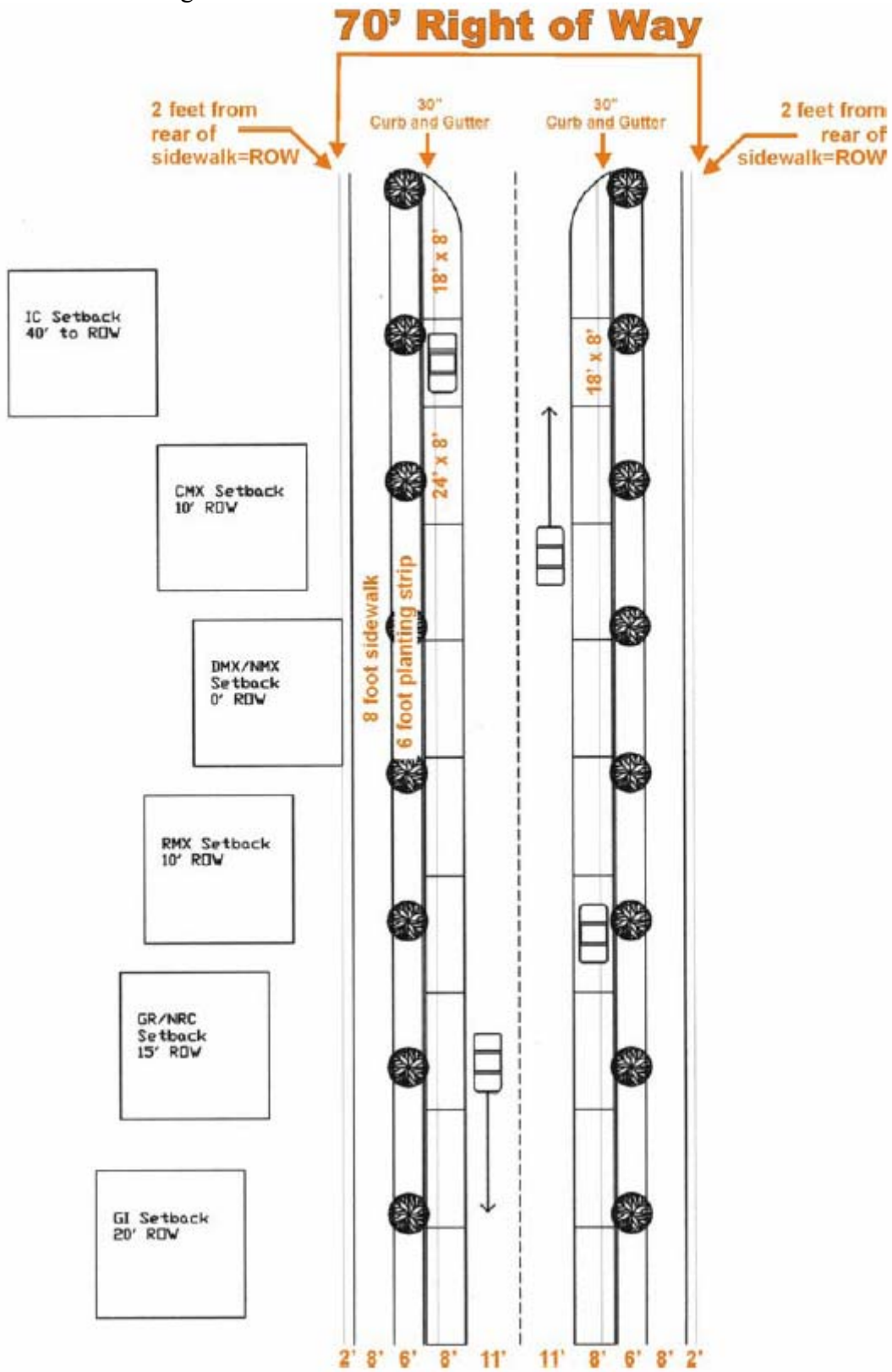
The avenue is a short-distance, medium-speed connector which transverses an urbanized area. It is unlike a Boulevard, in that its axis is terminated by a civic building or monument. The avenue is typified by carefully landscaped edges including three or more individual rows of street trees within the ROW.



H. Major Thoroughfare

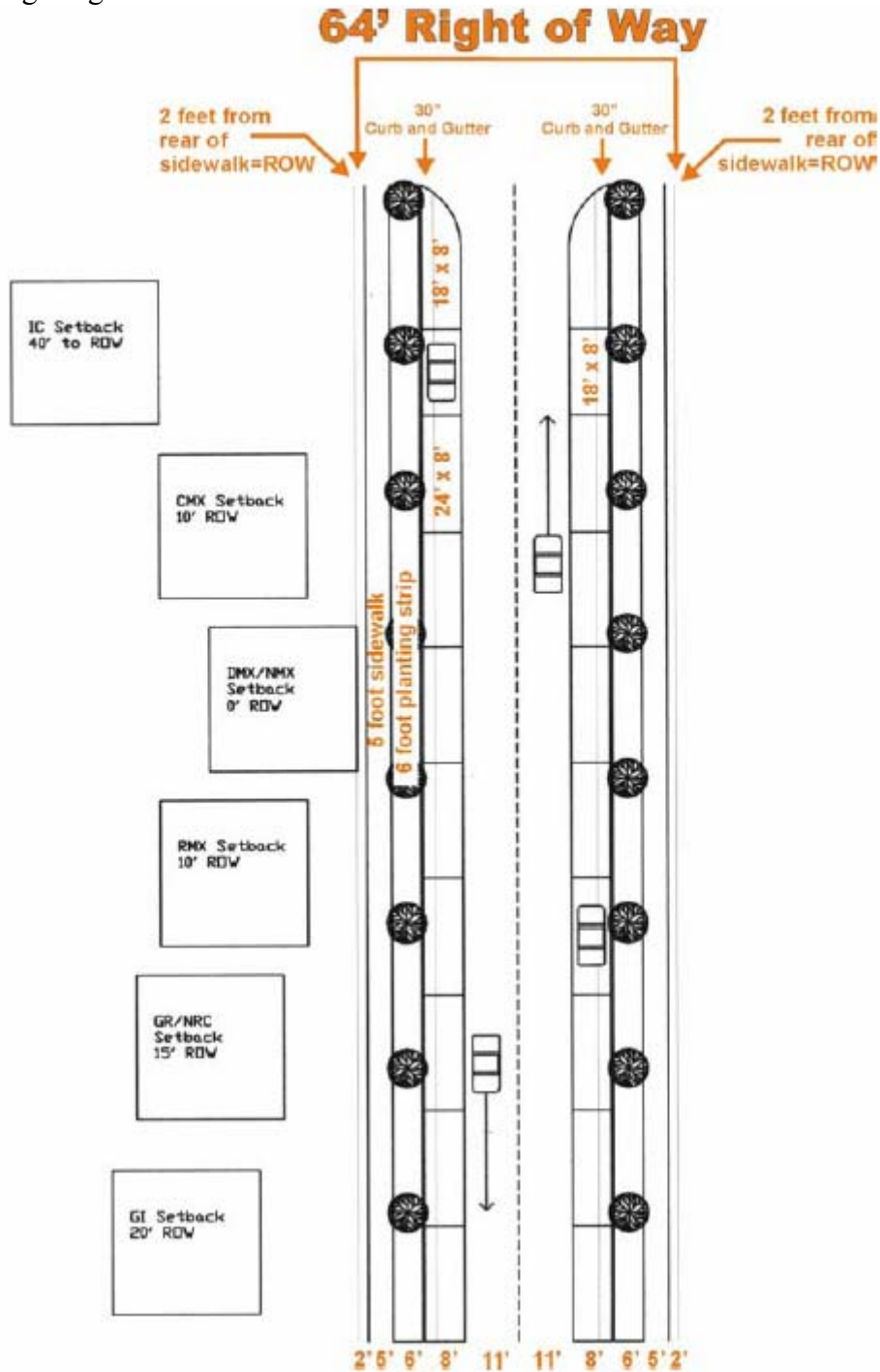


I. Minor Thoroughfare



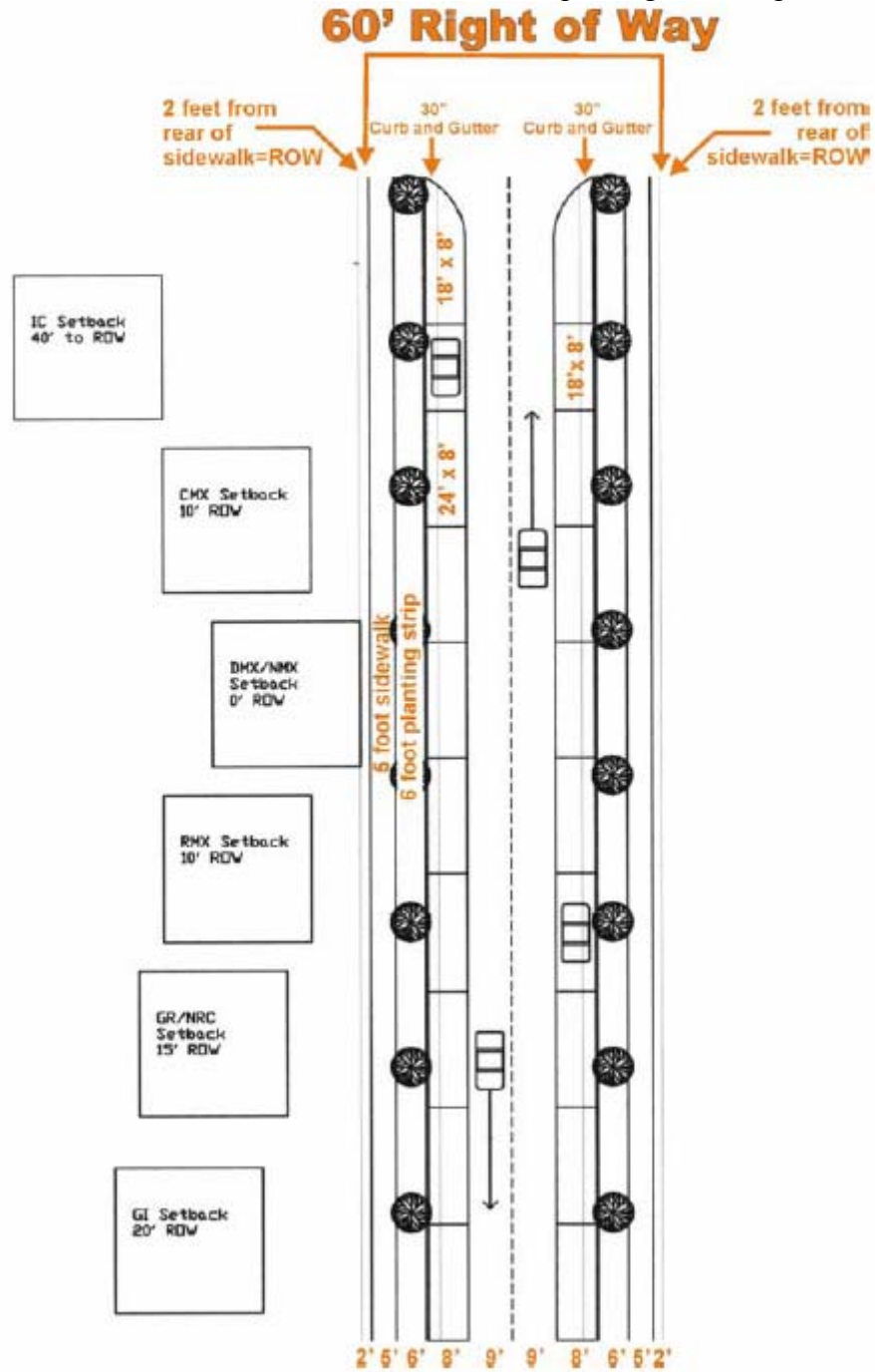
J. Local Collector Street

The local collector street serves as a small-scale, low-speed connector. Local collector streets provide frontage for high-density residential buildings such as apartment buildings and rowhouses. A residential main street is urban in character, with raised curbs, closed drainage, wide sidewalks, parallel parking, trees in individual planting areas, and buildings aligned on short setbacks.



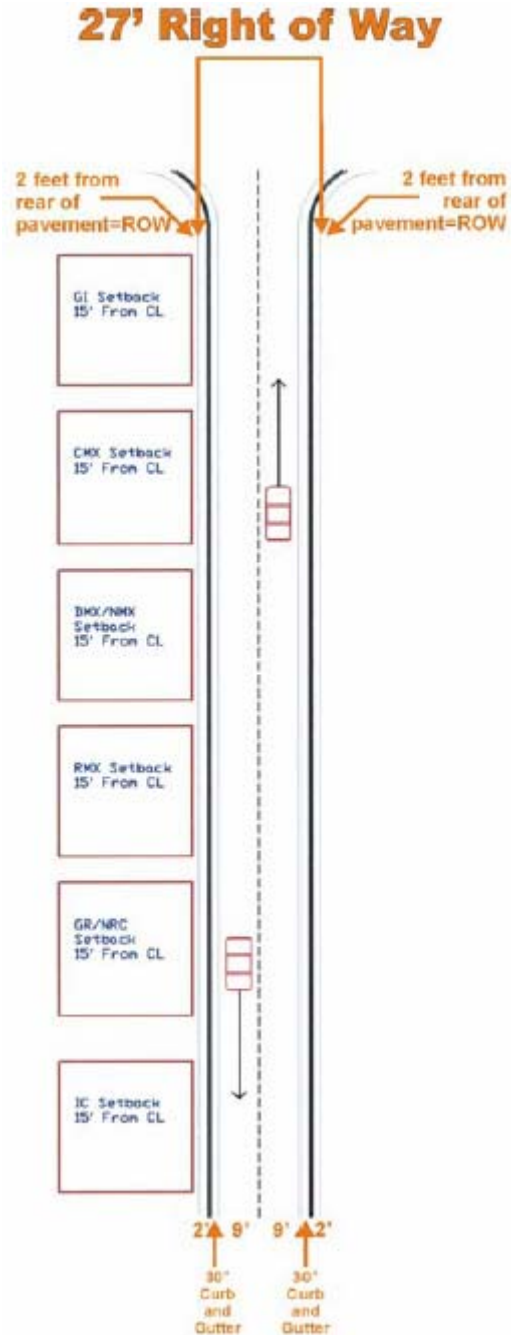
K. Neighborhood Street

The neighborhood street is a small-scale, low-speed connector. It serves low density residential buildings which accommodate all parking on-site. A minor street tends to be more rural in character with rolled curbs, open or closed drainage, narrow sidewalks, continuous plantings, and buildings set way back though this condition may also be appropriate for short urban blocks where on-street parking is not expected



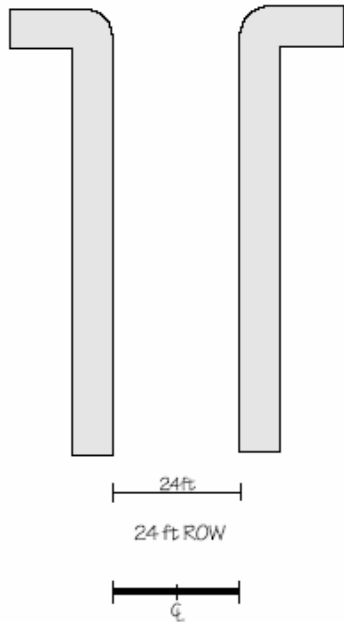
L. Commercial Service Street

The commercial service street is intended to provide service access to the rear or side of non-residential developments. Commercial service streets shall not replace a required street of higher classification. Sidewalks may be required when deemed necessary by the TRC.



M. REAR ALLEY

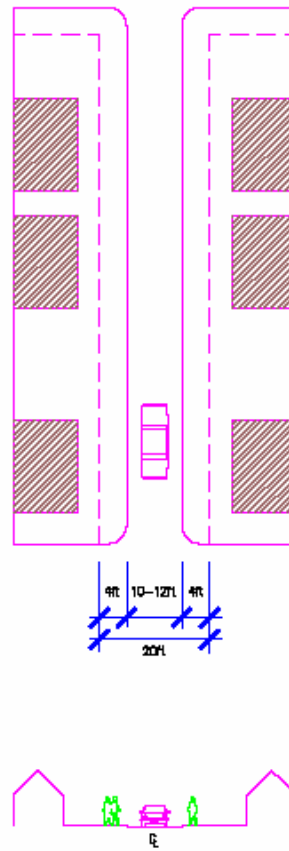
The alley is a narrow access route which services the rear of buildings on a street. Alleys have no sidewalks, landscaping or building setbacks. Alleys are used by trucks and must accommodate dumpsters. They are usually paved to the edges, with center drainage via an inverted crown.



Design Speed	N/A
Min. Centerline Radius	N/A
Pavement Width	24 ft
ROW Width	24 ft
Curb Radius	5 ft
Drainage	Curb & Gutter
On Street Parking	Yes
Street Trees	No
Sidewalks	No

N. J. REAR LANE

The rear lane is a narrow access route behind neighborhood streets. Lanes generally have a narrow strip of paving in the center and serve as areas for underground utilities.



Design Speed	N/A
Min. Centerline Radius	N/A
Pavement Width	10-12 ft
ROW Width	20 ft
Curb Radius	20 ft
Drainage	Open Swale / Closed Inverts
On Street Parking	N/A
Street Trees	No
Sidewalks	No

Section 13.7 Infrastructure requirements in Steep Slope Areas

A. Infrastructure shall be installed in steep slope areas as defined in Chapter 6 in accordance with the provisions of this Chapter. However, TRC and other approving authorities shall have the authority to modify infrastructure requirements and impose such conditions as are necessary to minimize the disturbance caused by the installation of

infrastructure in steep slope areas. Modifications and conditions shall be intended to minimize disturbance and increase the safety and stability of infrastructure, and shall not be granted or imposed to reduce cost to the developer. Modifications and conditions may relate but shall not be limited to pavement width, sidewalk width, curb and gutter design and width, and shoulder design. The maximum road grade requirement shall not be modified.

Section 13.9 (To be renamed Ch. 16, Sec. 16.18) Improvement Guarantees

A. In lieu of meeting the requirement for the completion, installation and dedication of any and all public infrastructure improvements (e.g., underground utilities, streets, sidewalks, storm drainage, trees, landscaping, supplemental buffer plantings, street lights, etc.) prior to final plat approval for subdivisions or Certificate of Occupancy for Site Plans, the TRC (or the Administrator for new sidewalks along existing streets and landscaping) may enter into a written agreement with the developer whereby the developer shall agree to complete all required improvements. Once the agreement is signed by the City Manager and the developer and the required financial security is provided, the final plat or Certificate of Occupancy (only in cases where public infrastructure is not the subject of the guarantee) may be approved by the Administrator if all other requirements of this Code are met.

CHAPTER 14. NONCONFORMITIES

Section 14.3 Nonconforming Principal Structures

E. If a nonconforming structure is significantly damaged or significantly improved, the following requirements shall apply in the issuance of any permit:

5. Significant damage and / or significant improvement to any non-conforming structure that is located within a lot containing any other non-conforming condition shall, to the maximum extent possible, require the satisfaction all other requirements of this Ordinance and the elimination of all non-conforming conditions. Examples include, but are not limited to, the provision of sufficient parking, landscaping and buffering, and public improvements (i.e., sidewalks and other), and full compliance with all applicable signage, flood hazard reduction, surface water protection, and storm water management provisions.

F. If a nonconforming structure is substantially damaged or substantially improved, the structure, all nonconforming uses contained therein, and all non-conforming conditions within the same property (i.e., parking, landscaping, signage, flood hazard issues, and etc.) shall be brought into full compliance with the requirements of this Ordinance.

Section 14.9 Nonconforming Street Frontages

A. A lot (excluding those containing one pre-existing single-family structure), group development, or planned development shall fully comply with all street improvement requirements of Chapter 11 along the fronting streets including, but not limited to turning and traffic storage lanes, sidewalks, multi-use paths, bike lanes, and curb and gutter, if any of the following activities occurs within the same lot or project:

- 1.** An expansion of the heated square footage of an existing primary structure.
- 2.** The construction of a new primary structure.
- 3.** Significant or substantial damage or improvement to a principal structure.
- 4.** A change to a more intensive use of an existing structure.

5. The establishment of additional primary uses.
 6. Approval of any application for the establishment of a Special Use, Conditional Use or Conditional Use District, Group Development, or Planned Development.
 7. Expansions to the parking area or loading areas which increase the total area more than 40 percent shall be required to comply with all applicable parking and loading area landscaping and screening.
- B.** New developments shall be required to comply with all street improvement requirements of Chapter 13 along the fronting streets including, but not limited to turning and storage lanes, access management, sidewalks, bike lanes, and curb and gutter.

CHAPTER 16. ADMINISTRATION

Section 16.1 Purpose and Intent

In order to establish an orderly process by which to develop land within the jurisdiction of the City of Brevard consistent with standard development practices and terminology it is the purpose of this Chapter to provide a clear and comprehensible development process that is fair and equitable to all interests including the petitioners, affected neighbors, City staff and related agencies, the City of Brevard Planning Board (hereafter, “BPB”), and the City Council. The intent of this Chapter is as follows:

- C.** To provide for the adequate and efficient provision of facilities and/or infrastructure, and the dedication of land, rights-of-way, and easements, so as not to burden the fiscal resources of the City. This includes the construction of buildings and utilities, streets and sidewalks, landscaping, recreational open spaces, and other provisions required for the public good of the City of Brevard.

Section 16.9. Planned Development Overlay Districts, Traditional Neighborhood Overlay Districts

A. Planned Development Overlay District (PD)

1. General Requirements

- b.** Use, density and dimensional (setbacks, building size, building height, building design) requirements may be modified; all requirements of this Ordinance [i.e. signage, streets, curb / gutter, sidewalks, natural resources, others] shall in all cases be adhered to and shall not be modified

B. Traditional Neighborhood Development Overlay District (TND): A Traditional Neighborhood Development Overlay District is a floating overlay district. The intent of this District is to allow for the development of fully integrated, mixed-use, pedestrian-oriented neighborhoods that minimize traffic congestion, suburban sprawl, infrastructure costs, and environmental degradation. Traditional Neighborhood Development Overlay Districts shall adhere to the design principles and requirements set forth in Chapter 2

Section 16.12 – Appeals and Variances

D. Variance Procedure

- 3.** The following are not cause for a variance:
- f.** To provide relief from any requirement of this Ordinance pertaining to the following: streets, sidewalks, sewer, water (Chapter 13); steep slope areas, stormwater management, sedimentation and erosion prevention, or any requirement pertaining to the regulatory floodway or Special Flood Hazard Area (Chapter 6). Variances pertaining to encroachments into Surface Water Protection

Areas other than that portion lying in the regulatory floodway (Chapter 6) may be considered. In no case shall a variance pertaining to a Surface Water Protection Area be issued for any purpose other than to facilitate the placement of a principal structure, and such variances shall only be considered after all opportunities for the issuance of other dimensional variances that would not cause an encroachment into the Surface Water Protection Area have been fully exhausted. No other variance, including but not limited to the clearance of vegetation for aesthetic purposes or the placement of accessory structures, shall be considered.

CHAPTER 17. DEVELOPMENT PLAN REQUIREMENTS

Section 17.6 – Construction Document Requirements

L. The location and size of all utility lines, easements, and rights-of-way. Easements shall be provided on all Construction Documents as follows:

4. Public Access Easements: Public Access Easements shall be provided for sidewalks, trails, greenways, and other pedestrian and bicycle facilities that provide connections other than within public rights-of-way.

Q. The location and dimensions of any sidewalks, curb cuts, curb and gutters to be installed along public street frontages, and other required street improvements designated in Chapter 11 of this Ordinance, or as called for in the Brevard Transportation Plan, the Rural Planning Organization Thoroughfare Plan or Transportation Improvement Program, or other plan or policy of the City. Required right-of-way shall be drawn in the location shown on any official plan at the width specified in this Ordinance.

Section 17.12 – Traffic Impact Study (TIS)

E. Contents: The Rezoning Traffic Analysis or the Traffic Impact Report shall contain the following:

10. Report Findings: If a proposed development does not meet the applicable service level standards, one or more of the following actions may be necessary:

x. Integrate non-vehicular design components (i.e. pedestrian and bicycle and transit improvements) to reduce trip generation;

CITY OF BREVARD PLANNING DEPARTMENT SUMMARY OF EXISTING SIDEWALK REGULATIONS

PART I. CHARTER

Article V. Public Improvements

Sec. 5.1. Assessments for street and sidewalk improvements; petition unnecessary.

(a) In addition to any authority which is now or hereafter may be granted by general law to the City for making street improvements, the City Council is hereby authorized to make street improvements and to assess the cost thereof against abutting property owners in accordance with the provisions of this section.

(b) The City Council may order street improvements and assess the total cost thereof against the abutting property owners, exclusive of the cost incurred at street intersections, according to one or more of the assessment bases set forth in G.S. 160A-216 et seq., without the necessity of a petition, upon the finding by the Council as a fact:

(1) That the street improvement project does not exceed 2,000 linear feet; and
(2) That such street or part thereof is unsafe for vehicular traffic, and it is in the public interest to make such improvements; or

(3) That it is in the public interest to connect two streets, or portions of a street already improved; or

(4) That it is in the public interest to widen a street, or part thereof, which is already improved, provided that assessments for widening any street or portion of street without a petition shall be limited to the cost of widening and otherwise improving such streets in accordance with the street classification and improvement standards established by the City's thoroughfare or major street plan for the particular street or part thereof to be widened and improved under the authority granted by this Article.

(c) For the purpose of this Article, the term "street improvement" shall include grading, regrading, surfacing, resurfacing, widening, paving, repaving, the acquisition of right-of-way, and the construction or reconstruction of curbs, gutters and street drainage facilities.

(d) In addition to any authority which is now or may hereafter be granted by general law to the City for making sidewalk improvements, the Council is hereby authorized without the necessity of a petition, to make or to order to be made sidewalk improvements or repairs according to standards and specifications of the City, and to assess the total cost thereof against abutting property owners, according to one or more of the assessment bases set forth in G.S. 160A-216 et seq.; provided however, that regardless of the assessment basis or bases employed, the City Council may order the cost of sidewalk improvements made only on one side of a street to be assessed against property owners abutting both sides of such street.

(e) In ordering street and sidewalk improvements without a petition and assessing the cost thereof under authority of this Article, the Council shall comply with the procedure provided by G.S. 160A-216 et seq., except those provisions relating to the petition of property owners and the sufficiency thereof.

(f) The effect of the act of levying assessments under the authority of this Article shall for all purposes be the same as if the assessments were levied under authority of G.S. 160A-216 et seq.

PART II. CODE OF ORDINANCES
Chapter 1. General Provisions

Sec. 1-2. Definitions and rules of construction.

Sidewalk The word "sidewalk" means any portion of a street between the curb line and the adjacent property line intended for the use of pedestrians.

Chapter 2. Administration

Article V. Finance and Taxation

Division 3. Construction and Financing of Public Improvements

Subdivision I. In General

Sec. 2-275. Minimum standards for improvements.

(a) *Right-of-way width.* No public improvement shall hereafter be made on any street right-of-way less than 30 feet in width or on any other right-of-way of less than 20 feet in width.

(b) *Street width.* See sections 62-63 and 62-64.

(c) *Sidewalks.* The minimum sidewalk width shall be five feet, and handicapped ramps shall be provided during the construction of sidewalks where the sidewalk intersects at streets and roads unless it is determined by the city public works director that such a ramp is impractical.

Chapter 30. Fire Prevention and Protection

Sec. 30-2. Burning trash or other material within fire district or on street or sidewalk.

No person shall burn or cause to be burned any trash, refuse, shavings, paper, leaves, litter or other material of any kind outside of any building, within the fire district, or on or in any street, sidewalk, alley or other public way in the city.

Chapter 38. Health and Sanitation

Article II. Nuisances

Division 1. Generally

Sec. 38-31. Prohibited.

It shall be unlawful for any person to cause, permit, maintain or allow the creation or maintenance of a nuisance.

Sec. 38-32. Enumeration.

The maintaining, using, placing, depositing, leaving or permitting to be or remain on any public or private property of any of the following items, conditions or actions are hereby declared to be and constitute a nuisance; provided, however, this enumeration shall not be deemed or construed to be exclusive, limiting or restrictive:

(7) Any improper or inadequate drainage on private property which causes flooding or interferes with the use of or endangers in any way the streets, sidewalks, parks or other city-owned property of any kind; provided, the notices required and powers conferred by this article by and on the inspection department in abating the nuisances defined by this subsection shall be given and exercised by the director of public works.

Sec. 38-45. Expectorating.

It shall be unlawful for any person to willfully expectorate or spit on any paved sidewalk leading to any public school building, courthouse or other public building, or any bank, hotel, store or other building open to the public, or on the steps, floors or walls thereof, or on the floor of any bank, courthouse, post office, hotel corridor or hotel office.

(Code 1980, § 8-34)

Division 3. Cleanliness of Premises; Littering

Sec. 38-92. Littering.

(a) It shall be unlawful for any person to cast, place, sweep or deposit anywhere within the city any refuse on any public property or on the property of another, without authority, or in such a manner that it may be carried or deposited by the elements upon any street, sidewalk, alley, sewer, parkway or other public place, or onto any premises of another, within the city.

(b) It shall be unlawful for any owner or keeper of any dog to allow fecal matter excreted by or from said animal to be placed or deposited upon any street, sidewalk or public way within the city or upon the property of another without authority. It shall also be unlawful to allow such matters to be placed or deposited in such a manner that it may be carried or deposited by the elements upon any street, sidewalk, alley, parkway or other public place or onto the premises of another. Provided, however, this section is only applicable in the areas from and including England Street on the West to Rice Street on the East, Probart Street on the North to Morgan Street on the South, public parks and recreational facilities and public school property. Appropriate signage shall be placed on city owned facilities.

Chapter 42. Offenses and Miscellaneous Provisions

Article I. In General

Sec. 42-3. Consumption or possession of alcoholic beverages in public place.

It shall be unlawful for any person to consume, serve or drink wine, beer, whiskey or alcoholic beverages of any kind, or to publicly display any of these alcoholic beverages if the container has been opened or the seal broken, on any of the following properties:

(1) Any public street, alley, sidewalk or parking lot within the city limits, whether the area is open to vehicular traffic or has been closed to vehicular traffic in connection with a parade or other special event.

(2) Any property owned, leased or controlled by the city.

Sec. 42-5. Trespass on posted property; loitering generally.

(a) It shall be unlawful for any person to loiter in or upon, or to drive any vehicle upon, any private property on any day between the hours of 12:00 midnight and 6:00 a.m. when signs have been posted by the owner or his employee or agent prohibiting such acts.

(b) It shall be unlawful for any person to loiter in or upon, or to drive any vehicle upon, any private business property parking lot or area at any time such business is not open to the public, when signs have been posted by the owner or his employee or agent stating that such loitering or driving of vehicles is prohibited at or during certain designated times.

(c) It shall be unlawful for any person to habitually loaf or loiter on the streets, or to congregate on the streets or sidewalks of the city in such a way as to obstruct or interfere with the free passage into or out of any public business or private property.

(d) It shall be the duty of the police officer who observes a violation of this section first to warn the offender, and upon his failure to heed the warning to arrest him.

Sec. 42-6. Loitering for purpose of engaging in drug-related activity.

(a) *"Public place" defined.* For purposes of this section, the term "public place" means any street, sidewalk, bridge, alley or alleyway, plaza, park, driveway, parking lot or transportation facility, or the doorways and entranceways to any building which fronts on any of those places, or a motor vehicle in or on any of those places, or any property owned by the city or by any public agency or public authority and located within the city.

(b) *"Known unlawful drug user, possessor or seller" defined.* For purposes of this section, the term "known unlawful drug user, possessor or seller" means a person who has, within the knowledge of the arresting officer or complaining witness, been convicted in any court within this state of any violation involving the use, possession or sale of any of the controlled substances or drug paraphernalia referred to in the North Carolina Controlled Substances Act, G.S. 90-89 et seq., or has been convicted of any violation of any substantially similar laws of any political subdivision of this state or of any other state, or of federal law.

(c) *Violations.* It shall be unlawful for a person to loiter, remain or wander about in a public place in a manner and under circumstances manifesting the purpose of engaging in a violation of any subdivision of the North Carolina Controlled Substances Act, G.S. 90-89 et seq. A prima facie showing of a violation of this section is made by proof that a person, while in a public place, loiters and commits any of the following acts or courses of conduct:

- (1) Repeatedly beckons to, stops or attempts to stop passersby, or repeatedly attempts to engage passersby in conversation;
- (2) Repeatedly stops or attempts to stop motor vehicles;
- (3) Repeatedly interferes with the free passage of motor vehicles or other persons;
- (4) Associates at such location with one or more persons who are known unlawful drug users, possessors and sellers;
- (5) Behaves in such a manner as to raise a reasonable suspicion that he is about to engage in, or is engaged in, an unlawful drug-related activity;
- (6) Repeatedly passes to or receives from passersby, whether on foot or in a vehicle, money or objects;
- (7) Takes flight upon the approach or appearance of a police officer;
- (8) Is at a location frequented by persons who use, possess or sell drugs; or
- (9) Uses or associates with the user of any vehicle which is registered to a known unlawful drug user, possessor or seller, or is known to be or have been involved in drug-related activities.

(d) *Penalty.* A violation of any provision of this section is a misdemeanor and shall subject the offender to punishment in accordance with section 1-8.

Sec. 42-7. Assemblages obstructing street.

It shall be unlawful for persons to assemble or collect and stand so as to obstruct any sidewalk, street or other public way, except an authorized parade conducted in accordance with section 66-7. All persons so collecting and standing shall disperse and move upon the demand of any police officer.

Sec. 42-11. Damaging streetlights, signs or other fixtures on streets or sidewalks.

It shall be unlawful for any person to damage, tamper with, remove or paint upon or deface any sign, sign post, streetlight, traffic signal or bulletin board or other municipal property upon the streets and sidewalks, except employees of the city in performance of their official duties.

(Code 1980, § 10-21)

Chapter 46. Peddlers and Solicitors

Article I. In General

Sec. 46-1. Selling on streets and sidewalks.

(a) *Permission for sidewalk merchandise.* Merchants lawfully conducting and operating a business from permanent physical business facilities which front on and abut a city sidewalk may display and sell goods, wares and merchandise from the sidewalk without need of application and approval from the city council; provided, however, that such merchandise, along with all racks, display cases and other accessories thereto, shall be removed from all public sidewalks at the end of every business day during which the same may have been displayed.

(b) *Setback.* On sidewalks more than 12 feet in width, excluding the curb, the maximum distance that the goods, wares or merchandise may be placed from the wall or front of the place of business shall be 36 inches. On sidewalks at least seven feet in width, excluding the curb, the maximum distance shall be 24 inches. On sidewalks less than seven feet in width, no goods, wares or merchandise shall be placed on the sidewalks or streets.

(c) *Additional restrictions.* Notwithstanding the other provisions of this section, authority for merchants to sell from city sidewalks does not extend to city streets, and is limited to those sidewalk areas immediately abutting their own property. Further, the setback requirements of subsection (b) of this section shall apply to all obstructions of every description.

(d) *Special events.* The city manager may waive the setback requirements for special events recognized by the city, such as the Brevard/Transylvania Festival of the Arts, but in no event shall any merchant be permitted to fully obstruct a city sidewalk.

(e) *Sidewalk dining areas.* Within the allowance made for merchandise on sidewalks as specified by this section, it is permissible for eating establishments to place tables and chairs on sidewalks adjacent to the wall which fronts their business and to the wall on the side of the business if located on a street corner. No portion of any table or chair may protrude beyond the allocated space on the sidewalk as specified in subsection (b) of this section. For safety purposes, tables and chairs may not obstruct access to building entrances and exits. Merchants with sidewalks dining are responsible for cleanliness and are subject to all applicable health department regulations. Umbrellas over tables are permissible but must also provide a clearance of a minimum of 78 inches from the lowest point of their canopy. Advertising messages may not be displayed from tables, chairs or umbrellas.

(f) *Placement of permanent and semi-permanent obstructions.* It shall be unlawful to place permanent and semi-permanent displays, fixtures, inventory racks, display cases, vending machines and other obstructions of a permanent or semi-permanent nature on city-maintained sidewalks, streets, or alleyways in the downtown area, as defined by section 46-32; provided, however, that this restriction will not prohibit placement of public telephones and newspaper display racks within the setback limitations imposed by subsection (b); and provided further that all such items in place as of July 7, 1997, and which would have been authorized by the former subsection 46-1(d) or (e) shall be allowed to remain in place until such time as the same no

longer operates or until such time as replacement is warranted due to a deterioration of physical condition or appearance, whichever might first occur.

(g) *Selling from streets and vehicles.* It shall be unlawful for any vendor, as defined in section 46-32, or for any person whatsoever, to sell, barter, exchange or attempt to sell, barter, or exchange any goods, wares or merchandise from any city street or from any vehicle except:

(1) As may be authorized with respect to vending carts in article II of this chapter; or

(2) As the prohibition may be waived by the city manager for special events recognized by the city, such as parades and the Brevard/Transylvania Festival of the Arts, provided that the applicant satisfies all permit requirements of section 66-7 and complies with article I of chapter 66 and other applicable provisions of this Code.

(Code 1980, § 12-1; Ord. No. 16-97, § 1, 7-7-97)

Sec. 46-2. Crying out or using noisemaking device for advertising.

No person shall advertise any article of any kind or any event of any kind by crying out the advertisement or by using therefore any megaphone, bell, horn or other noisemaking device on any street, sidewalk or other public way of the city without a permit issued by the city.

Article II. Vending Carts in Downtown Area

Sec. 46-31. Purpose of article.

It is found and declared that sidewalk vendors promote the public interest by:

(1) Making the downtown area an active and attractive pedestrian environment.

(2) Providing the opportunity for creative, colorful pedestrian-focused commercial activities on a day/night, year-round and seasonal basis.

(3) Encouraging commercial activities which add excitement, charm, vitality and diversity to the downtown area.

(4) Encouraging development within the downtown area.

(5) Promoting land conservation, redevelopment, energy saving, and direct and indirect tax revenue.

(Code 1980, § 12-20)

Sec. 46-32. Definitions.

The following words, terms and phrases, when used in this article, shall have the meanings ascribed to them in this section, except where the context clearly indicates a different meaning:

Downtown area means any of the following streets and sidewalks of the city, including portions of streets and sidewalks marking the boundary lines of such territory: beginning on the south side at Varsity and South Broad Streets, on the east side at Franklin and East Main Streets, on the west side at Oaklawn and West Main Streets, and ending on the north side at North Caldwell and North Broad Streets.

Pushcart means any self-contained rubber-wheeled vehicle used for displaying, keeping or storing any article by a vendor or peddler (other than a motor vehicle, bicycle or trailer) which may be moved without the assistance of a motor and does not require registration by the state department of motor vehicles.

Vendor means a person who hawks, peddles, sells or offers prepared food, fresh flowers or washed fresh fruit or vegetables, and approved specialty items.

Sec. 46-33. Permit required.

It shall be unlawful to sell, or offer for sale, any food, beverage or merchandise on foot or from any pushcart or mobile food unit on any right-of-way, street, sidewalk or public park within the

city without first obtaining a permit. To relieve any potential for traffic hazard or pedestrian congestion, or any safety hazard resulting therefrom, the number of street vendors with active permits at any given time shall not exceed four.

(Code 1980, § 12-22)

Sec. 46-34. Application for permit; permit fee.

The permit required by section 46-33 shall be issued only after the issuance of a city privilege license in accordance with this Code. The application shall be accompanied by payment of a permit fee of \$50.00 for each pushcart or mobile food unit for which the permit is sought. The application for a pushcart or mobile food unit permit shall include:

(1) The name, home and business address of the applicant, and the name and address of the owner of the vending business or of the cart to be used in the operation of the vending business.

(2) A description of the type of food, beverage or merchandise to be sold.

(3) A description of the proposed location and the recommendation by the Heart of Brevard. There shall not be subletting of locations.

(4) A description (including the size) and a photograph of any pushcart to be used in the operation of the business, including the license and registration number of any motor vehicle used in the operation of the business.

(5) Two prints of a full-face photograph, taken not more than 30 days prior to the date of the application, of any person who will sell or offer for sale any food, beverage or merchandise on any street or sidewalk within the city.

(6) A copy of any approval required by the county health department pursuant to the Rules Governing the Sanitation of Restaurants and other Foodhandling Establishments, 10 N.C.A.C., chapter 10, subchapter 10A.

(7) Proof of an insurance policy, issued by an insurance company licensed to do business in the state, protecting the permittee and the city from all claims for damages to property and bodily injury, including death, which may arise from operations under or in connection with the permit. Such insurance shall name the city as additional insured and shall provide that the policy shall not terminate or be canceled prior to the expiration date without 30 days' advance written notice to the city. Such insurance shall afford minimum limits of \$1,000,000.00 per person bodily injury, \$300,000.00 per occurrence bodily injury, and \$25,000.00 per occurrence property damage.

Sec. 46-35. Issuance of permit; term; renewal.

(a) Not later than 30 days after the filing of a completed application for a vendor's permit, the applicant shall be notified by the city clerk of the decision on the issuance or denial of the permit.

(b) The city council shall consider the standards set forth in this article in determining whether to grant a permit. If the issuance of the permit is approved, the city clerk shall issue the permit. If the permit is denied, the applicant shall be provided with a statement of the reasons therefore, which reasons shall be entered in writing on the application. A permit issued pursuant to this section is valid for a period of one year, from July 1 to June 30. Renewal of the permit by the city council shall be contingent upon the vendor's continued adherence to the established standards, with consideration given to the recommendation of the Heart of Brevard.

Sec. 46-36. Permitted locations for pushcarts.

Vending of foods, beverages and merchandise from pushcarts is permitted only on any sidewalk in the downtown area.

Sec. 46-37. Prohibited conduct.

(a) No vendor shall:

- (1) Vend within 300 feet of any church, while such church is holding a religious service.
- (2) Vend on any street or sidewalk where vending is otherwise prohibited.
- (3) Vend between 9:00 p.m. and 7:00 a.m. of the following day, except during city-approved festivals and events.
- (4) Leave any pushcart unattended.
- (5) Store, park or leave any pushcart overnight on any street or sidewalk.
- (6) Sell food or beverages for immediate consumption unless he has available for public use his own or a public litter receptacle which is available for his patrons' use and no more than ten feet distant from his pushcart or mobile food unit.
- (7) Leave the designated location without first picking up, removing and disposing of all trash or refuse remaining from sales made.
- (8) Allow any items relating to the operation of the vending business to be placed anywhere other than in, on or under the pushcart.
- (9) Set up, maintain or permit the use of any table, crate, carton, rack or other device to increase the selling or display capacity of his pushcart or where such items have not been described in his application.
- (10) Solicit or conduct business with persons in motor vehicles.
- (11) Sell anything other than that which he is licensed to vend.
- (12) Sound or permit the sounding of any device which produces noise, or use or operate any sound system, radio, sound amplifier or speaker to attract the attention of the public.
- (13) Vend without the insurance coverage specified in this article.

(b) No vendor selling from a pushcart on the sidewalk shall:

- (1) Vend within ten feet of any building.
- (2) Vend within 75 feet of any other vending cart.
- (3) Vend within 50 feet of any driveway entrance to a police or fire station, or within ten feet of any other driveway or of any alley.
- (4) Vend within ten feet of the crosswalk at any intersection.
- (5) Vend within ten feet of any fire hydrant or fire escape.
- (6) Allow the pushcart or any other item relating to the operation of the vending business to lean against or hang from any building or other structure lawfully placed on public property, without the owner's permission.

(c) It shall be unlawful for any person to maintain any pushcart upon any street or sidewalk which impedes, endangers or interferes with the travel upon or use of the street or sidewalk.

(d) If it becomes necessary for the regulation of traffic or the safety or convenience of pedestrians, any law officer of the city may direct vendors to move to another location. No person may refuse to comply with a law enforcement officer when the order is given under the authority of this section.

Sec. 46-38. Size of pushcarts.

No pushcart shall exceed 48 inches in width or 72 inches in length, unless approved by the Heart of Brevard. No pushcart shall exceed 60 inches in height, nor shall any canopy be less than 78 inches in height at its lowest point.

Sec. 46-39. Identification badges.

Each pushcart vendor shall display on the upper right chest a vendor's identification badge on which shall appear the vendor's name, photograph and vending space number. The Heart of Brevard shall issue the badge upon presentation of the city permit issued to the vendor or his employer and the payment of a fee of \$25.00. The badge shall be valid during the permit period and must be renewed annually.

(Code 1980, § 12-28)

Sec. 46-40. Display of badges and permits.

All permits and badges issued under this article shall be displayed at all times during the operation of the vending business.

Sec. 46-41. Operation during special events.

Vendor permits will be invalid during special downtown festivals and events in the downtown area. Vendors will need to apply for permits from the sponsoring organization or committee.

Sec. 46-42. Denial, suspension or revocation of permit.

Any vendor's permit may be denied, suspended or revoked by the city for fraud or misrepresentation in the application for the permit or in the conduct of the business, for conduct of the business in such a manner as to create a public nuisance or constitute a danger to the public health, safety, welfare or morals, or for conduct which is contrary to the provisions of this article.

Sec. 46-43. Penalty.

Any person violating any provision of this article shall be guilty of a misdemeanor, and upon conviction shall be punished in accordance with section 1-8.

Appendix A. Zoning Ordinance

Article VI. General Provisions

622. Regulation of awnings.

All awnings within the C-1 central business district shall be supported by means of a frame attached directly to the structure receiving beneficial use of the awning. In no case shall awnings be supported by a frame attached to a sidewalk or other public right-of-way. All awnings in other zoning districts shall be considered a part of the structure for purposes of measuring and complying with area and setback regulations.

630. Sidewalk requirements.

Notwithstanding any other provisions of this ordinance, it is the intent of this section that sidewalks shall be provided for non-residential construction in areas on at least one (1) side of every street. Sidewalks shall be constructed from property line to property line within the street right-of-way, or, in the alternative, within areas set aside by dedication, condemnation or otherwise, in accordance with the city's sidewalks and driveway entrance standards. In the case of corner lots, sidewalks shall be provided along both streets.

630.1 Sidewalk shall be required for non-residential construction in areas zoned, O & I, C-2, C-4, I-1 and I-2 when one of the following conditions is present:

When the property adjoins property with an existing sidewalk.

When an existing sidewalk is within 400 feet of the property on the same side of the street.

When the property is within one-half mile of a school, hospital, library or government building.

630.11 Notwithstanding the foregoing, however, within the city's area of extraterritorial jurisdiction sidewalks shall not be required.

630.12 In cases where a sidewalk already exists on the opposite side of the street, having three lanes or less, and no sidewalk exists along the property adjacent to the subject property, then a sidewalk is not required to be built along the frontage of the subject property.

630.13 Furthermore, if the new construction is intended for a commercial, industrial, or institutional use, sidewalks shall be required to connect street sidewalks with any entrances intended for public use.

630.2 *Relocation of sidewalks.* When site characteristics make the placement of a sidewalk along the street frontage of a property impractical resulting in an unnecessary hardship, the sidewalk may be relocated to the opposite side of the street upon approval of the city manager.

630.21 The city manager may require that a sidewalk be built across the street from the site of new non-residential construction when all of the following conditions exist:

- * No sidewalk adjoins the site,
- * A sidewalk exists on the other side of the street in the vicinity of the site, and
- * There is no legal impediment to constructing the sidewalk on the other side of the street.

630.3 *Payment of fee in lieu of construction of sidewalks.* When site characteristics and/or traffic patterns are such that the construction of sidewalks in accordance with this section would be a hardship and would not result in useful pedestrian walkways, the city manager may allow the applicant to pay the designated cost of constructing such sidewalks into the City sidewalk fund in lieu of requiring construction of the sidewalks.

Article IX. Off-Street Parking and Loading Regulations

901. Off-street loading regulations.

901.2. *Loading spaces adjacent to sidewalks.* Where a loading space is adjacent to a public sidewalk or other public pedestrian way, it shall be so located, arranged and improved with curbs or other barriers, as to provide adequate protection for pedestrians.

Article XI. Sign Regulations

1105.2. *Individual business (businesses outside shopping centers and like clustered developments) and planned business development (shopping centers and like clustered developments) ground signs.* One ground sign not to exceed the following surface area limitations:

(c) Individual business establishments located in separate buildings on the same tax parcel are considered administrative lots or private ground lease divisions...One ground sign is allowed per administrative lot in compliance with the zoning district size and area requirement...In lieu of ground signs, business establishments within the C-1 central business district may instead be permitted a single sandwich board sign to be placed on the sidewalk adjacent to the front of the individual business or on the brick paved area providing such a location does not pose a safety hazard. A sandwich board sign is an A-frame or inverted V-shape sign which is portable and usually double-sided. This sign must comply with section 46-1 of the city Code and must not exceed four feet in height or eight square feet in area per side of sign...

1105.5. *Marquee signs allowed in all commercial districts.* Movie theaters located within commercial districts shall be allowed a marquee with one or two copy sign surfaces. Total copy area allowance, for all sign surfaces, shall not exceed five square feet per linear foot of canopy

with a maximum total height limit of no more than five feet at any point. A marquee shall not extend more than ten feet from the building nor be less than nine feet above the ground or sidewalk at the lowest point.

1105.61. *Decorative flags, banners and other moving devices in the C-1 district.* Decorative flags, banners and other moving devices (balloons, windsocks, etc.) may be allowed in the C-1 district only, providing all of the following conditions are met:

(b) Banners or flags cannot exceed a width of four feet and a length of six feet; must be secured to the wall in such a manner that the bottom portion is at least seven feet from the sidewalk or does not interfere in any manner with pedestrian traffic.

1105.83. *Banners, flags, and pennants.* Banners, flags, and pennants shall be allowed on the premises provided the following requirements are met:

d. No banner, flag or pennant shall be placed at the height of less than seven feet above the sidewalk, in order not to impede pedestrian traffic. No banner, flag or pennant may overhang any portion of a vehicular travelway, except as otherwise allowed by this ordinance;

Appendix B. Subdivision Ordinance

Article IX. Development Standards

Section 92. Design standards.

92.4. *Blocks.* The length, width, and shape of blocks shall be reasonably designed to provide for the following: Adequate building sites for the proposed use, vehicular and pedestrian circulation including control of traffic. The maximum and minimum length of blocks shall be as follows:

92.41. *Length.* Block lengths shall not exceed 1,500 feet nor be less than 400 feet. Where deemed necessary by the planning board, a pedestrian crosswalk of at least ten feet in width may be required. Minimum block lengths shall not apply to cul-de-sacs.

Section 95. Sidewalks and pedestrian access to public facilities.

The planning board may require sidewalks on new streets in accordance with section 101.7. Sidewalks must meet current federal and state standards. The board may permit installation of walkways constructed with material other than concrete when it concludes that:

(a) Such walkways would serve the residents of the development as adequately as concrete sidewalk; and

(b) Such walkways would be more environmentally desirable or more in keeping with the overall design of the development.

(c) A buffer strip may also be required between the edge of the sidewalk and edge of the street.

95.1. *Pedestrian access.* Whenever the planning board finds that a means of pedestrian access is necessary from the subdivision to schools, parks, playgrounds, or other roads or facilities and that such access is not conveniently provided by sidewalks adjacent to the streets, the developer may be required to reserve an unobstructed easement of at least ten feet in width to provide such access. Access must be provided in compliance with all federal and state standards.

Article X. Installation of Improvements

101.7. *Sidewalks.* Sidewalks shall be constructed on such streets as the planning board considers sidewalks necessary. Sidewalks shall be constructed within the street right-of-way and installed as follows:

101.71. *Material and construction.* Sidewalks shall be constructed of not less than 4,000 P.S.I. concrete and shall not be less than four feet wide, four inches thick, constructed on an adequately graded base, except that where the sidewalk crosses a driveway it shall be six inches thick.

Other materials may be approved by the city but if other materials are used then said sidewalk must be installed in a satisfactory manner and meet the city's approval.

The surface of the sidewalk shall be steel trowel and light broom finished and cured with an acceptable curing compound. Tooled joints shall be provided at intervals of not less than five feet and expansion joints at intervals of not less than 30 feet. The sidewalk shall have a lateral slope of 1/4 inch per foot.

101.72. *Elevation.* The elevation of the sidewalk shall be related to the elevation of the crown of the adjacent roadway. The sidewalk shall be no less than six inches or more than 18 inches above the roadway crown, except that crosswalk elevations shall be properly related to existing or prospective roadway curb.

101.73. [*Compliance.*] All sidewalks shall comply with federal and state requirements.

Chapter 54. PLANNING

Article III. Historic Districts and Landmarks

Sec. 54-58. Same--Certain changes not prohibited.

Nothing in this article shall be construed to prevent the ordinary maintenance or repair of any exterior architectural feature in a historic district or on a landmark which does not involve a change in design, materials, or outward appearance thereof; the ordinary maintenance or repair of streets, sidewalks, pavement marking, street signs, or traffic signs; or the construction, reconstruction, alteration, restoration or demolition of any such feature which the building inspector shall certify is required by the public safety because of an unsafe or dangerous condition. Nothing herein shall be construed to prevent the maintenance or, in the event of an emergency, the immediate restoration of any existing above-ground utility structure without approval by the committee.

Chapter 58. SOLID WASTE MANAGEMENT*

Sec. 58-8. Special refuse problems.

(d) *Free curbside pickup service.* Rubble, wood, tree trimmings, hedge clippings and debris resulting from cleaning of property may be placed at curbside for collection during the first Thursday through Saturday period of each month. The director shall collect a reasonable amount of such rubbish at no charge, but he shall have the authority to refuse to collect heavy or bulky items that cannot be handled readily by two adult persons. It shall be unlawful for any person to place or allow to be placed any tree trimmings or shrubbery trimmings on any street or sidewalk so as to obstruct the free passage of vehicles or pedestrians.

Chapter 62. Streets, Sidewalks, and Other Public Ways

Article I. In General

Sec. 62-1. Placing objects on streets and sidewalks.

(a) No brick, stone or wood or other substance obstructing the free passage of persons or vehicles shall be placed or allowed to lie in any of the alleys, streets, sidewalks or other public ways of the city, nor, except as permitted in this chapter or by the provisions of section 46-1, shall any person place or allow to lie on or in any of the streets, sidewalks, alleys or other public ways of the city any boxes, crates, casks or barrels of any description, or any other obstruction of any kind; provided, any person erecting a building may, with the permission of the city manager, place building materials for immediate use on the streets or sidewalks in such a way as to not unduly interfere with vehicular or pedestrian traffic.

(b) Any existing door which opens outward over a public sidewalk may remain in place so long as the building owner executes an agreement acknowledging that the door could present a safety hazard to pedestrians and holding the city harmless for any and all costs to the city, including attorney's fees, court costs and all damages suffered by any pedestrian claiming injury from such door, provided that (1) no such door shall occupy more than 24 inches of space over the public sidewalk and (2) the city manager reviews, approves and accepts the specific provisions contained in the hold harmless agreement. Should no such agreement be made within 30 days' notice to the building owner, then all doors on such building which open over the public sidewalk shall be removed by the building owner.

Sec. 62-2. Newsracks.

Notwithstanding the provisions of section 62-1, racks for the display of newspapers, magazines and other printed material may be placed on sidewalks or other exterior areas if specific locations do not interfere with parking and loading areas [or] pedestrian traffic, or otherwise create safety problems, subject to the provisions of section 46-1.

Sec. 62-3. Cleaning of sidewalks; snow and ice removal.

Every occupant or owner of business or commercial property in the central business district in front or alongside of which the sidewalk is paved with stone, brick, asphalt or concrete shall be responsible for keeping the sidewalk clean and free of trash, dirt or other debris and shall remove snow and ice therefrom at the earliest possible time and as soon as the weather permits. For purposes of this section, the central business district shall be considered the area bounded on the north by Probart Street, on the east by Rice Street, on the south by Morgan Street and on the west by Oaklawn Avenue.

Sec. 62-4. Construction near sidewalk.

Before building or remodeling, at any place where the building or remodeling is in close proximity to the sidewalk, a covered passageway shall be constructed so as to leave the sidewalk unobstructed and provide safe and easy passage, provided a permit for such construction has been issued by the inspection department.

Sec. 62-5. Damage to streets, sidewalks, alleys or other public ways.

It shall be unlawful for anyone to damage any streets, sidewalks, alleys or other public ways on the city's street system. The city manager or his designee may require a written permit for any use of the street system that will involve the use of any heavy equipment or any other types of equipment which, in the judgment of the city, will damage the system. The permit, if required, shall provide satisfactory assurances, including a bond if deemed necessary, that any damage to the city's streets, sidewalks, alleys or public ways will be properly repaired.

Sec. 62-6. Handicapped ramps in sidewalks.

During the construction of any sidewalk, whether new or replacement, handicapped ramps shall be placed in the sidewalk where it intersects with streets and roads unless it is determined by the city public works director that such a ramp is impractical.

Article II. Excavations

Sec. 62-31. Permit required.

It shall be unlawful for any person to dig any hole, ditch or excavation, of any kind whatsoever, on, in or under any street, sidewalk or other public way in the city without first securing a written permit therefore from the city manager or his designated representative, and providing him satisfactory assurance, including a bond if deemed necessary, that the streets, sidewalks and other public ways will be properly repaired.

(Code 1980, § 15-6)

Sec. 62-32. Restoration of surface.

All openings made in any street, sidewalk or other public way under the provisions of this article shall, immediately upon completion of the work, be filled in and restored, and the surface thereof made flush with the adjacent surface, satisfactory to the city manager. Any hard surface, macadam or asphalt removed shall be replaced by or under the supervision of the city, at the expense of the applicant granted permission to open the street, sidewalk or other public way.

(Code 1980, § 15-7)

Sec. 62-33. Protection by cover or barricades; warning lights.

It shall be unlawful for any person making any excavation for any purpose whatsoever in or on any of the streets, sidewalks or other public ways to fail to securely cover such excavations with plank or place ropes around the excavation three feet from the ground, or fail to place a sufficient number of lights around such excavation before dark and to keep such lights burning all night every night such excavation shall be open.

(Code 1980, § 15-8)

Cross references: Coverings on wells, excavations and embankments, § 38-47.

Sec. 62-34. Illegal removal of barricades or warning lights.

It shall be unlawful for any person other than a city employee to remove, tear down or destroy any barricade erected by the city or by its authority, or any warning lights thereon.

(Code 1980, § 15-9)

Cross references: Driving through street barricades, § 66-86.

Sec. 62-35. Furnishing maps or plans of underground improvements.

(a) All persons having underground cables, wires, pipes or other appurtenances beneath the city's streets, sidewalks or other public ways shall furnish the city copies of maps or plans showing the size, depth and approximate location from the curb of the cables, etc. On all future installations of underground cables, wires or pipes, maps or plans shall be furnished the city at the time of obtaining the permit as required in section 62-31.

(b) Within 60 days from receipt of the maps and plans, the city will furnish to the party a copy of the city's water and sewer lines.

Article III. Construction and Repair

Sec. 62-61. Permit required.

No person shall begin to construct, reconstruct, repair, alter or grade any sidewalk, curb, curb cut, driveway or street or proposed street in any subdivision, whether in existence or proposed, or elsewhere in the city without first obtaining approval from the city manager.

Article VI. Pedestrian/Bicycle Pathway

Sec. 62-141. Definitions.

As used in this ordinance the words and expressions defined in this section shall be held to have the meanings herein given to them:

Bicycle: A non-motorized vehicle with two (2) or three (3) wheels tandem, a steering handle, one (1) or two (2) saddle seats, and pedals by which the vehicle is propelled.

Bikeways: All thoroughfares that explicitly provide for bicycle travel including facilities existing within street and highway rights of way and facilities along separate and independent corridors.

(1) *Bicycle route:* Those bikeways on existing street rights of way where bicycles share the roadway with motor vehicles. Bicycle routes are designated by signs placed on vertical posts or stenciled on the pavement.

(2) *Bicycle lane:* Those bikeways on existing street rights of way where a portion of the roadway is set aside for exclusive bicycle use and designated by signs on vertical posts or stenciled on the pavement and by a painted line marking the bicycle lane on the pavement. Through travel by motor vehicles or pedestrians is prohibited. Cross flows by motor vehicles and pedestrians when necessary to gain access to and from a public street or alley and/or a private driveway or other entranceway are permitted.

(3) *Bicycle path:* Those bikeways on existing street rights of way or on a completely separated right of way or easement. Use by pedestrians may or may not be permitted. The parking of any through traffic by motor vehicles is prohibited. Cross flows by motor vehicles and pedestrians when necessary to gain access to and from a public street or alley and/or a private driveway or other entranceway are permitted.

Electric personal assistive mobility device: A self balancing non tandem two wheeled device, designed to transport one person, with a propulsion system that limits the maximum speed of the device to 15 miles per hour or less.

Motor vehicle: Every self propelled device in, upon or by which any person or property is or may be transported or drawn upon a highway, with the exception of bicycles and electric personal assistive mobility device as defined herein. This term shall not apply to a device which is designed for and intended to be used as a means of transportation for a person with a mobility impairment, or who uses the device for mobility enhancement, is suitable for use both inside and outside a building, including sidewalks, and is limited by design to a maximum speed of 15 miles per hour when the device is being operated by a person with a mobility impairment.

Sec. 62-142. Bikeways (bike routes, bike lanes and bike paths as indicated) established.

The following bicycle paths are established for the use of bicycles and pedestrians within the jurisdiction of the City of Brevard.

City of Brevard Sports Complex trailhead to north side of its intersection with Allison Road.

Sec. 62-143. Regulated uses.

The following provisions shall apply upon the established bikeways of the City of Brevard.

- (1) Motorized vehicles are strictly prohibited on all bikeways. Emergency vehicles and those maintenance vehicles specifically authorized by the City Manager are exempted.
- (2) Cyclists and pedestrians using bikeways before dawn or after dusk shall be equipped with proper lights. Including a white light that is visible from five hundred feet to the front, and a red or amber light that is visible from five hundred feet to the rear. Pedestrians should use white lights visible two hundred fifty feet to the front, and wear light or reflective clothing.
- (3) Blocking of a bikeway by any user is strictly prohibited.
- (4) Possession and use of alcoholic beverages and/or drugs are strictly prohibited.
- (5) Littering is strictly prohibited.
- (6) Pets are permitted upon bikeways however they must be leashed at all times and the owner or responsible party must remove all animal waste.
- (7) Camping and campfires are strictly prohibited.
- (8) Smoking is strictly prohibited.
- (9) The cutting or damaging of any tree or plant is strictly prohibited unless specifically authorized by the City Manager.
- (10) Horses and/or other equines are strictly prohibited on all bikeways.

Sec. 62-144. Applicability of traffic regulations.

All bicycles used and operated upon and across designated bikeways within the City of Brevard's jurisdiction shall be considered vehicles and the riders of such bicycles shall be subject to all provisions of the Brevard City Code and Chapter 20 of the North Carolina General Statutes applicable to the driver of vehicles except those which by their nature can have no application. Bicyclists using bike paths shall comply with all traffic regulations posted along the path and shall stop or otherwise yield the right of way before entering or crossing a public street.

Sec. 62-145. Shared facilities.

While roadways are provided for the safety and convenience of motorists, sidewalks are provided for the safety and convenience of pedestrians and bikeways are provided for the safety and convenience of bicyclists, it is recognized that in many instances such facilities may be shared. On shared facilities, the following regulations shall apply:

- (1) When using bike paths and bike trails, pedestrians shall walk as far to the right as practicable; shall walk no more than two abreast; and shall exercise due care. Bicyclists shall give an audible signal before passing a pedestrian on a bike path or trail.
- (2) Persons riding bicycles upon a bikeway shall ride single file except when passing another bicycle.
- (3) On and across bikeways upon which pedestrian traffic is also permitted bicycles shall at all times yield the right of way to pedestrians.

Chapter 66. TRAFFIC

Article III. Operation of Vehicles

Sec. 66-6. Carts, bicycles or other vehicles blocking sidewalks and driveways.

It shall be unlawful for any person to stop or park any pushcart or pull cart, bicycle or other vehicle used for hauling any articles of merchandise for charge or hire upon any sidewalk or between the curb of any street and the private property line, or in any driveway leading from the street to a private driveway or private property, in front of any business establishment in the city, unless authorized by article II of chapter 46.

Sec. 66-13. Special events.

(a) *Application of regulations.*

This section shall apply to all street fairs, festivals, carnivals, parades, marches, rallies, demonstrations, and other activities or public events that require the temporary closing or obstruction of or that substantially hinders or prevents the normal flow of vehicular or pedestrian traffic on all or a portion of any city owned property, street, or other public right-of-way. Any such activity covered by this chapter shall hereafter be referred to as a "special event".

(b) *Permit required.*

No "special event" is permitted on any city owned property, street or other public right-of-way of the city unless a permit has been issued by the city manager; however, nothing in this section shall be construed to prevent the peaceful assembly of any group for orderly expression or communication between those assembled.

(1) No parade of vehicles or march of pedestrians or animals or combination thereof upon the streets or sidewalks of the city shall be allowed except as approved by the city manager. All holders of permits granted and approved by the city manager pursuant to this section shall indemnify and hold harmless the city in all respects against any and all claims and liabilities arising out of or related to the issuance of a special event permit.

(2) Any person denied a special event permit under this section may appeal to the city council at the next regularly scheduled meeting.

(c) *Permit application.*

Applications for a "special event" permit authorized by this section that require the closure of a public street, sidewalk or other city facility shall be submitted to the city manager not less than 15 working days and not more than 90 days before the parade or event is proposed to commence, excluding recognized holidays, on a form provided by the office of the city manager and shall contain the following information:

(1) Name, address, and telephone number of the person, organization or entity seeking to conduct or sponsor the event.

(2) Name, address, and telephone number of the individual contact person in charge of the event.

(3) The proposed date and time period when the event will be conducted.

(4) The estimated number of people expected to attend the event.

(5) Sufficient proof of liability insurance in accordance with the city's standard requirements.

(6) Any other information determined by the city manager to be necessary to insure compliance with these provisions.

(7) A sketch map showing;

a. The area where the event is to take place.

b. Names of any streets to be closed or obstructed.

c. Location of any barriers or traffic control devices that will be erected.

d. Location of any concession stand, booth, or other temporary structures or facilities.

e. Location of any proposed fences, stands, platforms, benches or bleachers.

(8) The number and type of animals included as part of the event, if applicable.

Approval of animals involved in any special event is subject to subsection (m) below, and the animals' location must be specified on the sketch map.

Applications for a "special event" permit as authorized by this section not requiring the closure of a public street or sidewalk shall be submitted to the city manager not less than two working days before the parade or event is proposed to commence, excluding recognized holidays, on a form provided by the office of the city manager and shall contain the items listed above, excluding subsections (5), (7)b through e, and (8).

Applications must be accompanied by a \$25.00 application fee before being reviewed and considered. The city manager may waive this requirement for nonprofit organizations providing valid proof of 501c(3) status. If the application is denied, the fee will be refunded with the notice of denial.

(d) *Permit revocation.*

Upon violation of the terms of a permit issued under this section by the applicant or participants the city manager and/or the chief of police or his designee is authorized to revoke the permit and direct those participating to disperse.

(e) *Staff review.*

Upon receipt of the permit application, it shall be circulated to the chief of police, fire chief, public works director, and other appropriate persons for their comment. The city manager may require a conference on the application between the applicant and one or more department heads.

(f) *Insurance requirement.*

The city manager shall require as a condition precedent or subsequent to the issuance of the permit that the applicant obtain a comprehensive general liability insurance policy providing coverage that includes the entire area described in the permit for events requiring the closure of a public street, sidewalk or city facility. If such a policy is required, under the provisions of this section, the city shall be named as an additional insured on the policy. A certificate of insurance shall be provided to the city as proof of coverage. The following policy limits shall apply as minimum coverage requirements:

\$1,000,000 Combination - Single Limits of General Liability Insurance

(g) *Fees; costs incurred by the city.*

If the city is requested to provide extraordinary services or equipment or the city manager determines that extraordinary services or equipment for reasons related to protection of the public health and safety, he/she may require the applicant to pay to the city a fee sufficient to reimburse it for the costs of any extraordinary services or equipment provided to the event unless sufficient funding has been included in the city budget as adopted by city council.

(h) *Standards for issuance of permit.*

(1) The city manager shall issue the permit authorizing the "special event" unless it is found that:

a. Insufficient funding and/or resources necessary to provide extraordinary services or equipment for the event are not available; or

b. The event will interfere with the movement of emergency vehicles to such an extent that adequate police, fire, or other emergency services cannot be provided throughout the city; or

c. The applicant has failed to obtain any necessary permits or licenses, including any required building permit or privilege license; or

d. The applicant is in violation of any city ordinance; or

e. The applicant fails to obtain liability coverage and show proof of said insurance as required by the city.

- f. The event will create a severe hardship on persons occupying property adjacent to the site, location, or route of the event as a result of the denial of access to property; or
- g. The event, if held at the time or at the location or along the route proposed will cause an unreasonable and unwarranted disruption to vehicular or pedestrian traffic; or
- h. The applicant has failed to comply with any of the provisions of the special events ordinance, including but not limited to payment of any required fees and costs; or
- i. A special events permit has previously been applied for or issued for substantially the same location and time requested in the application.

(2) In considering the matters listed above and the question of whether or not to issue the permit, the public safety and health shall be given paramount importance.

(i) *Conditions upon issuance of permit.*

If a permit is issued the city manager may attach any reasonable conditions that serve to promote the protection of the public safety and health. Any event held pursuant to a permit issued under this section shall be conducted within the applicable terms including any attached conditions.

(j) *Right of appeal.*

If the city manager denies or revokes a special event permit this action shall be final unless the applicant or permit holder files, within three working days, a written appeal with the city clerk. Such written appeal will be placed for consideration by city council at its next regularly scheduled meeting. The city council shall review the matter and its decision shall be final.

(k) *Street closing.*

(1) The city manager is authorized to permit the temporary closing of a street or other public right-of-way except as otherwise provided for in subsection (l) below if the provisions of the section are adequately complied with. Any temporary closing of a street or part thereof owned and/or maintained by the State of North Carolina shall require concurrence by the NC Department of Transportation.

The city manager may only permit the temporary closing of any portion of US Highway 64, US Highway 64 Business, US Highway 276, SR 1349 (West Main Street) for city recognized special events. An event shall be considered to be city recognized upon the adoption of a resolution by city council making such designation. In order to qualify for recognition an event must have received a commitment of funding and/or resources from the city for any necessary public safety and traffic control measures prescribed by the city manager for the special event.

(2) The city clerk shall, by any adequate means, notify persons occupying property abutting the street where the event is to take place as soon as possible once a permit is issued.

(l) *Temporary parking restrictions.*

The chief of police, or his duly appointed representative, is authorized to prohibit, on a temporary basis, the parking of vehicles under the following circumstances:

Along permitted parade routes or on streets closed pursuant to the issuance of a special events permit for a period of two hours before any special event begins, during the entire event and up to one hour after the completion of the event.

(m) *Permit for special events involving animals.*

No special event in which animals are involved shall be conducted without a special permit from the city manager. The city manager shall require a bond in the amount of \$50.00 (where five or less animals are involved) or \$100.00 (where more than five animals are involved), which will provide that the person obtaining the special permit will be responsible for cleaning the city streets or property after the parade, exhibition, demonstration or experiment. All holders of special permits granted and approved by the city manager pursuant to this section shall indemnify

and hold harmless the city in all respects against any and all claims and liabilities arising out of or related to the behavior of such animals or issuance of such a special permit.

(n) *Animals prohibited.*

It shall be unlawful for any person to have or keep any animal at any event requiring the issuance of a special event permit except as otherwise provided within this section.

Sec. 66-82. Emerging from alley or private driveway.

The driver of a vehicle emerging from an alley, driveway or building shall stop such vehicle immediately prior to reaching the sidewalk, or the sidewalk areas extending across any alleyway, and, upon entering the roadway, he shall yield the right-of-way to all vehicles approaching on the roadway.

Sec. 66-83. Riding bicycle or motorcycle without hands on handlebars; riding bicycle on sidewalk.

(a) No person shall ride a bicycle or motorcycle on any street without having his hands on the handlebars, nor shall any person ride a bicycle upon any sidewalk or walkway within the business district of the city.

(b) For purposes of this section, the business district shall include those streets within the area bounded by Probart Street on the north, Gaston Street on the east, Morgan Street on the south and England Street on the west.

Sec. 66-85. Use of coasters, roller skates and similar devices.

(a) No person on roller skates, roller blades or skateboards or riding on any coaster, toy vehicle or similar device shall go upon any granolithic or paved sidewalk within the business district.

(b) For purposes of this section, the business district shall include those streets within the area bounded by Probart Street on the north, Gaston Street on the east, Morgan Street on the south and England Street on the west.

Article IV. Standing, Stopping and Parking*

Sec. 66-112. Parking prohibited in specified places.

It shall be unlawful for any person to stop, stand or park a vehicle, except when conflict with other traffic is imminent or when so directed by a police officer or traffic control device, in any of the following places:

- (1) On a sidewalk.
- (2) Within an intersection.
- (3) On a crosswalk.

Chapter 70. Utilities

Article III. Sewer System*

Sec. 70-86. Installation of connections; connections to substandard property.

(1) *Excavations.* All excavations for sewer service line installation shall be adequately guarded with barricades and lights so as to protect the public from hazard. Streets, sidewalks, parkways and other public property disturbed in the course of the work shall be restored in a manner satisfactory to the city.

**BREVARD, NC COMPREHENSIVE TRANSPORTATION PLAN
PROJECT PRIORITY LIST**

On March 21, 2005, the City of Brevard enacted Resolution Number 22-05, thereby adopting the 2005 Comprehensive Transportation Plan (hereafter, "CTP") as official policy and a guide toward the development of a multi-modal transportation system for the City of Brevard.

The following is a listing of projects identified within the CTP. Projects are listed in descending order of highest priority to lowest to the City of Brevard:

LOCATION	TIP #	DESCRIPTION	PROJECT TYPE	STATUS	DIVISION	COUNTY	JURISDICTION
US 64 Alternative Rte.	TIP #R-2702	Brevard Radial Connector/Bypass /Boulevard	Feasibility Study	TIP- Unfunded	14	Transylvania	City of Brevard
New Road		West Loop Minor Thoroughfare	New Project-sidewalks, bike path	CTP 2005	14	Transylvania	City of Brevard
US 64/US 276		Asheville Highway Boulevard	Upgrade / Median	CTP 2005	14	Transylvania	City of Brevard
SR 1546, SR 1612 & SR 1118		East Loop Minor Thoroughfare	Upgrade /Safety /New Section	CTP 2005	14	Transylvania	City of Brevard
SR 1540		Wilson Road Minor Thoroughfare	Safety / Widen / Realign	CTP 2005	14	Transylvania	City of Brevard
SR 1533		Old Hendersonville Highway Minor Thoroughfare	Widen / Realign	CTP 2005	14	Transylvania	City of Brevard
¹ US 64		Hendersonville Highway Minor Thoroughfare	Safety / Upgrade / Additional Lanes	CTP 2005	14	Transylvania	City of Brevard
² US 276/US 64/NC 280		Pisgah Forest Gateway Boulevard	Safety / Upgrade Additional Lanes	CTP 2005	14	Transylvania	City of Brevard
US 64		Rosman Highway Boulevard	Upgrade / Median	CTP 2005	14	Transylvania	City of Brevard
US 276		Greenville Hwy Major Thoroughfare	Safety / Additional Lanes	CTP 2005	14	Transylvania	City of Brevard
US 64/US 64 Bypass		Broad-Caldwell Unbalanced Couplet	Widen/ Additional Lanes	CTP 2005	14	Transylvania	City of Brevard

¹ This project should be considered in concert with the Pisgah Forest Gateway Boulevard.

² This project should be considered in concert with the Hendersonville Highway Minor Thoroughfare.

The following is a more detailed description of projects listed above:

Roadway Improvements

(1) US 64 Alternative Route (Brevard Bypass/Boulevard) – Construct a new boulevard route that will be a bypass to the Brevard portion of US 64. Various alignments of the Bypass will be considered by NCDOT during the environmental analysis phase of the project. This proposed boulevard will relieve congestion on US 64 by diverting both local and through traffic.

(2) West Loop Minor Thoroughfare – Construct a western loop road using existing portions of Railroad Avenue, Cashiers Valley Road (SR 1344), and Nicholson Creek Road (SR 1346), as well as new sections connecting Railroad Avenue with Osborne Road and Cashiers Valley Roads. The purpose of the project is to address congestion problems on US 64 between Osborne Road to Caldwell Street by providing a more convenient alternative route for local traffic.

(3) Asheville Highway Boulevard (US 64/US 276) – Improve the existing five-lane section to a median-divided boulevard from Caldwell Street to the existing divided portion near Allison Road. This project will improve the road capacity and address congestion and safety in this segment.

(4) East Loop Minor Thoroughfare – Construct an eastern loop road that extends the current in-town loop road on Chestnut Street/Neely Road/Park Avenue/Parkview Drive south to Rosman Highway using Gallimore Road and a newly constructed section. . The purpose of the project is to address safety concerns on the current loop roads, while also alleviating congestion problems on US 64 by providing a low-speed alternative route for local traffic.

(5) Wilson Road Minor Thoroughfare (SR 1540) – Upgrade or realign traffic lanes and intersections on Wilson Road from Old US 64 to US 276. These improvements will address safety concerns at specific intersections (Wilson/Ecusta Roads) and the entire facility.

(6) Old Hendersonville Highway Minor Thoroughfare (SR 1533) – Widen the travel surface from 18 feet to 24 feet from the northeast planning boundary south to US 64. This improvement will improve the flow of traffic.

(7) Hendersonville Highway Minor Thoroughfare (US 64) – Widen the existing two-lane facility to a multi-lane facility from the eastern CTP planning boundary to the intersection of US 64 and NC 280. The purpose of this project is to provide greater access to commercial areas along US 64 and reduce congestion caused by turning vehicles.

(8) Pisgah Forest Gateway Boulevard (US 276/US 64/NC 280) – Upgrade the intersection of US 276/US 64/NC 280 with additional turn lanes and traffic lanes. Also begin upgrade of NC 280 to a boulevard. This project will address congestion and safety concerns at the busy intersection.

(9) Rosman Highway Boulevard (US 64) – Improve existing five-lane section to a boulevard from South Caldwell Street to the existing divided portion near the southwestern CTP planning boundary. The improvement will improve the road capacity and safety of this section of US 64.

(10) Greenville Highway Major Thoroughfare (US 276) – Widen the existing two-lane facility to three-lanes from Gallimore Road to south of the Brevard Elementary School and add a center turn

lane. This improvement will help relieve congestion caused by the turning movements of vehicles accessing the school.

(11) Broad-Caldwell Unbalanced Couplet (US 64/US 64 Bypass) – Convert Broad and Caldwell Streets into an unbalanced lane couplet and widen Caldwell St. to a three-lane facility from Jordon Street to Rosman Highway (US 64). This project will also include making Jordan and Morgan Streets a one-way couplet, restriping Main Street, and adding a left turn lane on Broad Street onto Morgan Street. All of these changes will be coordinated with an upgrade of the downtown traffic signal system. The improvements are recommended in order to address congestion and safety issues on US 64 in downtown Brevard.

The following non-roadway projects are not prioritized. However, the City of Brevard intends to identify and pursue funding and development of all projects listed below. Also, the City intends to pursue incremental implementation of pedestrian and bicycle projects in conjunction with the development of relevant roadway projects listed above, as well as in the development other non-CTP projects not addressed herein (i.e., Small Construction and other Division or District projects).

Bicycle / Pedestrian Improvements

Bicycle Improvements, On-Road – Improve existing roadways and design future roadways to develop a safe, continuous on-road system of routes to connect residential areas to major attractors of bicycle trips. Such improvements should be targeted to NCDOT-designated Bicycle Routes as well as other facilities. Such improvements will improve bicycle safety, minimize air pollution, and provide for Brevard’s growing bicycling population.

Pedestrian / Bicycle Pathway Improvements – Construct an off-road pedestrian / bicycle pathway connecting Pisgah National Forest to downtown Brevard and points beyond. This improvement will provide a main bicycle / pedestrian “expressway” linking the City’s primary centers of development, improve bicycle and pedestrian safety, minimize air pollution, and provide for Brevard’s growing bicycling and pedestrian population.

Pedestrian / Bicycle Pathway Improvements, Other – Construct other off-road pedestrian / bicycle pathways. Such improvements will create linkages between and among the aforementioned Pedestrian / Bicycle Pathway, aforementioned On-Road Bicycle Improvements, residential neighborhoods, key destinations, natural resource areas, and etc.

Pedestrian Improvements – The City of Brevard and NCDOT are developing a Comprehensive Pedestrian Plan that will examine the City’s existing pedestrian infrastructure and establish priorities for the improvement of that system and for the installation of new infrastructure. Upon completion, this Comprehensive Transportation Plan will be updated to reflect relevant recommendations of the Comprehensive Pedestrian Plan.

Public Transportation Improvements

Public Transportation Improvements – Continue supporting the County’s existing Transportation Program, and pursue the extension of transportation services to areas within and outside Transylvania County for general public and special populations such as the elderly, handicapped, and youth for medical and other needs not available within the county.

Park and Ride Improvements – Establish a Park and Ride program, and identify and pursue opportunities to create Park and Ride lots within the City of Brevard and Transylvania County. This will relieve congestion and minimize air pollution associated with commuters, particularly those who work in Buncombe County, Henderson County, and upstate South Carolina.

Rail Improvements – Actively work with Norfolk Southern Corporation, rail users, and the NCDOT Rail Division to maintain and preserve the existing Norfolk Southern Railway extension to Pisgah Forest. This will enhance economic development programs and opportunities, and will position the City and County to take advantage of rail-based transit opportunities that may arise in the future.

**SIDEWALK REPAIR PRIORITY LIST
REVISED JANUARY 30, 2006**

SIDEWALK LOCATION	LENGTH X WIDTH	SQUARE FEET	CUBIC YARDS
Whitmire Street			
#325	6x5	30	0.4
#315	12x5	60	0.7
#307	12x5	60	0.7
#303	24x5	120	1.5
#312	6x5	30	0.4
#326	12x5	60	0.7
Brevard Moving & Storage	48x5	240	3.0
South Caldwell Street			
#201	24x5	120	1.5
#240 - #242	72x5	360	4.5
#248, 12' Curb	30x5	212	2.7
#316 - #318	36x5	180	2.3
#322	12x5	60	0.7
Rear of McCosh's	12x5	60	0.7
#412 - #414, 12' Curb	36x5	212	2.7
#420	18x5	90	1.1
#502 - Jamar Plaza	60x5	300	3.8
Funeral Home	25x5	125	1.5
West Main Street			
#220	12x10	120	1.5
King Street			
Brevard Lumber Company	200x8	1,600	19.5
South Gaston Street			
County Sheriff's Department	48x5	240	3.0
Maple Street			
#206	6x5	30	0.4
#124	6x5	30	0.4
#262	6x5	30	0.4
#268	54x5	270	3.3
#270	6x5	30	0.4
#278	6x5	30	0.4
#254	6x5	30	0.4
East Main Street			
#539	24x5	120	1.5
No Number	18x5	90	1.1
#542	24x5	120	1.5
#555	18x5	90	1.1

Multi-Use Path		
Streets	Length (Feet)	Length (Miles)
Phase I (Asheville Hwy, Allison Rd to Sports Complex)	8,483	1.61
Phase II (Ecusta Rd, Sports Complex to Davidson River Bridge)	1,210	0.23
Grand Total	9,693	1.84

Sidewalks		
Streets	Length (Feet)	Length (Miles)
Asheville Hwy	5,372	1.02
Blueberry Hill	313	0.06
Broad N	3,012	0.57
Broad S	5,361	1.02
Caldwell	6,625	1.25
Cambridge Dr.	3,630	0.69
Camp Sapphire	108	0.02
Camp Town	1,167	0.22
Canterbury	960	0.18
Carver	648	0.12
Cashiers Valley	1,788	0.34
College Station	74	0.01
Country Club	3,287	0.62
Danbury	782	0.15
Deer Lake	1,469	0.28
Deer Run	531	0.10
Deerlake	1,940	0.37
East Main St. Ext.	865	0.16
Elm Bend	801	0.15
England	275	0.05
English Hills	436	0.08
Fisher	1,368	0.26
Franklin	1,313	0.25
French Broad	5,136	0.97
Gaston	2,578	0.49
Greenville Hwy	2,001	0.38
Hillview Ave	2,436	0.46
Hopewell	415	0.08
Johnson	1,402	0.27
Jordan	3,415	0.65
King	1,345	0.25
Knight Ct.	285	0.05
London	135	0.03
Main Street (E)	4,924	0.93
Main Street (W)	2,945	0.56
Maple	3,800	0.72
Meadow View	1,682	0.32
Mills	1,279	0.24
Morgan	2,362	0.45
Neely Rd / Park View	1,952	0.37
Oakdale Ln	1,924	0.36
Oaklawn Ave	887	0.17
Old Cottage Ln	486	0.09
Park Ave	1,644	0.31

Pine Mountain	576	0.11
Probart St.	2,625	0.50
Rail Road Ave	882	0.17
Sheffield Pl	446	0.08
Silversteen	815	0.15
Straus Parkway	1,574	0.30
Throwbridge	1,231	0.23
Tutor	208	0.04
Unity	1,862	0.35
Whitmire	3,867	0.73
NA / Unknown	14,286	2.71
Grand Total	113,532	21.50

Undeveloped Public Trail		
Streets	Length (Feet)	Length (Miles)
Phase I (Asheville Hwy, Allison Rd to Sports Complex)	1,090	0.21

**CITY OF BREVARD
PEDESTRIAN PLAN STEERING COMMITTEE**

Appointments: September 7, 2004 – City Council Meeting
 The purpose of this Committee is to develop a comprehensive Pedestrian Facility Plan for the City. The Committee consists of 11 members with City Planner Josh Freeman serving as the Chairman. An experienced engineering/planning firm will also provide assistance in development of the Plan. It is expected to take 12-18 months to complete this project.

<u>Appointee</u>	<u>Phone</u>	<u>Email</u>
John Tinsley Transylvania Board of Education 400 Rosenwald Lane Brevard, North Carolina 28712	884-6173 (W) 877-3798 (H)	jtinsley@transylvania.k12.nc.us
Steve Smith Transylvania Co. Health Director 203 E. Morgan Street Brevard, North Carolina 28712	884-3135 (W) 862-8110 (H)	steve.smith@health.co.transylvania.nc.us
Russ Knights 316 Whitmire Street Brevard, North Carolina 28712	884-5099 (H)	russki@citcom.net
David Carter Brevard Police Department 151 W. Main Street Brevard, North Carolina 28712	883-2212 (W)	brevardpdcpt@citcom.net
Don Owen Brevard Public Works Dept. 151 W. Main Street Brevard, North Carolina 28712	884-4121 (W)	donpwks@citcom.net
Amber P. Morrish Director of Development Brevard College 400 North Broad Street Brevard, NC 28712	884-8233	morrisha@brevard.edu
Josh Freeman Brevard Planning Dept. 151 W. Main Street Brevard, North Carolina 28712	883-8580 (W)	brevplandir@citcom.net
Frank Pearsall 227 Grove Circle Brevard, North Carolina 28712	883-9879 (H)	

Carrie Runser-Turner Land Of Sky Regional Council 25 Heritage Drive Asheville, North Carolina 28806	251-6622	carrie@landofsky.org
Tissie Stroup Transylvania Recreation Dept. 1150 Ecusta Road Brevard, North Carolina 28712	884-3156 (W) 883-2242 (H)	recdept@citcom.net
David Bender NCDOT Planning Program Manager Division of Bicycle and Pedestrian Transportation 104 Fayetteville St. Mall Raleigh, NC 27601	919-715-2342	dbender@dot.state.nc.us

<u>Consultant</u>	<u>Phone</u>	<u>Email</u>
Dale Pennell McGill Associates, P.A. 55 Broad Street Asheville, NC 28801	252-0575 252-2518	pennell@mcgillengineers.com
Rhonda H. Honeycutt McGill Associates, P.A. 55 Broad Street Asheville, NC 28801	252-0575 252-2518	rhonda@mcgillengineers.com

Pedestrian Plan Authorization Documents



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

MICHAEL F. EASLEY
GOVERNOR

LYNDO TIPPETT
SECRETARY

April 8, 2005

Rick Howell, City Manager
City of Brevard
151 West Main Street
Brevard, NC 28712

Re: WBS No. 37309.1.1 Notice to Proceed
NCDOT Bicycle and Pedestrian Planning Grant Initiative
Comprehensive Pedestrian Planning Grant

ROUTE TO:
MAYOR
CITY COUNCIL
CITY ATTORNEY
CITY MANAGER
CITY CLERK
B & G
DPU

___ DPW
___ FIN. DIR.
___ FIRE CHIEF
___ HR
___ PLANNING DIR.
___ POLICE CHIEF
___ FILE

[Handwritten initials and notes]

Josh/Sony: Please get together on this. R 4/14/05 J & K-18-06

Dear Rick,

This is to authorize tasks found in the attached scope of work in accordance with the attached Municipal Reimbursement Agreement dated August 24, 2004 for professional services related to the development of a Comprehensive Pedestrian Plan in the City of Brevard.

These combined tasks are authorized for an amount not to exceed \$16,000. Any cost or expenses incurred in addition to this amount and not in accordance with the attached scope of work and Municipal Reimbursement Agreement will be the responsibility of the municipality. The municipality shall submit no more than three (3) invoices for reimbursement and the final invoice reimbursement will be withheld until final approval by NCDOT. All invoices are to be submitted to David P. Bender, Bicycle and Pedestrian Division, Project Administrator.

If you accept these tasks and terms, please confirm in writing below and remit to me. If you have any questions about this agreement please feel free to contact me at 919-715-2340.

Sincerely,

David P. Bender, AICP
Planning Program Manager

Cc: Tom Norman, Director
Joshua S. Freeman, Brevard Planning Director

Attachments

MAILING ADDRESS:
NC DEPARTMENT OF TRANSPORTATION
DIVISION OF BICYCLE & PEDESTRIAN TRANSPORTATION
1552 MAIL SERVICE CENTER
RALEIGH NC 27699-1552

TELEPHONE: 919-715-2340
FAX: 919-715-4422

WEBSITE: WWW.NCDOT.ORG/TRANSIT/BICYCLE/
EMAIL: DBENDER@DOT.STATE.NC.US

LOCATION:
TRANSPORTATION BUILDING
1 SOUTH WILMINGTON STREET
ROOM 304
RALEIGH NC

WBS Element No.: 37309.1.1
NCDOT Bicycle and Pedestrian Planning Grant Initiative
Comprehensive Pedestrian Planning Grant

For professional services related to the scope of work and in accordance with the Municipal Reimbursement Agreement dated August 24, 2004.

Acknowledgement and Acceptance:

City of Brevard

Date



February 23, 2005

Mr. Rick Howell
City Manager
City of Brevard
151 West Main Street
Brevard, North Carolina 28712

RE: Revised Proposal for Engineering Services
Brevard Pedestrian Plan
Brevard, NC

Dear Mr. Howell:

As you have requested, we have estimated the fees for our consulting engineering services for the preparation of a NCDOT funded Pedestrian Plan for the City of Brevard.

It is our understanding that the project generally consists of the developing a master plan for pedestrian needs within the City of Brevard. A recommended template for developing municipal bicycle or pedestrian plans has been developed by the NCDOT's Division of Bicycle and Pedestrian Transportation and Statewide Planning Branch, and this template includes a number of goals and objectives. These include the following:

- Evaluation of Current Conditions
- Review of current plans, programs, and policies
- Developing bicycle or pedestrian system plans
- Design of facility standards and guidelines
- Ancillary facilities and programs
- Project development
- Maps and illustrations

We met with NCDOT staff David Bender and Mary Meletiou in Raleigh on December 2 to better understand their objectives, criteria, and concerns for pedestrian plans. After this meeting, our review of the pedestrian plan template, and a December 9 meeting with Josh Freeman, we anticipate providing the following services to prepare this plan for the City:

E n g i n e e r i n g • P l a n n i n g • F i n a n c e

McGill Associates, P.A. • P.O. Box 2259, Asheville, NC 28802 • 55 Broad Street, Asheville, NC 28801

PROJECT OVERSIGHT

1. Assist in establishing the Project Steering Committee (PSC).
2. Develop project objectives.
3. Establish final project scope and schedule
4. Determination of PSC's tasks, including GIS and system inventory.
5. Conduct a needs assessment.
6. Participate in three (3) PSC meetings.
7. The Planning Staff will provide leadership to the PSC, and McGill Associates staff will attend PSC meetings as needed.

REVIEW EXISTING PEDESTRIAN FACILITIES

1. Evaluate scope of existing programs.
2. Determine types and extent of existing deficiencies in walkways, maintenance programs, and policies.
3. Assess general connectivity within the existing infrastructure.
4. Appraise the general condition of existing facilities.
5. Define areas of the community exhibiting greatest needs for pedestrian facilities.
6. Review all existing studies and plans relating to thoroughfares, city master plans, ordinances, development regulations, and other documents that may be associated with the pedestrian plan.

DEVELOP PROGRAM PARAMETERS / STANDARDS

1. Develop general program parameters for walkways.
2. Establish walkway development standards for construction, location, classification, landscaping, access, street crossings, and other design and service issues.
3. Establish bikeway standards, where necessary as a part of the pedestrian plan.
4. Develop signage parameters and standards.

PREPARE PEDESTRIAN PLAN

1. Establish a concise statement of plan objectives and policy.
2. Develop a Pedestrian Plan encompassing maps and text that addresses the magnitude of future improvements, including the correction of existing deficiencies and the development of new infrastructure.
3. Establish a Capital Improvement Plan (CIP) addressing scheduling, cost estimates, and priority setting for proposed walkway improvements. Preparation of the CIP will include

clarification and identification of the City's priorities for improvements, and implementation of the overall plan and its priorities.

4. If communications are needed with NCDOT staff during preparation of the Pedestrian Plan, the Planning Staff and McGill Associates staff will coordinate the necessary contacts.
5. The Pedestrian Plan will generally contain the content requirements as described in the NCDOT plan template.
6. Identify opportunities for both on-road and off-road facilities.
7. Formulate a preventative maintenance program.
8. Recommend implementation of user safety, law enforcement, and user conflict protocols among involved City departments.
9. Develop a program financing strategy addressing routine capital outlay requirements, grant opportunities, potential debt for major improvements, and financial issues related to the maintenance program.

PUBLIC PARTICIPATION

1. Develop a public involvement strategy for the project
2. Conduct two (2) public involvement workshops/charettes to include the following objectives:
 - A. Results of community needs assessment
 - B. Public visioning for the program
 - C. Solicitation of community group involvement
 - D. Coordination with other community planning and improvement initiatives.

PROGRAM IMPLEMENTATION

1. Recommend changes in current development ordinances which will require and/or encourage participation by developers and private parties.
2. Programs will be recommended that emphasize pedestrian education, safety, enforcement, encouragement of use, and related initiatives in Brevard. These are critical to the implementation of a working Pedestrian Plan, both in terms of support for improvements and success of the overall pedestrian system.

PROJECT TIMELINE

The anticipated project timeline will generally follow the schedule below, and begins upon approval of our proposal by the City and NCDOT, and notification to proceed:

- Month 1: Review all project information provided by the City regarding existing master plans, ordinances, regulations, etc.; develop overall project objectives; review all Pedestrian Plan data available from NCDOT and related websites; discuss PSC responsibilities with Planning staff; schedule first PSC meeting with NCDOT and Planning Staff; prepare draft of needs assessment survey.
- Month 2: Establish final project scope; prepare base mapping for Pedestrian Plan; attend first PSC meeting; coordinate existing facilities inventory through PSC; prepare needs assessment survey forms and distribute through mail, website, etc.
- Month 3: Review mapping and conduct field work to identify points of interest for connectivity of pedestrian routes (college, schools, library, public buildings, recreational facilities, etc.); examine general connectivity of routes within City; review existing City ordinances and policies relating to pedestrian activities; schedule second PSC meeting.
- Month 4: Review inventory as prepared by PSC; determine deficiencies in existing system, including existing conditions; develop general parameters for all types of pedestrian features, including details for construction; establish standards for signage and bikeways; attend second PSC meeting; schedule first public involvement meeting; begin draft of Pedestrian Plan; review results from needs assessment survey.
- Month 5: Attend first public involvement meeting; review existing budgets and programs for sidewalk/bikeway maintenance; prepare draft of Pedestrian Plan, including plan objectives, policies, mapping, CIP, cost estimates, financing, etc.
- Month 6: Schedule and attend meeting with Planning staff to review draft of Pedestrian Plan; develop schedule and strategy for second public involvement meeting; address staff and PSC's comments on draft Plan.
- Month 7: Attend second public involvement meeting; respond to comments from meeting; attend third PSC meeting.
- Month 8: Prepare final version of Pedestrian Plan; meet with staff to review final Plan; transmit copies of final Plan to City and NCDOT.

DELIVERABLES

1. Transmit five (5) copies of the final plan to NCDOT for their use in planning minor or major construction projects within the City and surrounding area.

Mr. Rick Howell
February 23, 2005
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2. Transmit seven (7) hard copies and one (1) digital copy of the Brevard Pedestrian Plan to the City of Brevard.
3. All digital and hard copy formats of the final Brevard Pedestrian Plan and associated maps, attachments, appendices, etc. will become the property of the City of Brevard and the NCDOT.

We anticipate providing the above services for the **Lump Sum fee of \$16,000**, based on the amounts described in the attached spreadsheet. If authorized to proceed in March, we should be able to complete the plan by November (approximately 8 months).

If the above is acceptable to you, please provide appropriate signature below and return one (1) copy to us. We appreciate your consideration for our services with the project and look forward to having the opportunity of working with you. We are available at all times to respond to any questions or concerns.

Sincerely,
McGILL ASSOCIATES, P.A.



DALE E. PENNELL, P.E., P.L.S.

Cc: Joel Storrow
Josh Freeman

ACCEPTED:

Rick Howell
NAME

City Manager
TITLE

3/11/05
DATE

NORTH CAROLINA
WAKE COUNTY

NORTH CAROLINA DEPARTMENT OF
TRANSPORTATION

AND

TOWN OF BREVARD

MUNICIPAL AGREEMENT

WBS ELEMENT: 37309 1.1

THIS AGREEMENT, made and entered into this the 24th day of August, 2004, between the NORTH CAROLINA DEPARTMENT OF TRANSPORTATION, an agency of the State of North Carolina, hereinafter referred to as the Department, and the TOWN OF BREVARD, a municipal corporation, hereinafter referred to as the Municipality.

WITNESSETH:

WHEREAS, according to G.S. 136-68.2, Section 1, the General Assembly requires each municipality, with the cooperation of the Department to develop a comprehensive transportation plan that will serve present and anticipated travel demand, and,

WHEREAS, the Department may provide financial assistance in the preparation of such plans, and,

WHEREAS, the Department is initiating a new program to encourage the development of comprehensive municipal bicycle plans and pedestrian plans, and,

WHEREAS, to encourage the development of comprehensive local bicycle plans and pedestrian plans, the Department's Division of Bicycle and Pedestrian Transportation (DBPT) and the Transportation Planning Branch (TPB) have created a matching grant program to fund plan development, and,

WHEREAS, all North Carolina municipalities are eligible and are encouraged to apply for a bicycle or pedestrian planning grant, and,

WHEREAS, due to the diverse nature of comprehensive bicycle and pedestrian plans, combined bicycle/pedestrian plans will not be accepted, and,

WHEREAS, the Department and the Municipality have agreed that a comprehensive plan (the "Comprehensive Plan") is needed to evaluate and identify critical municipal needs for planning and/or implementation of bicycle or pedestrian improvements, and,

WHEREAS, the Department and the Municipality have agreed to participate in the cost of the Comprehensive Plan with the Municipality agreeing to coordinate and oversee the study in accordance with the provisions hereinafter set forth.

NOW, THEREFORE, the parties hereto, each in consideration of the promises and undertaking of the other as herein provided, do hereby covenant and agree, each with the other, as follows:

1. The Municipality may hire a consulting firm or enlist the aid of Council of Government staff to develop the Comprehensive Plan, undertake the plan preparation with municipal staff, or produce the plan with a combination of consultant services and municipal staff. If a consultant is hired to develop the plan or any portion of the plan, Department approval must be sought before finalizing the contract. The Municipality shall produce a comprehensive bicycle or pedestrian transportation plan developed in accordance with the Recommended Plan Template made available by the DBPT. The plan will address (but not be limited to) the following, and adhere to the following format: 1. Introduction; 2. Evaluating Current Conditions; 3. Existing Plans, Programs and Policies; 4. Develop Bicycle or Pedestrian System Plan; 5. Facility Standards and Guidelines; 6. Ancillary Facilities and Programs; 7. Project Development; 8. Maps and Illustrations; 9. Recommendations; and 10. Implementing the Plan.

2. The study process shall be governed and guided by the Municipality, which will serve as the Project Manager for the work. The responsibilities of the Project Manager shall include, but not be limited to:

- (A) Overseeing the preparation of a bicycle or pedestrian plan to be produced by Municipal staff, a Private Consulting Firm, or a combination of both and the Department's Project Administrator;
- (B) Submitting quarterly progress reports describing accomplishments and detailing expenditures
- (C) Submitting invoices to the Department for payment.
- (D) Establishing and Coordinating Steering Committee meetings and maintaining records
- (E) Conducting and overseeing the public involvement process

3. The DBPT representative will serve as the Project Administrator on the Department's behalf. The Department's Project Administrator shall make decisions concerning the scope and approach of each Task Order. The responsibilities of the Project Administrator shall include, but not be limited to the following:

- (A) Negotiating the Task Orders for the Comprehensive Plan in accordance with the scope of work;
- (B) Representing the DBPT on the local Steering Committee;
- (C) Providing technical assistance to municipal staff
- (C) Processing the invoices submitted by the Municipality's Project Manager;
- (D) Reviewing and approving the final draft.

4. All documents, including digital files will become the property of the Municipality and the Department. Any and all original graphics, technical drawings, photographs, maps and promotional items produced for the plan or for any public meetings shall be available for use by

the Department in other publications, on the DBPT web site and for display purposes. The Department shall be credited for its participation in all documents, publicity, announcements and materials prepared by the municipality for public meetings.

5. Funding shall be provided for the project as follows:

(A) The Department shall reimburse 80% (Eighty Percent) of the Municipality's actual costs of the Study in an amount not to exceed \$16,000 in accordance with the Department's approved sliding scale, based on municipal population.

(B) The Municipality may bill the Department for actual costs by submitting itemized invoices to the DBPT. These invoices shall be submitted when the project is one-third complete and again when the project is two-thirds complete. One-third of the total allocation will be withheld pending submission of a completed plan including hard copy and digital files with all mapping and related materials to the Department for review and approval by the DBPT. Reimbursement to the Municipality shall be made upon approval of each invoice by the Project Administrator and the Department's Fiscal Section. Proper supporting documentation as may be required by the Department shall accompany said invoice.

(C) The Municipality agrees that it shall bear all project costs for which it is unable to substantiate actual costs and all costs above the Department cap for the Department's matching amount.

(D) Any costs incurred by the Municipality prior to written notification by the Department to proceed with the work shall not be eligible for reimbursement.

(E) Failure on the part of the Municipality to comply with any of these provisions will be grounds for the Department to terminate participation in funding the costs of the project.

(F) All invoices must be submitted within 6 months of completion and acceptance of the project by the Department.

6. The Municipality must adhere to North Carolina Department of Transportation "Rules and Regulations for Major Professional or Specialized Services Contracts". This policy conforms to NCGS 143-64, Parts 31 and 32, and Title 23 of the Code of Federal Regulations, Part 172. Prior to entering into a third party consultant agreement, the Municipality must submit said agreement to the Department for review and approval.

7. The Municipality shall complete said study within twenty-four (24) months of the date of execution of this agreement. If the Municipality has not completed its responsibilities within the specified number of months, the unexpended balance of funds may be recalled by the Department and assigned to the development of other Municipal bicycle or pedestrian planning efforts.

8. If desired and deemed necessary, a quarterly meeting will be scheduled to discuss and review the progress of the study.

9. The public involvement process for the study shall be jointly agreed upon between the Municipality and the Department.

10. Hard copy and digital files of the final documents along with distribution letters shall be submitted to the Department in sufficient quantities to meet normal distribution requirements. The total number of documents will be determined at a later date. The Municipality shall be responsible for the distribution of the final documents.

11. All terms and conditions of this agreement are dependent upon and, subject to the allocation of funds for the purpose set forth in the agreement and the agreement shall automatically terminate if funds cease to be available.

IT IS UNDERSTOOD AND AGREED that the approval of the project by the Department is subject to the conditions of this Agreement and that no further expenditure of funds on the part of the Department will be made until the terms of this Agreement have been complied with on the part of the Municipality.

IN WITNESS WHEREOF, this Agreement has been executed, in duplicate, the day and year heretofore set out, on the part of the Department and the Municipality by authority duly given, as evidenced by the attached certified copy of Resolution, Ordinance or Charter Provision, as the case may be.

L.S.

ATTEST:

Glenda W. Sansosti
CLERK

(MUNICIPAL SEAL)

TOWN OF BREVARD

BY: [Signature]

TITLE: Mayor of Brevard

This instrument has been preaudited in the manner required by the Local Government Budget and Fiscal Control Act.

Terry Scruggs
DIRECTOR OF FINANCIAL SERVICES

TERRY SCRUGGS
NAME PRINTED

Federal Tax Identification Number

56-6001186
Town of Brevard

APPROVED AS TO FORM:

CITY ATTORNEY

NAME PRINTED

DEPARTMENT OF TRANSPORTATION

BY: David S. King

TITLE: Deputy Secretary for Transit

ATTEST:

[Signature]

COPY OF A RESOLUTION PASSED BY THE CITY/TOWN COUNCIL
OF THE TOWN OF BREVARD, NORTH CAROLINA

A motion was made by D. Perkins and seconded by S. Neumann

for the adoption of the following Resolution, and upon being put to a vote was duly adopted:

WITNESSETH:

WHEREAS, according to G.S. 136-66.2, Section 1, the General Assembly requires each municipality, with the cooperation of the Department of Transportation (the "Department") to develop a comprehensive transportation plan that will serve present and anticipated travel demand, and,

WHEREAS, the Department may provide financial assistance in the preparation of such plans, and,

WHEREAS, the Department is initiating a new program to encourage the development of comprehensive municipal bicycle plans and pedestrian plans, and,

WHEREAS, to encourage the development of comprehensive local bicycle plans and pedestrian plans, the Department's Division of Bicycle and Pedestrian Transportation (DBPT) and the Transportation Planning Branch (TPB) have created a matching grant program to fund plan development, and,

WHEREAS, the Department and the Municipality have agreed that a comprehensive plan is needed to evaluate and identify critical municipal needs for planning and/or implementation of pedestrian improvements, and,

WHEREAS, the Department and the Municipality have agreed to participate in the cost of this comprehensive plan with the Municipality agreeing to coordinate and oversee the study in accordance with the provisions hereinafter set forth, and,

WHEREAS, the Town of Brevard desires to enter into a municipal agreement with the Department whereby the Municipality shall develop a Bicycle or Pedestrian Comprehensive Plan (Estimated cost of Study is \$20,000), and,

WHEREAS, the Department shall reimburse 80% (Eighty Percent) of the Municipality's actual costs of the Study in an amount not to exceed \$16,000 in accordance with the approved sliding scale, based on municipal population.

NOW, THEREFORE, BE IT RESOLVED that this Study is hereby formally approved by the City/Town Council of the Town of Brevard and that the Mayor and Clerk of this Municipality are hereby empowered to sign and execute the Agreement with the Department of Transportation.

I, Glenda W. Sansosti Clerk of the Town of Brevard, do hereby certify that the foregoing is a true and correct copy of excerpts from the Minutes of the meeting of the City Council duly held on the 2 day of August, 2004.

WITNESS, my hand and the official seal of said Municipality on this the 9 day of August, 2004.

(SEAL)

Glenda W. Sansosti
CLERK
TOWN OF BREVARD
NORTH

CAROLINA

RESOLUTION NO. 46-06

A RESOLUTION ADOPTING
THE CITY OF BREVARD
COMPREHENSIVE PEDESTRIAN PLAN

WHEREAS, walking is an important mode of transportation which benefits the quality of life for the communities, businesses, residents and visitors within the City of Brevard; and

WHEREAS, a well-designed, extensive, safe pedestrian transportation system is critical to the quality of life of the citizens of Brevard and to the continued vibrancy and growth of the community; and,

WHEREAS, the City of Brevard Land Use Plan and the Focus 2020 Community Report recognize the importance of improving and expanding the City's pedestrian infrastructure to the City's economy, to protecting air quality by reducing automobile emissions, to providing diverse modes of transportation for all citizens, and to maintaining the quality of life within the City; and,

WHEREAS, the City of Brevard has entered into a municipal agreement with the N.C. Department of Transportation whereby the City shall develop and adopt a Comprehensive Pedestrian Plan; and,

WHEREAS, the Brevard City Council established the City of Brevard Comprehensive Pedestrian Plan Committee (hereafter, "the Committee") and charged the Committee with the task of developing a Comprehensive Pedestrian Plan; and,

WHEREAS, the City contracted with McGill Associates to provide consultant services in the development of a Comprehensive Pedestrian Plan; and,

WHEREAS, McGill Associates, in consultation with the Committee and City Staff, and after extensive input from the citizens of the City of Brevard, has prepared the City of Brevard Comprehensive Pedestrian Plan; and,

WHEREAS, on June 6, 2006 the Committee unanimously recommended that Brevard City Council adopt and implement the City of Brevard Comprehensive Pedestrian Plan.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF BREVARD, NORTH CAROLINA, THAT:

Section 1. The City of Brevard Comprehensive Pedestrian Plan (July 17, 2006) is hereby adopted as policy of the City of Brevard, as the primary guide to the installation and improvement of new and existing pedestrian infrastructure within the City.


Section 2. The City of Brevard Comprehensive Pedestrian Plan is hereby referred to the City Manager and the Public Works Director for implementation. The City Manager is directed to prepare proposals for implementation for consideration by Council, including but not limited to a bond referendum, amendments to the adopted Capital Improvement Program, grant applicants, and other appropriate measures.


Section 3. The Public Works Director and the Planning Director are hereby directed to update this plan on an annual basis, with major revisions every five years, and to bring such revised plans before City Council for consideration and adoption.

Section 4. The City Manager is hereby directed to forward this adopted City of Brevard Comprehensive Pedestrian Plan to the Land-of-Sky Rural Planning Organization and the North Carolina Department of Transportation for review, adoption, and implementation as appropriate. The Planning Director is authorized to make such minor amendments and modifications as are required by the North Carolina Department of Transportation for compliance with agency and grant requirements.

Section 5. This Resolution shall become effective upon its adoption and approval.
Adopted and Approved this the 7th day of August, 2006.




ATTEST:


Glenda W. Sansosti, MMC
City Clerk





Jimmy Harris
Mayor

Map 8-1: Existing Pedestrian Facilities

Legend

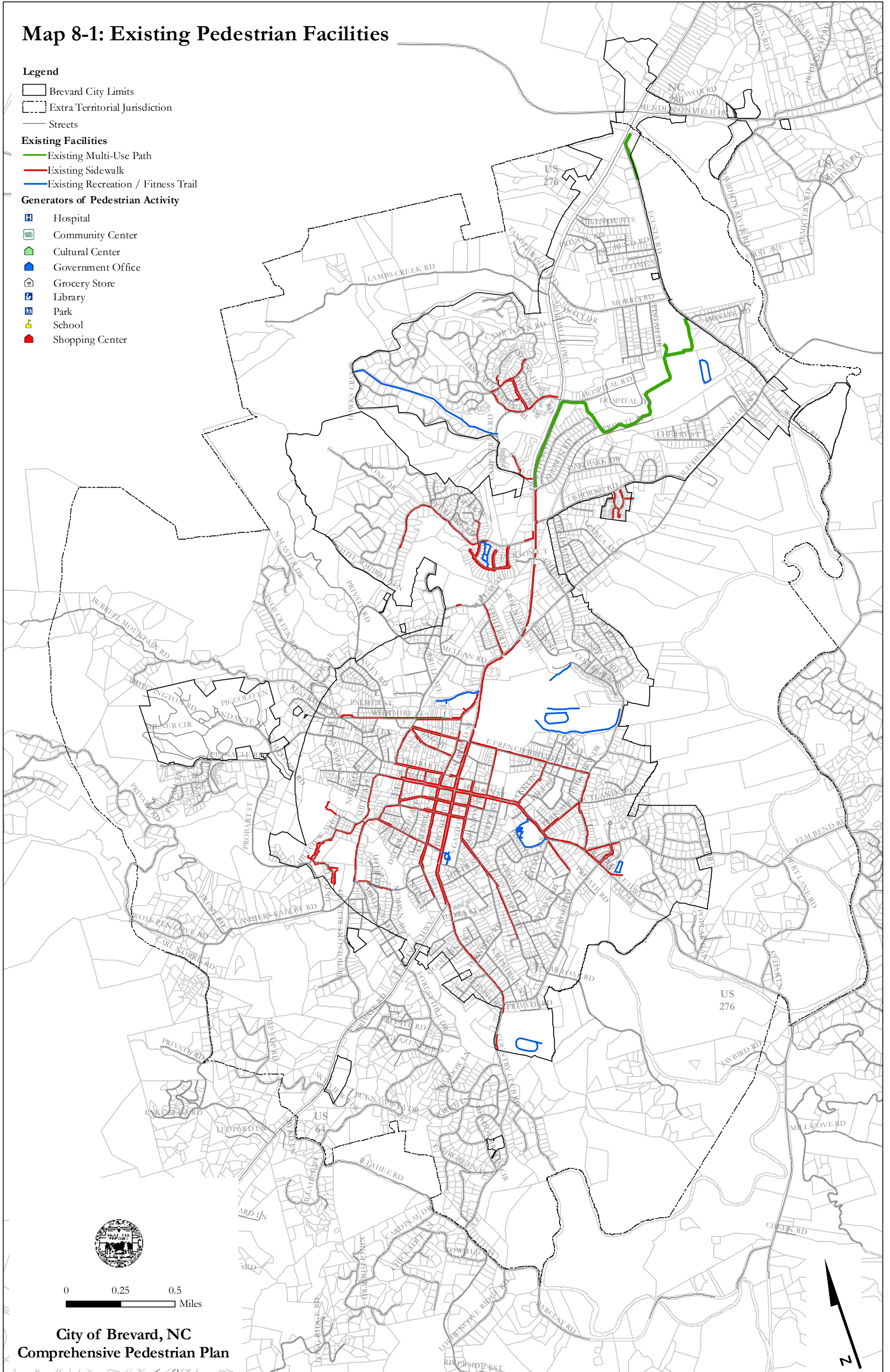
-  Brevard City Limits
-  Extra Territorial Jurisdiction
-  Streets

Existing Facilities

-  Existing Multi-Use Path
-  Existing Sidewalk
-  Existing Recreation / Fitness Trail

Generators of Pedestrian Activity

-  Hospital
-  Community Center
-  Cultural Center
-  Government Office
-  Grocery Store
-  Library
-  Park
-  School
-  Shopping Center


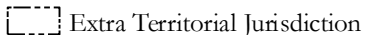
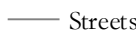


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Map 8-2: Condition of Existing Facilities










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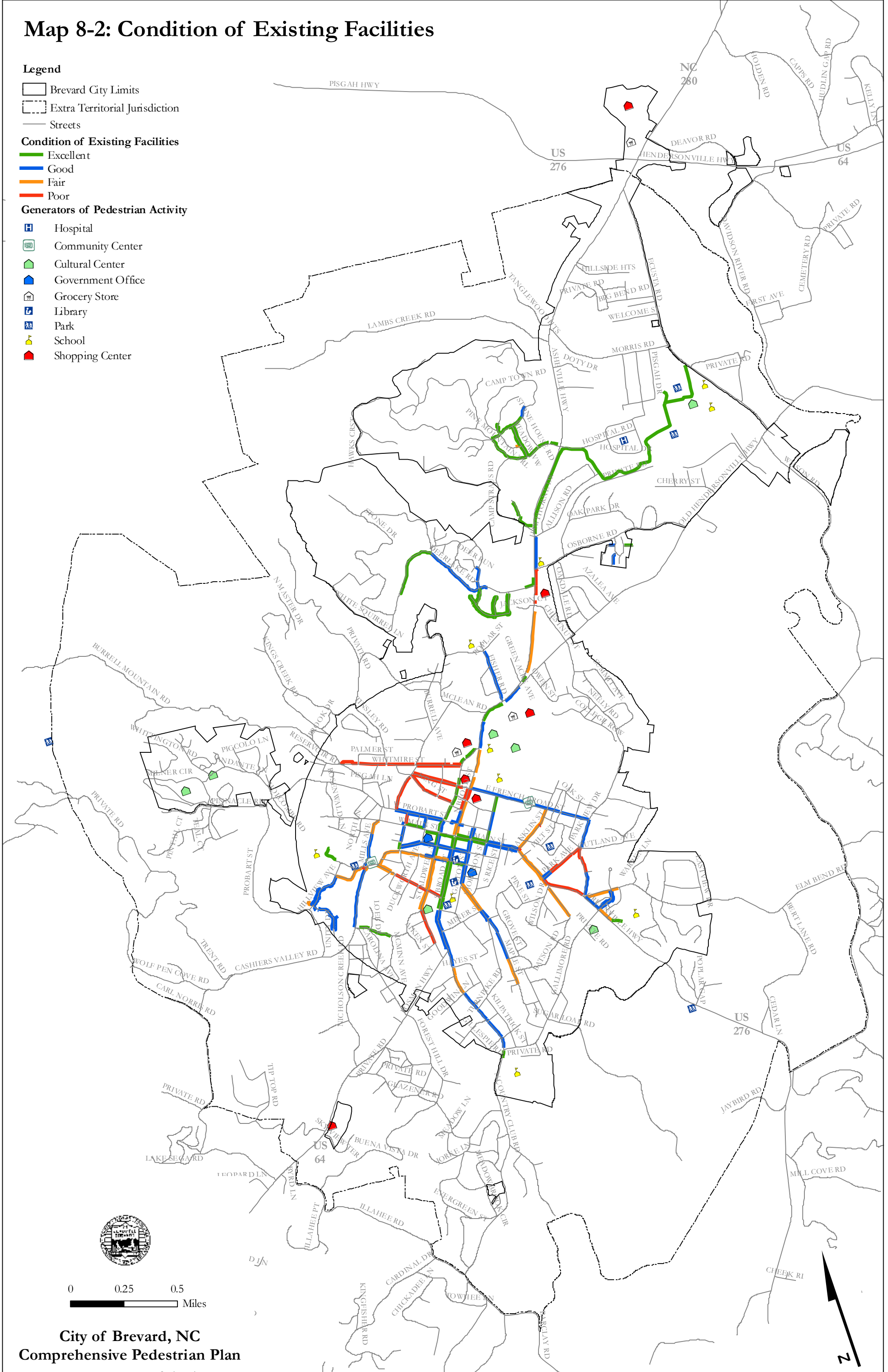
-  Brevard City Limits
-  Extra Territorial Jurisdiction
-  Streets

Condition of Existing Facilities

-  Excellent
-  Good
-  Fair
-  Poor

Generators of Pedestrian Activity

-  Hospital
-  Community Center
-  Cultural Center
-  Government Office
-  Grocery Store
-  Library
-  Park
-  School
-  Shopping Center



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Miles



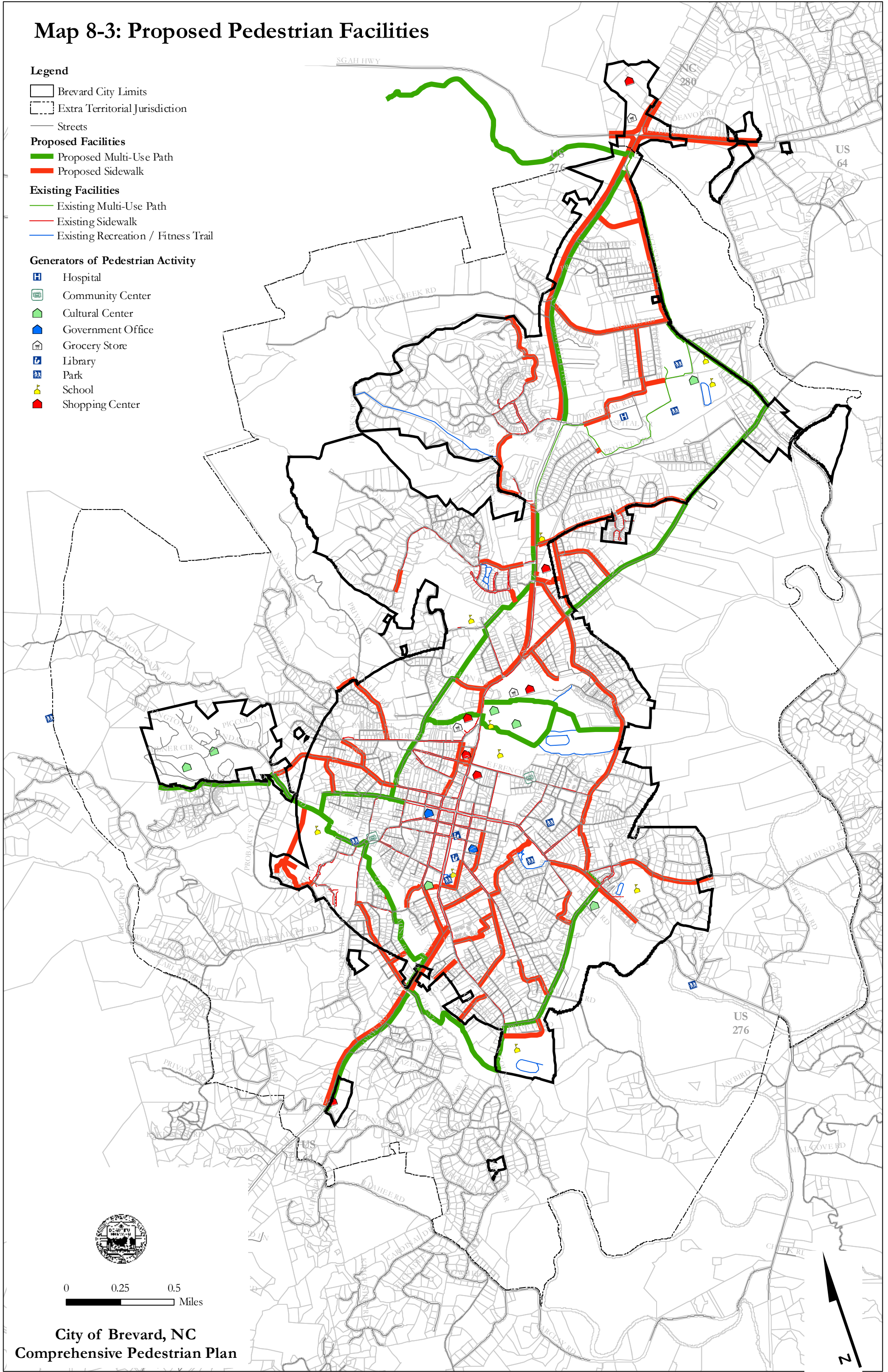
Map 8-3: Proposed Pedestrian Facilities

Legend

- Brevard City Limits
- Extra Territorial Jurisdiction
- Streets
- Proposed Facilities**
- Proposed Multi-Use Path
- Proposed Sidewalk
- Existing Facilities**
- Existing Multi-Use Path
- Existing Sidewalk
- Existing Recreation / Fitness Trail

Generators of Pedestrian Activity

- Hospital
- Community Center
- Cultural Center
- Government Office
- Grocery Store
- Library
- Park
- School
- Shopping Center



















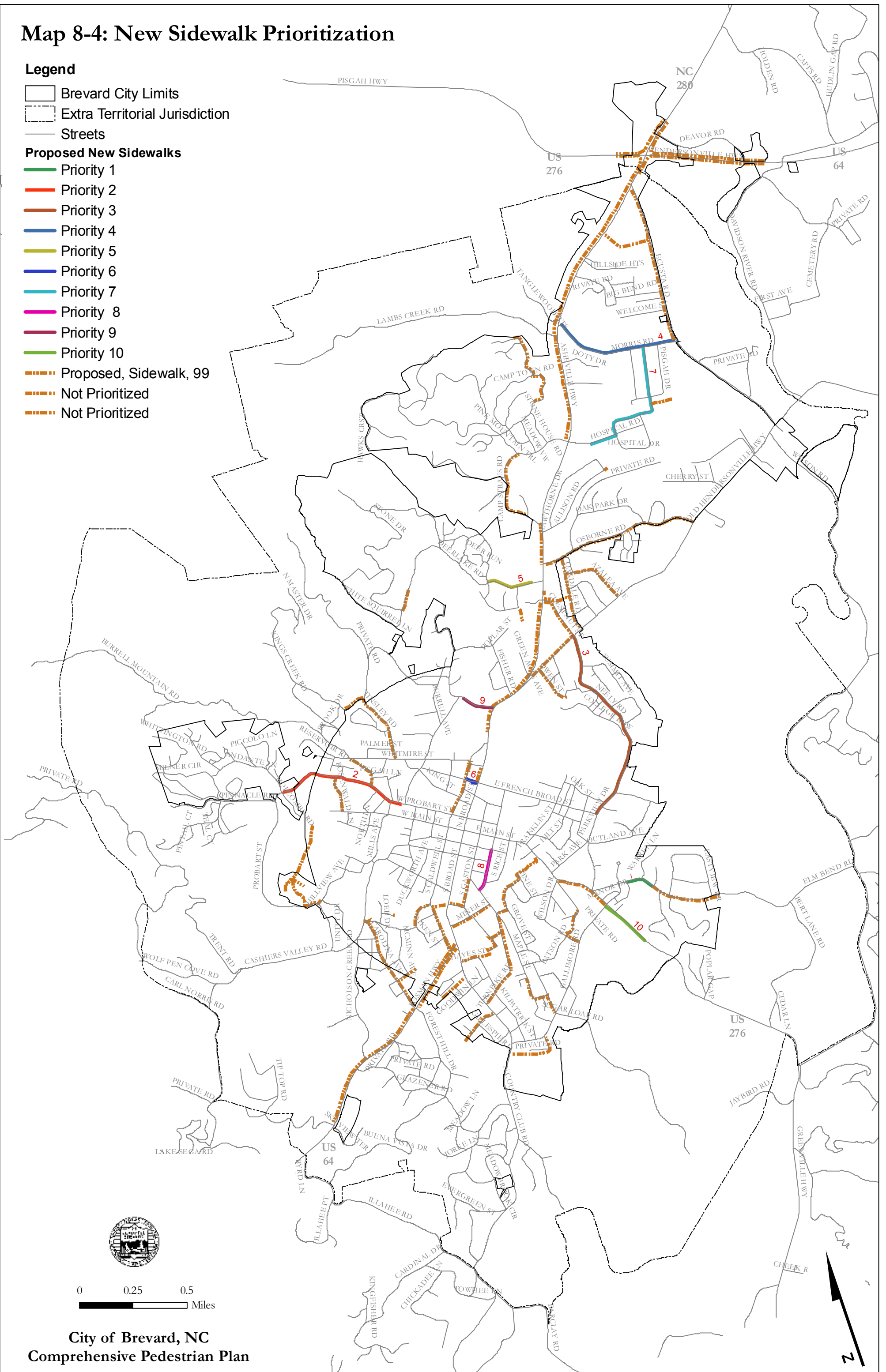
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Miles



Map 8-4: New Sidewalk Prioritization

Legend

-  Brevard City Limits
-  Extra Territorial Jurisdiction
-  Streets
- Proposed New Sidewalks**
-  Priority 1
-  Priority 2
-  Priority 3
-  Priority 4
-  Priority 5
-  Priority 6
-  Priority 7
-  Priority 8
-  Priority 9
-  Priority 10
-  Proposed, Sidewalk, 99
-  Not Prioritized
-  Not Prioritized






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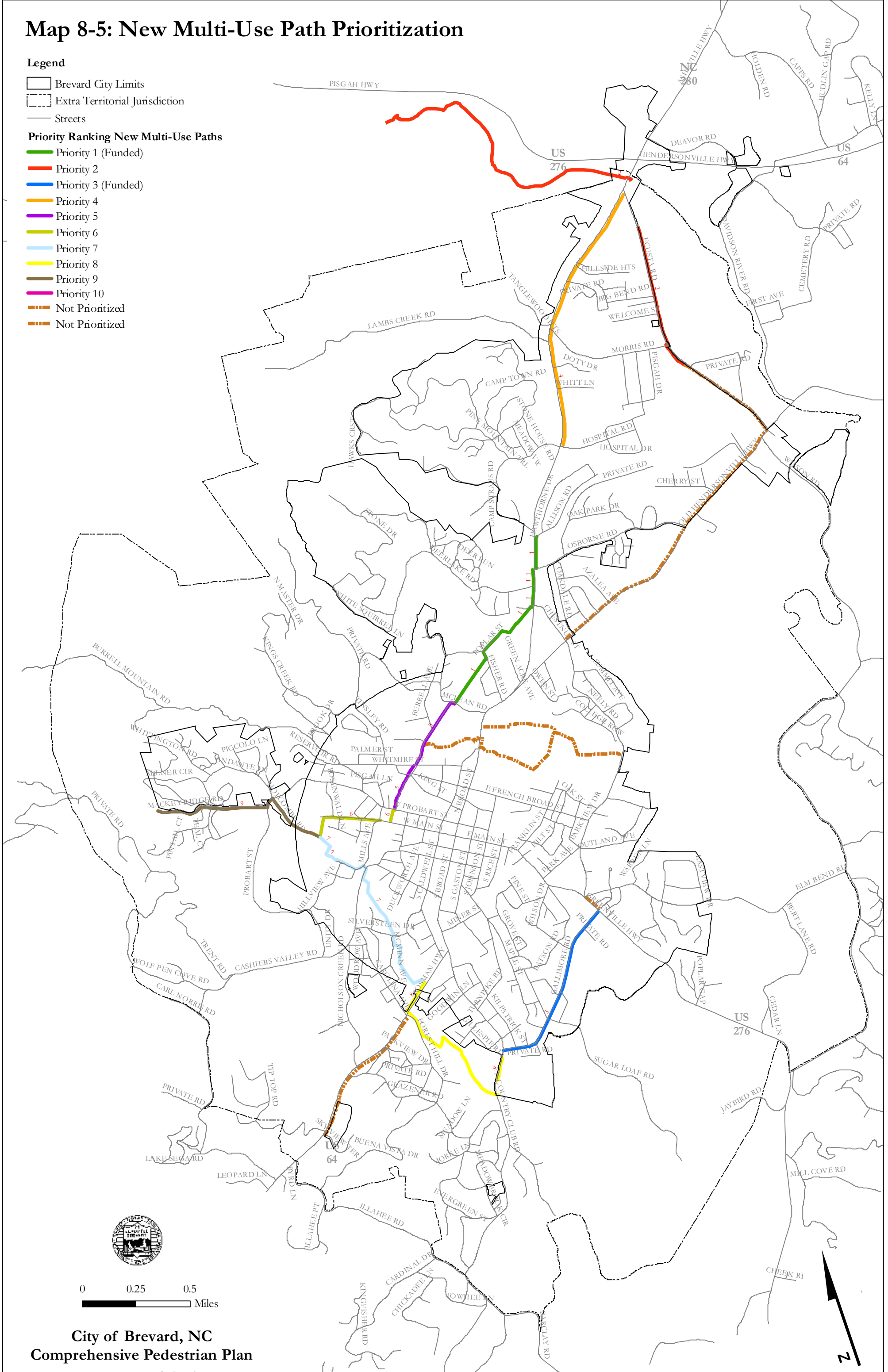
Map 8-5: New Multi-Use Path Prioritization

Legend

-  Brevard City Limits
-  Extra Territorial Jurisdiction
-  Streets

Priority Ranking New Multi-Use Paths

-  Priority 1 (Funded)
-  Priority 2
-  Priority 3 (Funded)
-  Priority 4
-  Priority 5
-  Priority 6
-  Priority 7
-  Priority 8
-  Priority 9
-  Priority 10
-  Not Prioritized
-  Not Prioritized



0 0.25 0.5
Miles

